

BZA

COMPREHENSIVE PERMIT APPLICATION

PETITIONER: CC HRE 2072 MASS AVE TENANT LLC

PETITIONER'S ADDRESS: C/O CAPSTONE COMMUNITIES LLC
1087 BEACON STREET, SUITE 302
NEWTON, MA 02459

NAME, ADDRESS, AND
PHONE NUMBER OF
CONTACT PERSON:

CAPSTONE 2072 MASS AVE LLC
C/O CAPSTONE COMMUNITIES LLC
1087 BEACON STREET, SUITE 302
NEWTON, MA 02459
ATTN: JASON KORB
617.513.6320

HRE 2072 MASS AVE LLC
C/O HOPE REAL ESTATE ENTERPRISES LLC
907 MASSACHUSETTS AVENUE, SUITE 300
CAMBRIDGE, MA 02139
ATTN: SEAN D. HOPE
617.492.0220

LOCATION OF SITE: 2072 MASSACHUSETTS AVENUE, CAMBRIDGE, MA 02140

DESCRIPTION OF PROJECT: CC HRE 2072 MASS AVE TENANT LLC – C/O CAPSTONE COMMUNITIES LLC
COMPREHENSIVE PERMIT: TO CONSTRUCT A NEW MULTI-FAMILY 100%
AFFORDABLE HOUSING COMMUNITY WITH 49 RESIDENTIAL RENTAL UNITS,
GROUND FLOOR COMMERCIAL AND BUILDING AMENITY SPACE. THE PROPOSAL
INCLUDES AN EIGHT STORY BUILDING WITH THREE (3) ACCESSIBLE PARKING
SPACES AND TWO (2) DROP OFF SPACES.

SPECIFY LOCAL
REGULATIONS OR
REQUIREMENTS FROM
WHICH RELIEF IS
REQUESTED:

SEE SECTION 5 REQUESTED WAIVERS FROM LOCAL REQUIREMENTS AND
REGULATIONS

THE PETITIONER IS: A LIMITED DIVIDEND ORGANIZATION

IS THE PROPOSED PROJECT
NEW CONSTRUCTION:

THE PROPOSED PROJECT IS NEW CONSTRUCTION

SITE CONTROL:

CC HRE 2072 MASS AVE TENANT LLC EXECUTED A 99-YEAR GROUND LEASE
WITH CC HRE 2072 MASS AVE LLC (AN AFFILIATE OF CC HRE 2072 MASS AVE
TENANT LLC) FOR THE LAND AND BUILDING AT 2072 MASSACHUSETTS AVENUE.
CC HRE 2072 MASS AVE LLC CURRENTLY OWNS 2072 MASSACHUSETTS AVENUE.

SITE ELIGIBILITY:

THE PETITIONER IS SIMULTANEOUSLY APPLYING TO DHCD FOR SITE ELIGIBILITY
APPROVAL UNDER THE FOLLOWING SUBSIDIES: AFFORDABLE HOUSING TRUST
FUND, DHCD HOUSING STABILIZATION FUND (HSF), HUD HOME PROGRAM
(RENTAL PRODUCTION), STATE LOW INCOME HOUSING TAX CREDIT PROGRAM
AND THE FEDERAL LOW INCOME HOUSING TAX CREDIT PROGRAM (LIHTC).

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OFFICE OF THE CITY CLERK
CAMBRIDGE, MASSACHUSETTS

TOTAL NUMBER DWELLING UNITS PROPOSED: 49
TOTAL NUMBER OF AFFORDABLE RENTAL UNITS: 49
TOTAL NUMBER OF AFFORDABLE HOME OWNERSHIP UNITS: 0

RESIDENT ELIGIBILITY STANDARDS: 41 UNITS AT OR BELOW 60% AMI, 8 UNITS AT OR BELOW 30% AMI, PURSUANT TO THE LOW INCOME HOUSING TAX CREDIT PROGRAM


PLEASE SEE **SECTIONS 3 – 16** FOR A COMPLETE DESCRIPTION OF THE PROPOSED PROJECT, AND EACH OF THE FOLLOWING ITEMS:

- a) Site Development Plans - Site development plans showing locations and outlines of proposed building, existing street elevations, traffic patterns and character of open areas, if any, in the neighborhood;
- b) Report on Existing Site Conditions - a summary of conditions in the surrounding areas, showing the location and nature of the existing building, existing street elevations, traffic patterns and character of open areas, if any, in the neighborhood;
- c) Drawings – scaled, architectural drawings, including typical floor plans, typical elevations and sections, and identifying construction type and exterior finish. All projects of five or more units must have Site development plans signed by a registered architect;
- d) Building Tabulations - a tabulation of the proposed building by type, size (number of bedrooms, floor area) and ground coverage, and a summary showing the percentage of the tract to be occupied by the building by parking and other paved vehicular areas and by open areas; (2 copies)
- e) Subdivision Plan - where a subdivision of land is involved, a preliminary subdivision plan; (2 copies)
- f) Utilities Plan - a preliminary utilities plan showing the proposed location and types of sewage, drainage, and water facilities, including hydrants.
- g) Dimensional Form- provided with application; (2 copies)
- h) Photographs – photographs of Site and existing building;
- i) Assessor's Plat – available at City of Cambridge, Engineering Department, 147 Hampshire Street, Cambridge, MA;
- j) Ownership Certificates – 2 Notarized copies, provided application.

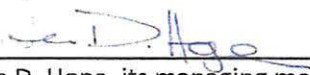
I certify that the information contained herein is true and accurate to the best of my knowledge and belief.

CC HRE 2072 MASS AVE TENANT LLC

By: Capstone 2072 Mass Ave LLC, its managing member


By: Jason Korb, its managing member

By: HRE 2072 Mass Ave LLC, its managing member


By: Sean D. Hope, its managing member

Date: November 10, 2020

BZA APPLICATION FORM - OWNERSHIP INFORMATION

To be completed by OWNER, signed before a notary and returned to The Secretary of the Board of Zoning Appeals.


I/We CC HRE 2072 Mass Ave LLC
(OWNER)

Address: c/o Capstone Communities LLC, 1087 Beacon Street Suite 302, Newton MA 02459

State that I/We own the property located at 2072 Massachusetts Avenue,
which is the subject of this zoning application.

The record title of this property is in the name of CC HRE 2072 Mass Ave LLC

*Pursuant to a deed of duly recorded in the date 04/10/2018, Middlesex South
County Registry of Deeds at Book 70850, Page 295; or
Middlesex Registry District of Land Court, Certificate No. _____
Book _____ Page _____.

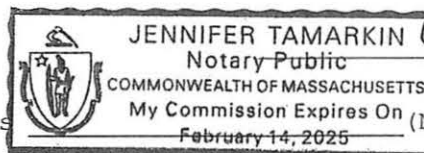

SIGNATURE BY LAND OWNER OR
AUTHORIZED TRUSTEE, OFFICER OR AGENT*

*Written evidence of Agent's standing to represent petitioner may be requested.

Commonwealth of Massachusetts, County of Middlesex

The above-name Jason Korb personally appeared before me,
this 9th of November, 2020, and made oath that the above statement is true.

My commission expires



 Notary

(Notary Seal).

- If ownership is not shown in recorded deed, e.g. if by court order, recent deed, or inheritance, please include documentation.

DIMENSIONAL INFORMATION
Project Address: 2072 Massachusetts Avenue

		EXISTING CONDITIONS	ORDINANCE REQUIREMENTS		REQUESTED CONDITIONS
			BA-2 / BUSINESS A-2	RESIDENCE B	RESIDENTIAL USE
Lot Area (SF)		8,515 SF	No minimum	5,000 SF (min.)	8,515 SF
Lot Width (Ft)		~75.46' @ Massachusetts Avenue	No minimum	50' (min.)	~75.46' @ Massachusetts Avenue
Total Gross Floor Area (GFA)(SF)		1,860 SF	15,755 SF (max.)(g)	608 SF (max.)(g)	57,395 SF
Residential Base		0	13,129 SF (max.)(g)	507 SF (max.)(g)	54,425 SF
Non-Residential Base		1,860 SF	0	0	2,970 SF (d)
Inclusionary Housing Bonus w/20% affordable		N/A	2,626 SF (max.)(g)	101 SF (max.)(g)	N/A
Ratio of Floor Area to Lot Area	Baseline:	N/A	1.0 / 1.75 (max.)	0.5/0.35 for portions exceeding 5,000 SF (max.)	6.74
	MAOD:		1.75 for mixed-use / 1.0 for all other uses (max.)	1.75 for mixed-use / 1.0 for all other uses (max.)	6.74
Residential Base	Baseline:		1.75	0.5/0.35 for portions exceeding 5,000 SF	6.27
	MAOD:		1.75	1.75	6.27
Non-Residential Base	Baseline:		1.0	N/A	0.47
	MAOD:		N/A	N/A	0.47
Inclusionary Housing Bonus - %	Baseline/MAOD:		20% bonus = 2,626 SF (GFA)(g)	20% bonus = 101 SF (GFA)(g)	N/A
Total Dwelling Units	Baseline/MAOD:		16 (max.)	0	49
Base Units			600 SF / D.U. = 12	2,500 SF / D.U. = 0	49
Inclusionary Bonus units - 20%		N/A	2	0	N/A
Base Lot Area / Unit (SF)			625 SF / D.U. @ 12 UNITS	0 UNITS	174 SF / D.U. @ 49 UNITS
Total Lot Area / Unit (SF)			536 SF / D.U. @ 14 UNITS	0 UNITS	174 SF / D.U. @ 49 UNITS
Building Height(s) (Ft)	MAOD : Requirements:	13'	45' (max.)(Baseline Zoning)	35' (max.)(Baseline Zoning)	8 Stories / ~89'-8"
		N/A	50' max. (Massachusetts Avenue Overlay District) - Active non-residential ground floor use - Minimum ground-floor use depth of 40' - Ground floor located at mean grade of abutting sidewalk - Minimum 75% Mass Ave frontage occupancy - Minimum 15' ground floor height - Maximum 5,000 sf per ground floor tenant - No bank frontage > 25'		
Front Yard Setback - Massachusetts Avenue (Ft)(a) [Baseline Zoning - Article 5.33, Table 5-3, footnote (m)]		3.8'	Principal wall plane of an adjacent building facing the same street OR the BA-2 baseline requirement; whichever is less		
Front Yard Setback - Walden Street (Ft)(a)		3.5'	5' (min.)(Baseline/MAOD)	15' (min.)(Baseline Zoning)	
Side Yard Setback - Abut City of Cambridge parking lot (Ft)(a)		42.2'	10' (min.)(Baseline/MAOD)	7'-6" (min.)(sum of 20)(Baseline Zoning)	
Side Yard Setback - Abut Cambridge Housing Authority (Ft)(a)		42.4'	10' min.	7'-6" (min.)(sum of 20)(Baseline Zoning)	0' (Abut Cambridge Housing Authority)
Open Space (% of Lot Area)		78.2%	No minimum		0
Private Open Space		78.2%	No minimum	40% Minimum Private Open Space to Lot Area =	0
Permeable Open Space		0.0%	No minimum	405 SF (min.)(g)	0
Other Open Space (Specify)		N/A	No minimum		0
Off-Street Parking Spaces	Baseline and MAOD:	15 (14 regular, 1 accessible)	1 per D.U. = 49 (min.)	N/A (Multifamily dwellings not allowed)	3 accessible (b)(c)
Long-Term Bicycle Parking		0	1:1 first 20 D.U., then D.U. x 1.05 (min.)	1:1 first 20 D.U., then D.U. x 1.05 (min.)	51 (Residential) + 0.4 (Commercial) = 51 (c)
Short-Term Bicycle Parking		0	0.10 per D.U. (min.)	0.10 per D.U. (min.)	0 (a)
Loading Bays		0	N/A	N/A	N/A
Allowable Uses		N/A	Multi Family Residential, Retail, Restaurant, Office, Institutional and Lab	Residential	Residential / Ground floor commercial / restaurant (f) and other uses as described on the Waiver List

(a) Lot is located on a corner. Project team assumed two front and side yards with no rear yard.

(b) Accessible parking requirement rounded up under UFAS (required for Section 504) to three (3) spaces

(c) Commercial Parking is waived under Article 6.36 based on actual quantity required being below four (4) required spots

(d) Garage and bicycle parking exempt from calculation

(e) Project team pursuing public contribution approach for short-term bicycle parking per Article 6.104.2 (b)

(f) Along with other future possible uses as described on the Waiver List

(g) ~1,013 SF of the total lot area is in Residence B, with the remainder in BA-2

COMPREHENSIVE PERMIT APPLICATION

**2072 MASS AVE APARTMENTS
2072 MASSACHUSETTS AVENUE, CAMBRIDGE, 02140**

**SECTION 1
COVER LETTER AND NARRATIVE**

CC HRE 2072 Mass Ave Tenant LLC
c/o Capstone Communities LLC
1087 Beacon Street, Suite 302
Newton, MA 02459

November 10, 2020

Board of Zoning Appeal
City of Cambridge
831 Massachusetts Avenue
Cambridge, MA 02139

Re: Comprehensive Permit Application for 2072 Mass Ave Apartments
2072 Massachusetts Avenue, Cambridge, MA

Dear Board of Zoning Appeal Members:

CC HRE 2072 Mass Ave Tenant LLC ("Applicant"), an affiliate of Capstone Communities LLC (www.capstonecommunities.com) ("Capstone") and Hope Real Estate Enterprises LLC ("Hope"), propose to construct an affordable housing community located at 2072 Massachusetts Avenue in Cambridge's Porter Square and North Cambridge (the "Site"). The resulting residential community will consist of 49 affordable rental apartment homes with a variety of unit sizes – 14 one-bedroom, 21 two-bedroom, and 14 three-bedroom apartments – that will be affordable to individuals and families earning a range of incomes – from 30% to 60% of the area median income (the "Project"). Capstone is a Newton-based developer of mixed income, affordable, and historic apartment communities, and Hope is a Cambridge-based real estate development company with significant zoning consultation and permitting expertise. Capstone and Hope are currently developing Frost Terrace, a 40-apartment 100% affordable housing community located a few blocks south at 1785-1791 Massachusetts Avenue which is anticipated to open in spring 2021. Previously, Hope and Capstone developed Port Landing, a 20-apartment, 100% affordable housing community in The Port/Kendall Square neighborhood in 2016.

Development Proposal

Existing Site Conditions

Located with frontage on Massachusetts Avenue to the northeast and Walden Street to the northwest, the Site is comprised of one lot with approximately 8,515 square feet. Currently, the Site is occupied by an 1,860 sf, one-story building leased to Darul Kabab restaurant. The building is widely known to have housed Kentucky Fried Chicken for many years.

The immediate context along Massachusetts Avenue includes a mix of commercial, residential, and institutional buildings directly fronting the Avenue. The current one-story building and surface parking lot on the Site were constructed in 1971, having replaced a ca. 1890 four-story Odd Fellows Building which occupied most of the parcel. Though the Massachusetts Avenue corridor maintained a small-scale residential character throughout much of the 19th century, by the early 20th century the blocks north of Porter Square were dominated by multi-story commercial, civic, and religious buildings directly fronting the

Avenue. Massachusetts Avenue remains a densely developed artery lined with multi-story commercial, residential, and institutional buildings.



I.O.O.F. Building formerly located at 2072 Massachusetts Avenue, depicted during the 1968 fire which led to its demolition (*Cambridge Chronicle*).

Proposal Summary

The Project will include forty-nine (49) 100% affordable rental housing apartments. The affordability will be in perpetuity. Approximately 71% (35 apartments) will be two and three bedrooms for families. Specifically, the proposal includes 14 one-bedroom apartments, 21 two-bedroom apartments, and 14 three-bedroom apartments. Units average in size from 625 sf (one-bedroom apartments) to 824 sf (two-bedroom apartments) to 1,087 sf (three-bedroom apartments). The high proportion of two- and three-bedroom apartments will provide safe and accommodating housing to families.

Additionally, the Project will include three handicapped accessible parking spaces and two short-term drop-off/pick-up spaces in a covered garage area at the rear of the Site, along with 51 long term bike parking spaces located on the lower level of the building (48 regular and 3 tandem). Electric vehicle charging stations will be provided for the three accessible spaces and power outlets will be provided in the bike room for electric bicycles and repairs. The Project will also include retail space on Massachusetts Avenue and a resident amenity space within the first floor. The retail space is slated for a community use. The Project's operating budget also includes a part-time resident services coordinator who will plan events for families and others in the building and will assist with creating community building and educational activities.



Current Condition



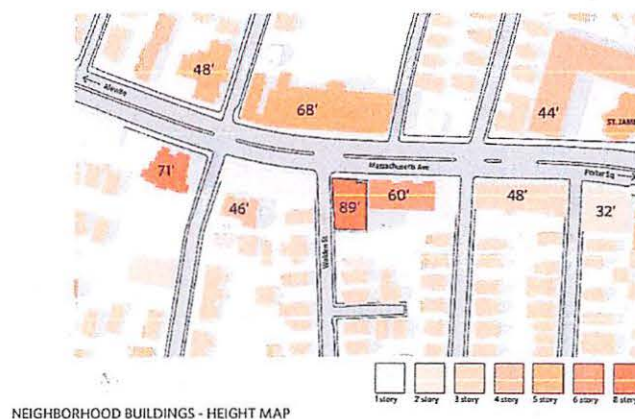
Proposed Condition

The new eight-story building will be constructed to Passive House US (PHIUS) standards and will target PHIUS Certification, and will include a green roof, rooftop solar along with a stormwater management system. The resilient, environmentally friendly materials used to construct the contemporary building façade will ensure long-term sustainability for the Project while adding dimension and rhythm to the Avenue. See **Section 9 Green Building Report** for more information.

Neighborhood

Situated to the northwest (less than 0.3 miles from the center) of Porter Square, the Project will complement an already vibrant mixed-use area of institutional, residential, office, restaurant and retail uses. Located in the North Cambridge neighborhood and adjacent to Neighborhood Nine, the Project's density and scale aligns with other buildings on Massachusetts Avenue. Directly to the Site's southeast and fronting on Massachusetts Avenue is the six-story affordable age restricted Russell House Apartments that is owned and operated by the Cambridge Housing Authority. Russell House Apartments is approximately 60' and contains 52 apartments. The five-story, 68' historic Henderson Carriage Building is located across Massachusetts Avenue from the Site. 2130 Massachusetts Avenue, an eight-story, 71' building is located one block to the northwest of the Site. Directly across Walden Street from the Site is a single-story retail building and the three and a half story 5 Walden Street condominiums. Spanning the entire rear of the Site is a municipal parking lot that provides an almost 50' wide buffer from the mostly three-story multifamily residential neighborhood to the west. The owners of the Henderson Carriage Building and Russell House Apartments support the Project.

The below Neighborhood Buildings – Height Map details the surrounding building heights.



Area Amenities

This Site is well-served by public transit. The Porter Square MBTA Redline and Commuter Rail stations are less than 0.3 miles from the Site and the Davis Square MBTA Redline is located 0.5 miles from the Site. In addition, the #77 and #83 MBTA bus routes are at the Site, the nearest BLUEbikes station is located directly across the street, and the nearest Zipcar space is less than 200 feet away. These various rapid transit options provide access to jobs, services, and amenities throughout Cambridge, Boston, and the suburbs. Additionally, the Site is located within walking distance to the Porter Square Shopping Center, which provides several key family-friendly amenities including a grocery store, pharmacy, hardware store and various other community-serving businesses. In the context of Cambridge's extreme shortage of affordable family housing, the Project will provide forty-nine (49) households with access to a desirable, transit oriented, and thriving neighborhood. According to Walkscore.com, the Site's Walk Score is 97, which is considered a Walker's Paradise.

The Site is also within walking distance to a number of public elementary schools and daycare centers including the Rindge Avenue Upper School (0.2 miles or 6-minute walk), Wild Rose Montessori School (0.3 miles or 7-minute walk) and Benjamin Banneker Public Charter School (0.4 miles or an 8-minute walk). Below is a partial list of amenities within 0.5 miles of the Site. The play yard at St. James Church will be open to the Project's residents as well as the general public from 8am to dusk every day of the week with few exceptions. St. James Church, at 1991 Massachusetts Avenue, is one block from the Project.

Restaurants		Groceries		Shopping	
Wasabi at Porter	0.03 Miles	Star Market	0.2 Miles	China Fair	0.01 Miles
McCabe's on Mass	0.08 Miles	Pemberton Farm	0.2 Miles	Barefoot Books	0.04 Miles
Andy's Diner	0.1 Miles	Spindler Confections	0.2 Miles	Seth Berman Gardeners	0.05 Miles
Posto	0.2 Miles	Stop & Shop	0.3 Miles	Drinkwater's	0.05 Miles
Palm Sugar Thai Cuisine	0.2 Miles	Davis Square Farmers Market	0.3 Miles	Big Picture Framing	0.09 Miles
Dakzen	0.2 Miles			General Optical Co.	0.01 Miles
The Shawarma Place	0.2 Miles	Parks		Fun Antiques	0.2 Miles
Newtowne Grille	0.2 Miles	Bergin Playground	0.2 Miles	Stellabella toys	0.2 Miles
Domino's Pizza	0.2 Miles	Rindge Field	0.2 Miles	The Caning Shoppe	0.2 Miles
Sugar & Spice Thai	0.2 Miles	Kenney Park	0.3 Miles	Books by Design	0.2 Miles
Urban Hearth	0.3 Miles	Corcoran Playground	0.4 Miles	Nebia	0.2 Miles
Punjabi Grill	0.3 Miles	Statute Park	0.4 Miles	Buffalo Exchange	0.3 Miles
Rosebud American Cuisine	0.3 Miles	Sheridan Square	0.4 Miles	Watch Shop	0.3 Miles
Snappy Kitchen	0.3 Miles	Seven Hills Park	0.4 Miles	Ace Wheel Works	0.3 Miles
Anna's Taqueria	0.3 Miles	Saint Peters Field	0.5 Miles	Michaels	0.3 Miles
Christopher's	0.3 Miles			Family Dollar Store	0.3 Miles
Café Barada	0.3 Miles	Schools		Magpie	0.3 Miles
Redbones BBQ	0.3 Miles	Ringe Avenue Upper School	0.2 Miles	Sprint	0.3 Miles
Toad	0.3 Miles	Wild Rose Montessori School	0.3 Miles	Mind's Eye Yarns	0.3 Miles
Wok N Roll Restaurant	0.3 Miles	Benjamin Banneker Public Charter	0.4 Miles	Porter Square Books	0.3 Miles
Panera Bread	0.3 Miles	Cambridge Friends School	0.4 Miles	Bike Boom	0.4 Miles

Building Program and Site Design

The Project's design balances a complex set of contextual priorities for the Site which have arisen from analysis, community meetings, and various meetings with the City of Cambridge Urban Planning, Community Development and Traffic and Parking teams. The design goals are:

Create a high-quality affordable housing apartment community for families in Cambridge

The design reflects its context, a strong corner on Cambridge's main street. The building massing is slender, allowing for light and air for all apartment homes. The site is linked to amenities and services in this walkable neighborhood and the unit interiors are simple, but elegant.

Create significant transparency: Activate the streetscape

A primary design goal is to maintain transparency at street level to facilitate visual connections and to activate the streetscape. The first floor along Mass Ave will be wrapped with transparent storefront glazing and will include both the retail and residential entryway. The residential entryway will be pushed back to create a more pedestrian-scaled experience for residents. The storefront turns the corner onto Walden St and continues along with transparency into the retail and resident amenity spaces. An entrance to limited parking and services area is hidden on the rear part of the site.

Shaping: Building Form

The building consists of two interconnected 8-story volumes. Along Massachusetts Avenue and West along Walden Street, a seven-story suspended aluminum clad cube sits above one story of recessed with storefront glazing. The suspended cube of the floors is clad in light colored anodized aluminum / zinc vertical panels with a staggered pattern, creating a light and airy volume that floats above the street, minimizing the Project's perceived mass, and also has a strong corner presence that is visible from the street intersection. A grid of openings accentuates the suspended cube, incorporating large windows which provide natural daylight into the interior units, as well as vertical infill composite panels that resemble wood, bringing warmth and texture. Each window and infill panel pair are framed with a metal projection that provides a rhythm to the façade.

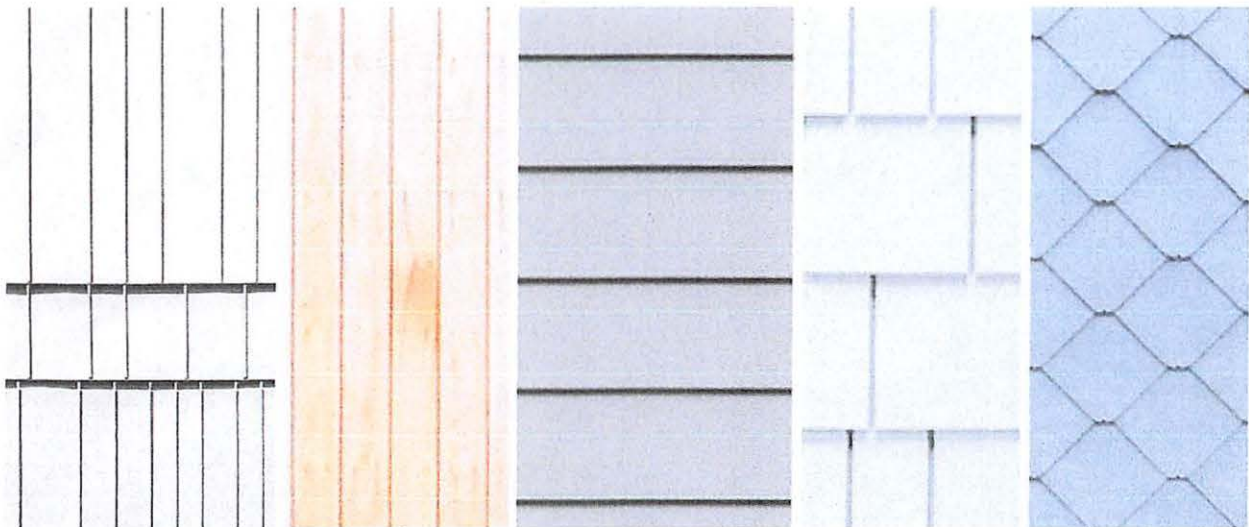
Human Scale: Material Palette

The residential anchor consists of a materials palette that relates to the surrounding residential buildings. The brick plinth / base is located on the ground floor, most prominently at the residential entry which is recessed along Mass Ave, adjacent to the retail space and bus shelter. The crafted long brick at street levels enlivens the pedestrian experience and allows for interactions as the wall angles back and forth. This culminates in a gradient of increasingly frequent openings that also serve as visibility into the parking area, as well as creating ventilation. Above the parking entry, the brick is oriented vertically as a contemporary response to the traditional lintel approach, while highlighting the playfulness of the material.



Brick pattern material precedents

Above the brick plinth, painted clapboard comprises the main field of the body. Proportionately sized and spaced windows bordered by a trim are located along all orientations of the residential anchor. On the south elevations, each window has an infill panel below, clad in a lighter colored shingle panel to provide an accent from the main field, as well as to give the perception of the same proportions compared to windows on the north and west facades. In addition, solar shades are proposed on the South façade that not only livens the rhythm of the façade, but also serves a functional purpose of providing shade to reduce the building's overall energy consumption. The top of the residential anchor is defined by a cornice band of half-round shingles.



Suspended cube and residential anchor material precedents

Streetscape and Greenery

Except as indicated below, while there is no open space or landscaping on the Site, several street trees are located on the opposite side of Walden Street. Due to the existing electrical duct banks below the sidewalks adjacent to the Site on Massachusetts Avenue and Walden Street, it is not possible to plant street trees in these locations. Planters and fences supporting the growth of vertical green are located on the Project's

southwest facade, and at the openings to the parking and service areas. An intensive green roof is also visible along Walden Street above the parking entrance, with a variety of non-invasive plant species.

There is a 12" DBH *Tilia cordata*, Littleleaf linden located on the Site's south property line. This tree is mostly located on the 2050 Massachusetts Avenue property. Daniel E. Cathcart, an ISA Board Certified Master Arborist, developed a Tree Protection Plan (included herein) that will be followed by the development team prior to, during, and after construction. See **Section 10** for the **Tree Protection Plan**.

Accessibility

The Project is designed to comply with the requirements of 521 CMR, Massachusetts Architectural Access Board (MAAB), the Fair Housing Act, the Americans with Disabilities Act (ADA), as well as the Uniform Federal Accessibility Standards (UFAS). 3 apartments (1 of each bedroom type) in the Project will be Group 2 units for individuals with mobility difficulties (521 CMR 9.4), with the rest being Group 1 units (521 CMR 9.3). In addition, 1 apartment will be a Group 1 unit that also provides sleeping accommodations for persons who are deaf or hard of hearing (521 CMR 9.7).

Sustainability

The Project will incorporate sustainable and resilient design strategies that reflect a commitment to environmental stewardship that aligns with affordable housing with a strong focus on lowering utility costs, carbon and greenhouse gases reduction and creating healthy environments. The Project is targeting Passive House certification to the standards set by the Passive House Institute US (PHIUS) for their PHIUS+ Core rating system, as well as certifying through the EPA Indoor air PLUS program. The PHIUS+ CORE rating system includes stringent and verified building performance metrics as well as professional testing of the building envelope and air sealing at two stages during building construction. EPA Indoor air PLUS certification includes verification of indoor air quality (IAQ) quality control measures including but not limited to: moisture control, HVAC venting and sealing, and use of low VOC materials in construction. In addition, the design team is also planning to conduct a systems commissioning process in addition to the envelope. The above will result in a highly efficient building that lowers utility costs, protects occupant health through excellent indoor air quality, as well as contributes to the overall reduction in carbon and greenhouse gases emissions. Specifications for a simple, durable materials palette will emphasize the choice of healthier building materials and reinforce the Passive House approach, these measures also act as quality-of-life improvements for the residents, and will be integrated with the management of the property.

In terms of addressing resiliency concerns such as extreme weather events and future climate change, the project team evaluated the flood risk based on current maps and future projections for the site and surrounding area. In addition, various protection, adaptation, and backup strategies have been incorporated. Additional details can be found in **Section 9 Green Building Report**.

Affordable Housing

Due to its unit mix and income set asides, the Project's 49 apartment homes will attract a variety of households. 41 units will be set aside for households that earn at or below 60% of AMI, currently a household income ranging from \$53,760 – \$82,920, depending on household size. Eight (8) units will be Section 8 PBV units set aside for households that earn at or below 30% of AMI, currently there is no minimum household income, and a maximum income of \$30,720 – \$41,460, depending on household size. Including all utilities and for the 60% AMI apartments, monthly one-bedroom rents are estimated to be \$1,440, monthly two-bedroom rents are estimated to be \$1,728, and monthly three-bedroom rents are

estimated to be \$1,995. When accounting for utilities, these rents are approximately 40% of the neighborhood's market rate rents, which are deeply unaffordable to households in this income range.

The proposed income mix provides housing that is affordable to families and individuals earning a range of incomes. The immediate market area shows strong demand for this unit mix as effective vacancy rates are 0%. The demand for affordable units is even more significant than that for market rate apartments. At Port Landing, which was opened in 2016, 1,386 applicants applied for its 20 apartments prior to the lottery. Data obtained from The Finch, an affordable apartment community owned by Homeowner's Rehab which completed its lease-up in 2020, showed that 2,261 individuals or families applied for 98 apartment homes. There are currently 20,703 unique applicants on the Cambridge Housing Authority waitlists.

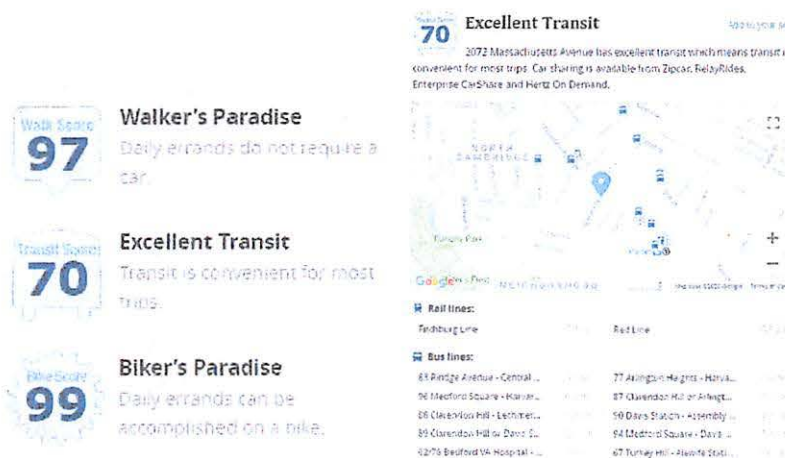
To the extent permitted by Department of Housing and Community Development, 70% of the apartments will give preference to current Cambridge residents, municipal and school department employees, and employees of local businesses.

Transportation and Parking

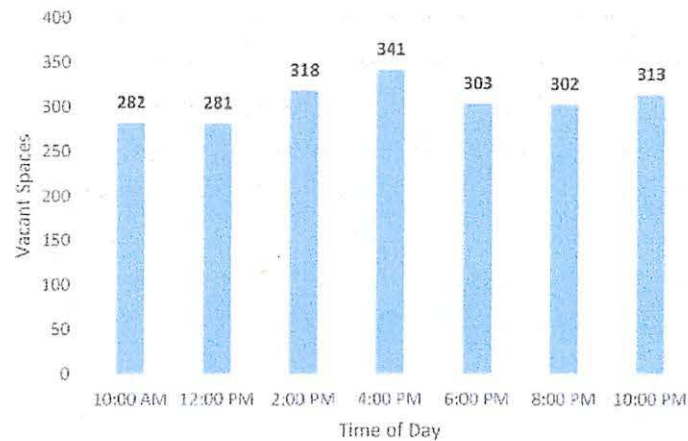
The site plan proposes three (3) covered, on-site accessible parking spaces that will be dedicated to residents and guests with disability plates or placards. In addition, there will be two (2) short-term COVERED drop-off and pick-up spaces to aid in reducing congestion along Walden Street.

As previously indicated, the Site is a short walk from numerous forms of transit options including the Porter Square Station less than 0.3 miles from the Project that includes the MBTA subway Red Line, Fitchburg/South Acton Commuter Rail Line, four Bus Lines (Bus Route #77, 96, 83 and 87) and several car sharing locations. The Parking and Traffic Assessment by Vanasse & Associates, Inc. (VAI) included in **Section 6** suggests approximately half of the building's households, or ~25 households, will have cars. Those residents with or without cars will benefit from Transportation Demand Management (TDM) that will include covering the cost of MBTA passes for property management staff, subsidizing MBTA passes for residents, BLUEbikes memberships, or ride sharing memberships, as further detailed in the VAI report. In addition, the installation of public transportation and ride share timing screens at a centralized location will provide residents with information for easy access to transit. Ownership and building management will provide information at move-in to all residents on all public transportation options within a short distance of the Site.

Below is a map from Walkscore.com highlighting the Site's adjacent transit options:



VAI determined the Project's projected parking demand and quantified the availability of on-street parking within a quarter (0.25) mile from the Site entirely within the City of Cambridge boundaries during peak parking demand hours. In summary, at the time of its study on Tuesday, October 20, 2020, VAI determined that the peak hour demand was at 12:00pm at which time a total of 281 on-street parking spaces were available within a quarter (0.25) mile of the Site. VAI concludes its report by stating: "In summary, a detailed parking survey was completed in the area of the Project and based upon this data it can be concluded that there is more than sufficient availability of on-street parking to accommodate the Project. The Project proponent is committed to implementing a Travel Demand Management plan which promotes alternatives modes of transportation and will minimize the Project's impact on available on-street parking and traffic in the area."



VAI Parking Study, October 20, 2020, Figure 4

It is expected that many of the Project's residents will utilize alternative modes of transportation other than automobiles. Based upon the U.S. Census and 2018 American Community Survey data for Census Tract 3547, the tract in which the Project is located, the mode split characteristics of the Project are estimated as follows: 32 percent automobile trips; 43 percent transit; 10 percent walk; six (6) percent bicycle, and nine (9) percent other trips. Pursuant to VAI's report, the Project is expected to generate approximately 98 vehicle trips on an average weekday (49 entering/49 exiting), with approximately six (6) vehicle trips (2 entering/4 exiting) expected during the weekday morning peak-hour. During the weekday evening peak hour, the Project is expected to generate approximately 9 new vehicle trips (5 entering/4 exiting).

The Project's proximity to several alternative transit options and community serving amenities (schools, pharmacy, grocery store) within walking distance will produce a thriving walkable residential community. The Project's design is consistent with smart growth principles and the Cambridge City Council's goal to reduce reliance on vehicle usage while promoting alternative forms of transportation.

Walden Street Widening

Through discussions with Cambridge Community Development, Traffic and Parking, Department of Public Works, and community outreach, the development team identified that the portion of Walden Street adjacent to the Site is unusually narrow for a three lane street. Currently Walden Street is ~26'-8", with a ~10'-0" travel lane outbound from Massachusetts Avenue and two ~8'-4" travel lanes inbound. This constriction slows traffic moving through the intersection, makes turns difficult, and does not align with best practices and guidelines.

The proposed design includes the widening of Walden Street from ~26'-8" to 30'-0". This is achieved by locating the exterior walls of the ground floor ~4'-0" inside the property line and then granting an easement to the City for a sidewalk in that area, resulting in three (3) 10'-0" travel lanes and making a more spacious street condition. The proposed design also improves the sidewalk by widening it from its current ~6'-5" width to be ~7'-2" wide, both measurements are inclusive of the curb. The proposed upper floors of the new building overhang the sidewalk by ~3'-6" with an overhead clearance of ~13'-0".

Regarding the constructability of widening Walden Street, the development team approximately located electrical duct banks in the Walden Street sidewalk. Based on visual inspections of the manholes and a utility Ground Penetrating Radar (GPR) report by GPRS, it seems unlikely that the location of these duct banks will conflict with the proposed construction associated with the widening of Walden Street. Conversations with Eversource on the exact elevation of the duct banks are ongoing. The GPR report is included in **Section 8**.

Evidence of Need for Affordable Housing:

The City of Cambridge historically was the home of immigrants and low and middle-income earners that were vital to Cambridge's glassworks and furniture factories from the 1920's through the 1970's. However, since rent control ended in 1994, Cambridge has experienced an exponential increase in land value, resulting in a disproportionate impact on the availability of affordable housing options for low and middle-income individuals and families. HUD defines "cost burdened" households as those who pay more than 30% of their income for housing.¹ According to the U.S. Census Bureau, 43% of renters in Cambridge are paying 30% or more of their household income on rent and are therefore considered cost burdened.²

Lower and middle income families in Cambridge find it exceptionally difficult to secure family friendly housing throughout the City. In the last six (6) months, accordingly to MLS, the median sale price for a single family home in Cambridge was \$1,733,500 and the average condominium sale price was \$812,500. Based on an informal October 2020 survey of larger apartment communities in the immediate market area, two-bedroom apartment rents are \$3,600 and three-bedroom apartment rents are \$4,500.³ In order to afford these rents and not be cost burdened, a household would need to earn at least \$152,000 – \$190,000 annually. Over 60% of Cambridge households make less than \$150,000, making these units unaffordable to a majority of current Cambridge residents.⁴

The City's housing stock is also older and many units contain lead based paint hazards, which further constrain families from locating safe, quality housing for their children. In fact, 71.4% of Cambridge's housing inventory was constructed prior to 1980.⁵ Upon completion, all of 2072 Massachusetts Avenue's apartments will be new.

Once completed, the Project will provide urgently needed affordable housing in an area where a significant number of families and individuals are unable to afford quality housing. Additionally, the high number of three-bedroom apartments (29%) will specifically be occupied by families.

¹ U.S. Department of Housing and Urban Development, "Housing Choice Voucher Program" <https://www.hud.gov/hudprograms/hcwp> (accessed October 9, 2020)

² U.S. Census Bureau American Community Survey 2019: ACS 1-Year Estimates TableID DP04

³ The Wyeth – 120 Rindge Avenue, Cambridge

⁴ U.S. Census Bureau American Community Survey 2019: ACS 1-Year Estimates TableID S2503

⁵ U.S. Census Bureau American Community Survey 2019: ACS 1-Year Estimates TableID DP04

Financing

If 2072 Massachusetts Avenue's Site Comprehensive Permit application is approved, the development team anticipates applying for an array of local, state and federal subsidies as well as private investments. The development team will apply to the Massachusetts Department of Housing and Community Development (DHCD) for an allocation of state and federal low-income housing tax credits as well as additional subsidies.

In 2018 the Cambridge Affordable Housing Trust provided a \$3.8 million loan to finance the Site acquisition and certain predevelopment expenses. If the Project receives its permitting, the development team anticipates applying for additional City funding and for eight (8) Cambridge Housing Authority Section 8 Project Based Vouchers.

Site Control, Permitting and Community Process

Site Control

Capstone and Hope have created separate entities that own/will own the Site/Project and that will develop the Project. CC HRE 2072 Mass Ave LLC, an affiliate of Capstone and Hope, purchased the 2072 Mass Ave land and building using loan proceeds from the Cambridge Affordable Housing Trust in April 2018.

On November 9, 2020, CC HRE 2072 Mass Ave LLC entered into a 99-year ground lease with CC HRE 2072 Mass Ave Tenant LLC for the 2072 Massachusetts Avenue land. See the attached organizational chart that outlines the entity structure once the Project receives all its financing. This ground lease structure is typical of affordable housing developments in Cambridge due to the complicated financing structure required by local and state financing agencies.

Permitting

CC HRE 2072 Mass Ave Tenant LLC is a Limited Dividend Organization under M.G.L. c.40B, §§ 20 through 23. Pursuant to 760 CMR 56, CC HRE 2072 Mass Ave Tenant LLC is applying to the Department of Housing and Community Development, as the Subsidizing Agency, for Project Eligibility under the following subsidy programs: Affordable Housing Trust Fund, DHCD Housing Stabilization Fund (HSF), HUD HOME Program (Rental Production), State Low Income Housing Tax Credit Program, and the Federal Low Income Housing Tax Credit Program (LIHTC). An affordable housing restriction will be recorded against the land and buildings with a term in perpetuity and the Project will comply with the Affirmative Fair Housing Marketing and Resident Selection Plan as required by 760 CMR 56.

According to the Zoning Map, the majority of the Site has a base zoning of Business A-2 and a small portion (13'+/-) at the rear of the parcel is in the Residence B base zoning district. Both districts allow for residential uses as of right although the Business A-2 district also allows for a range of commercial/retail and multi-family residential uses whereas the Residence B district is a one- and two-family district only. The Site also is in the Massachusetts Avenue Overlay District and North Massachusetts Overlay Sub- district (the "Massachusetts Avenue Overlay").

The development team is proposing to obtain all of 2072 Massachusetts Avenue's local approvals through an MGL Chapter 40B Comprehensive Permit from the Cambridge Board of Zoning Appeal (BZA). Since the City of Cambridge has met its obligations under MGL Chapter 40B, the development team is requesting that the BZA accept this Comprehensive Permit application. Pursuant to 760 CMR 56.05(7), please refer to

Section 5 for a detailed list of requested Waivers from Local Requirements and Regulations. **Section 4** is the Dimensional Form.

Community Process

The development team has worked extensively with the City of Cambridge, including the Community Development Department (Zoning, Housing, Sustainability, Urban Design, Pedestrian and Bicycle), Fire Department, Traffic and Parking, Department of Public Works, and Historical Commission.

In addition, the development team has engaged the community through individual abutter Zoom meetings and a large Zoom community meeting on September 29, 2020 that was attended by 82 people. The Project was also presented to the Porter Square Neighbors Association (PSNA) on October 15, 2020 and is scheduled to reappear at the PSNA on November 19, 2020. Understanding that many residents of the adjacent Russell House Apartments do not have access to computers and/or the internet, and in conjunction with the building's management, the development team displayed large renderings of the Project and comment cards in the building's lobby so that Russell House residents would have an opportunity to provide input.

Community outreach and various City department input resulted in the following changes to the proposal: (1) setting back further the first floor facing Walden Street, (2) relocating the main pedestrian entrance from Walden Street to Massachusetts Avenue, (3) widening Walden Street and the sidewalk adjacent to the Project, (4) programming the retail space to accommodate varied and community uses, and (4) developing partnerships with community stakeholders to access additional amenities for the Project's residents. An additional community meeting via Zoom is scheduled for November 16, 2020 to share design updates and elaborate on changes that were made to incorporate community feedback. The development team has maintained a website, www.2072massaveapts.com, that provides updated and detailed information on the proposal. The website includes copies of plans, FAQs (forthcoming), news and events, information on the development team, and contact information for the community to provide feedback and ask questions.

Development Team

The following development team has been formed to include industry experts ensuring a seamless and successful completion:

- **Developers:**

- *Capstone Communities LLC* (www.capstonecommunities.com), is a Newton, Massachusetts based real estate development firm experienced in structuring complex financing involving multiple federal and state subsidies. Jason Korb is the principal of Capstone Communities LLC where he has developed market rate, mixed income, and 100% affordable housing. Since founding Capstone in October 2010, Jason has successfully completed a total of \$60,000,000 of development transactions in Cambridge, Somerville, Arlington, Newton and Brockton Massachusetts. These include converting Brockton's first brick shoe factory into 25 mixed-income apartments, co-developing 20 100% affordable family apartments on a vacant lot in Cambridge's Port neighborhood. Additionally, Capstone and Hope are currently developing Frost Terrace, a 40 apartment, 100% affordable community in Porter Square which is estimated to be complete in Spring of 2021.

Prior to forming Capstone, Jason was the Vice President of Acquisitions at Beacon Communities LLC, a developer, owner, and manager of over 9,000 apartment homes in the Northeast. At Beacon, Jason was responsible for sourcing new acquisitions and overseeing

mixed income, affordable and market rate development and financing opportunities. In his seven years at Beacon, Jason was responsible for developing over 600 apartment homes totaling over \$100M. Prior to joining Beacon in 2004, Jason was a Housing Project Manager at the Fenway Community Development Corporation in Boston. Jason is a former Director of Caritas Communities and a former Vice-Chair of Preservation Massachusetts. Jason received an MS from the Massachusetts Institute of Technology's Center for Real Estate and a BA from the University of Michigan, Ann Arbor. Jason's MIT thesis, The Low Income Housing Tax Credit: HERA, ARRA, and Beyond has been cited by Harvard University's Joint Center for Housing Studies and the US Senate Budget Committee.

- *Hope Real Estate Enterprises LLC*, is led by Cambridge based attorney and Real Estate Developer Sean D. Hope who specializes in Zoning and municipal permitting/consulting. Sean was co-developer with Jason Korb of Port Landing - a 100% affordable housing development located in The Port/Kendall Square that opened at the end of 2016 providing 20 units of family friendly housing to the Port neighborhood where Sean's family first moved to from the island of Barbados. Additionally, Hope has partnered with Capstone again to develop Frost Terrace, a 40 apartment, 100% affordable community in Porter Square which is estimated to be complete in Spring of 2021. Sean has also represented developers and property owner on numerous construction projects in Cambridge including new construction, historic preservation and adaptive reuse projects. Prior to entering into private practice in 2008 Sean was an associate member of the Cambridge Board of Zoning Appeal voting on several keys applications including Print Shop Condominiums, an affordable home ownership project developed by CASCAP in 2007. Sean also serves as in-house counsel for Pentecostal Tabernacle in Cambridge and is former member of the Cambridge Historic Society.
- **Preconstruction Cost Estimator:** *Callahan Construction Managers* (www.callahan-inc.com): Established in 1954, Callahan Construction Managers is a full-service construction company experienced with a variety of building types and construction methods in Massachusetts and the Northeast. Callahan specializes in a wide range of preconstruction and construction management services, and including projects in multi-family residential, senior housing, hospitality, retail, corporate office, life sciences, educational, and other markets. Most recently, Callahan began the historic renovation, addition, and new construction project at Squirrelwood Apartments, an affordable housing community owned by Just-A-Start Corporation in Cambridge, MA. The Squirrelwood Apartments contain 88 units and the new construction building will be built to Passive House Standards.
- **Architect:** *Bruner/Cott Architects (Bruner/Cott)* (www.brunercott.com): Bruner/Cott is a mid-sized, full service architecture and planning firm, located in Boston, Massachusetts. Founded 45 years ago, Bruner/Cott is dedicated to enhancing the quality of life, economic vigor, and sense of community through thoughtful, sustainable design. As pioneers in transformative reuse, Bruner/Cott strives to recognize the character and value of an existing structure and understand that sustainable design extends beyond the boundaries of a single building. Whether new construction, transformative reuse, historic preservation, or a large-scale planning project, Bruner/Cott makes buildings that communicate with their surroundings, transforming place by creating architecture of enduring value.

Bruner/Cott is committed to its mission of achieving design excellence through collaboration, creativity, and critical thinking, crafting thoughtful design solutions that fulfill their clients'

aspirations, and enhancing the human experience while respecting the natural environment. The firm's work has been consistently recognized for design, winning awards for renovation, adaptive reuse, and new construction. In the Cambridge/Boston area, Bruner/Cott is working on Frost Terrace (currently under construction), and completed the Lunder Arts Center at Lesley University in Porter Square and The Viridian at 1282 Boylston Street.

- **Legal (General and Zoning):** *Nixon Peabody LLP* (www.nixonpeabody.com): Nixon Peabody is a full-service law firm with more than 600 lawyers nationwide and internationally. Our clients range from developers (for-profit and nonprofit), financing institutions and governmental entities to *Fortune* 100 companies. Nationally recognized in real estate, the firm handles highly complex development and financing transactions involving every class of assets, and has been at the forefront of financing, developing and preserving affordable housing for more than 45 years. In fact, with approximately 25 attorneys and paralegals possessing significant experience working with federal, state and local governmental agencies, NP has one of the largest affordable housing legal teams in the country.

The NP team also handles land use, zoning and permitting for a range of development projects, and is regularly brought into transactions to review site plans and perform zoning analyses. Through this work, the team has developed a comprehensive understanding of the local zoning and permitting processes and the multiple administrative steps that developers face during the course of development, which can include navigating the zoning approval process, obtaining Comprehensive Permit approvals, or establishing zoning overlay district areas and zoning map amendments.

Conclusion


The Project will bring high-quality housing to the Porter Square/North Cambridge neighborhood that will be affordable to a diverse array of low- and moderate-income households. With immediate proximity to rapid transit and essential community services, the Project can provide critically needed housing in an attractive, sustainable development. Designed to provide contemporary amenities with a focus on transit-oriented development and sustainability, the Project will be high-quality family housing in the heart of one of Cambridge's most vibrant neighborhoods.

We look forward to presenting this exciting Project to the Board at your earliest convenience.

Sincerely,

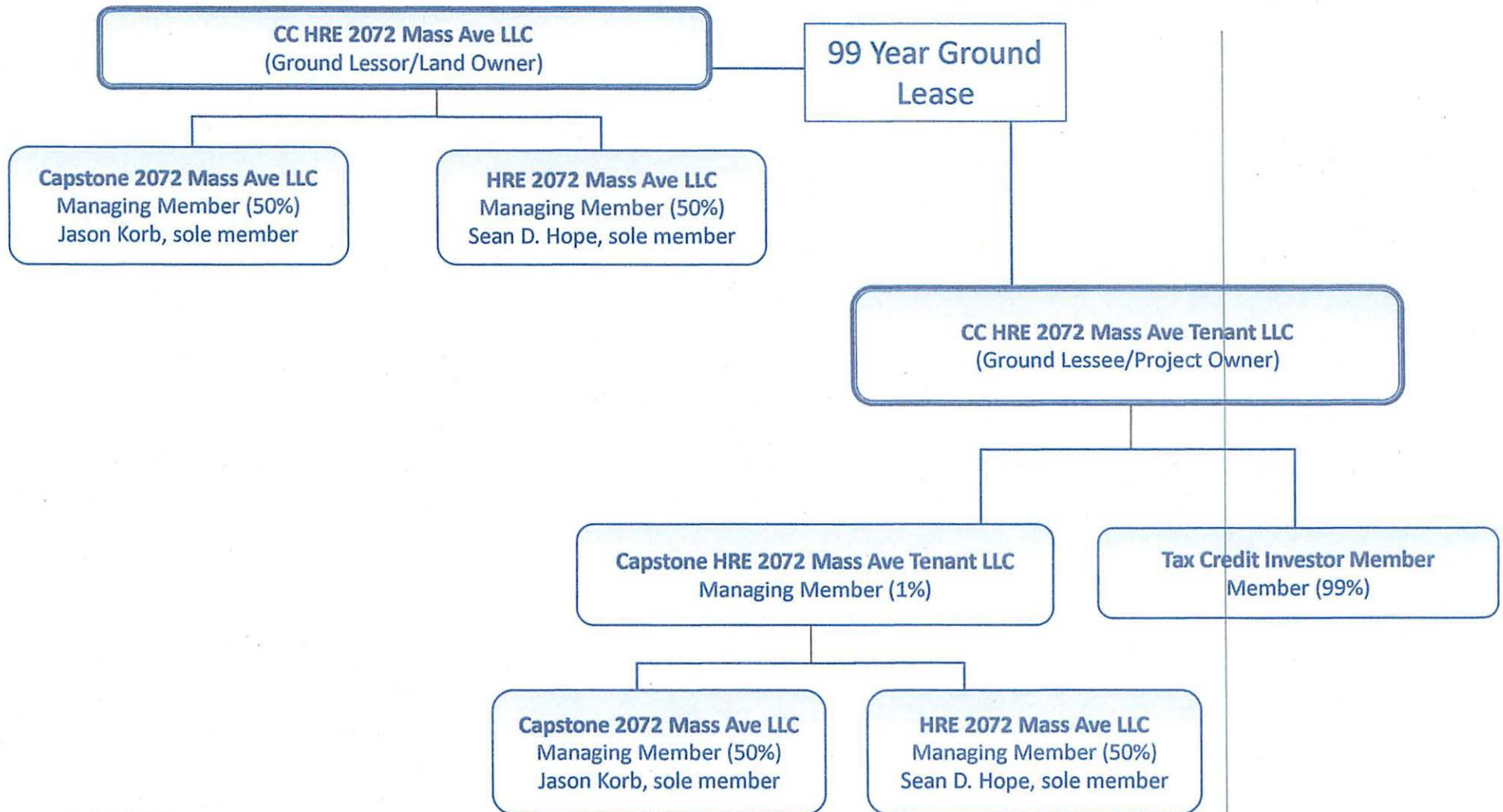


Jason Korb
Managing member of managing member



Sean D. Hope
Managing member of managing member

2072 Mass Ave Apartments Organizational Chart



11/10/2020

Pacheco, Maria

From: Gabriela Cipriano <gabi.cipriano@yahoo.com>
Sent: Monday, November 30, 2020 8:38 PM
To: Pacheco, Maria; Clerk
Subject: BZA-017326-2020

Hello Board of Zoning Appeals,

My name is Gabi Cipriano and I live at 79 Allston St in Cambridge. I am asking you to support the development of affordable housing units at 2072 Massachusetts Ave, case number BZA-017326-2020.

I support this development in Cambridge because it will make Cambridge a more equitable city to live in and is one small step in the right direction to relieving our current housing shortage. The development can benefit others in a mix of ways; it can ease the burden of rent on current residents who are currently paying the market rate or allow new people to move to Cambridge who could not otherwise have afforded it. I think it would be a shame to deny people these benefits over the concern of parking - a reason which is likely overstated given that the apartment is within blocks of transit so it is unlikely that every new resident comes with their own car.

Regardless of how many cars are added to the nearby streets from this development, our first priority should be to allow more people to live, work, and thrive in our community since it can offer many benefits to an individual and, reciprocally, new residents would benefit our community in many ways including, but of course not limited to, increased foot traffic, participation in civic institutions, and spending at our local small businesses.

Thank you,

Gabi

Pacheco, Maria

From: Hanna <hannaegerman@gmail.com>
Sent: Monday, November 30, 2020 10:44 PM
To: Pacheco, Maria
Subject: Writing in support of BZA-017326-2020

Hello,

I'm writing to voice my support for case #BZA-017326-2020 to support affordable housing. I live a couple blocks down from the proposed site, and would be delighted to have this building in my neighborhood. With housing so expensive in this area, it's important to me that everyone has somewhere they can afford to live, and this building will provide units toward that goal. I'm also glad to see that there are units for families with children and that this site will help families with kids remain in their community. Further, the designs look beautiful, and the designers took care to ensure that it is harmonious with the streetscape and human-scale. It certainly seems like it will be nicer to walk by than the empty parking lot currently on half of the site. I was also very excited to read about the environmental considerations--like the passive house standards, green roof, and solar panels--and how efficient this building will be. I'm always proud to live in a such a forward thinking city where we have buildings like this. In summary, I support BZA-017326-2020.

Thank you for your time,
Hanna

Hanna German
4 Beech St #3
Cambridge, MA

*CC HRE 2072 Mass Ave Tenant LLC
c/o Capstone Communities LLC
1087 Beacon Street, Suite 302
Newton, MA 02459*

December 3, 2020

Board of Zoning Appeal
City of Cambridge
831 Massachusetts Avenue
Cambridge, MA 02139

Re: Comprehensive Permit Application for 2072 Mass Ave Apartments
2072 Massachusetts Avenue, Cambridge, MA

Dear Board of Zoning Appeal Members:

Pursuant to 760 CMR 56.04, we are submitting revised materials for the 2072 Mass Ave Comprehensive Permit Application. While the design has changed, the unit mix has not. The project continues to include 71% two (2) and three (3) bedroom apartment homes for families as indicated in the following table.

1BR	2BR	3BR	Total
14	21	14	49
29%	42%	29%	71% Family

The project's design changes are enumerated below and incorporate significant feedback from the site's direct abutters, the neighborhood and the City of Cambridge's urban design, traffic, and community development staff.

1. Increased height of the "floating cube" along Mass Ave. from eight (8) stories to nine (9) stories to concentrate the building's height fronting along Mass Ave.;
2. Lowered height of rear residential volume from eight (8) stories to six (6) stories to step down the building facing the residential neighborhood; and,
3. Added active landscaped terrace on the 6th floor roof for resident use (~1,850 SF).

As indicated above, the most significant change is in the building's massing. After discussions and further review with the City of Cambridge's urban design staff and abutters on Walden Street, the design was modified to increase the height along Mass Ave. by one (1) story (from 8 to 9 stories) and to decrease the height of the rear residential anchor by two (2) stories (from 8 to 6 stories). This results in the building stepping down to the neighborhood and rising up at the major urban street.

A landscaped terrace is now included at the residential anchor's sixth floor roof, which will support a flexible outdoor space that may safely accommodate raised gardens, native plantings, and unique programming opportunities for residents. These changes are shown throughout the architectural drawings, but best seen in the rendering sheets A-305 to A-310, elevation sheets A-300 to A-304 and plan sheets A-107 to A-110.

The widening of Walden Street, which was included in the original BZA application, requires the proposal to provide an easement to the City of Cambridge over a portion of the 2072 Mass Ave land to be utilized as a public sidewalk. The metes and bounds of the easement cannot be finalized until the building is constructed. Therefore, if this application is approved, we respectfully request the following condition be added to any BZA Decision:

Prior to the issuance of the Project's final certificate of occupancy, the Applicant shall record a pedestrian easement along Walden Street from the Applicant and Owner in favor of the City of Cambridge, the location of which is generally as indicated on C-101 Proposed Easement Plan dated November 10, 2020 by Nitsch Engineering. The easement shall be approved by the Department of Public Works staff, which approval shall not be unreasonably withheld, conditioned or delayed, and shall not require additional approvals by this Board.

The following plans and materials dated December 3, 2020 (unless otherwise noted) reflecting these changes are included herein. These plans and materials replace (or supplement where applicable) the plans and materials submitted on November 10, 2020.

1. Plans

Cover Page, G-100, G-101, EC (10/5/20), C-100 (11/9/20), C-101 (11/9/20), A-100, A-101, A-102, A-107, A-108, A-109, A-110, A-300, A-301, A-302, A-303, A-304, A-305, A-306, A-307, A-308, A-309, A-310, A-400, A-401, A-402

2. Dimensional Information dated November 24, 2020

3. List of Requested Exemptions/Waivers from the Applicable City of Cambridge Ordinances and Regulations

4. Shadow Studies

5. Recorded Notice of Ground Lease dated November 9, 2020 (recorded November 16, 2020)

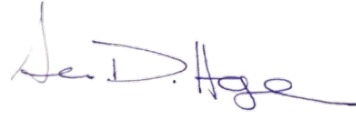
The community process has been robust and responsive to neighborhood feedback and concerns. The development team has met with the following direct abutters multiple times: the Cambridge Housing Authority (2050 Mass Ave/Russell Apartments), The Davis Companies (2067 Mass Ave/Henderson Carriage), and the 5 Walden Street Condominium. There have also been two large virtual community meetings, two virtual meetings with the Porter Square Neighbors Association, an upcoming meeting with the North Cambridge Stabilization Committee, a virtual meeting with neighbors on Creighton Street, and multiple email and phone communications with neighborhood residents. The project also has an active website (www.2072massaveapts.com) that includes all current materials, FAQ's and a comment forum for residents to comment on the project. A copy of the website FAQ's and comment forum are included herein for your reference. Finally, the development team has engaged with the residents of 2050 Mass Ave (Russell Apartments) by working with CHA management to place presentation boards (the original proposal and then later the revised proposal) in the lobby. Recognizing that many Russell residents are elderly, COVID-19 is a major concern, and do not have internet access, the development team placed in the lobby comment cards and copies of a memo with color renderings attached so that residents could view the proposal in the safety of their apartment home.

We believe the changes outlined in these supplemental materials are extremely positive and respond to feedback from various stakeholders. We look forward to presenting these plans to you at our December 10th hearing. Please contact Sean Hope at (617) 953-8369 if you have any comments.

Sincerely,

A handwritten signature in blue ink, appearing to read 'J. Korb', with a stylized flourish at the end.

Jason Korb
managing member of managing member

A handwritten signature in blue ink, appearing to read 'Sean D. Hope', with a long horizontal flourish extending to the right.

Sean D. Hope
managing member of managing member

Enclosures

Cc: Louis A. DePasquale, City Manager, City of Cambridge
City of Cambridge Community Development Department



2072 MASS AVE
2072 MASSACHUSETTS AVENUE
CAMBRIDGE, MA 02140

COMPREHENSIVE PERMIT REVISIONS

DECEMBER 3, 2020

CC HRE 2072 MASS AVE TENANT LLC
OWNER

C/O CAPSTONE COMMUNITIES LLC
1087 BEACON ST, SUITE 302
NEWTON CENTRE, MA 02459

C/O HOPE REAL ESTATE ENTERPRISES LLC
907 MASSACHUSETTS AVE, SUITE 300
CAMBRIDGE, MA 02139

BRUNER/COTT ARCHITECTS
ARCHITECT

225 FRIEND ST, SUITE 701
BOSTON, MA 02114

NITSCH ENGINEERING INC.
CIVIL ENGINEER

2 CENTER PLAZA, #430
BOSTON, MA 02108

PETERSEN ENGINEERING INC.
MEP/FP ENGINEER

127 PARROTT AVE
PORTSMOUTH, NH 03801

L.A. FUESS PARTNERS
STRUCTURAL ENGINEER

211 CONGRESS ST, SUITE 810
BOSTON, MA 02110

LEMON BROOKE LLC
LANDSCAPE ARCHITECT

56 D WINTHROP ST
CONCORD, MA 01742

NEW ECOLOGY INC
SUSTAINABILITY CONSULTANT

15 COURT SQ, SUITE 420
BOSTON, MA 02108

CODE RED CONSULTANTS LLC
CODE CONSULTANT

154 TURNPIKE RD, SUITE 200
SOUTHBOROUGH, MA 01772



Rev	Date	Remarks

Date	DECEMBER 3, 2020
Scale	
Project Number	20.001
Drawn By	Author

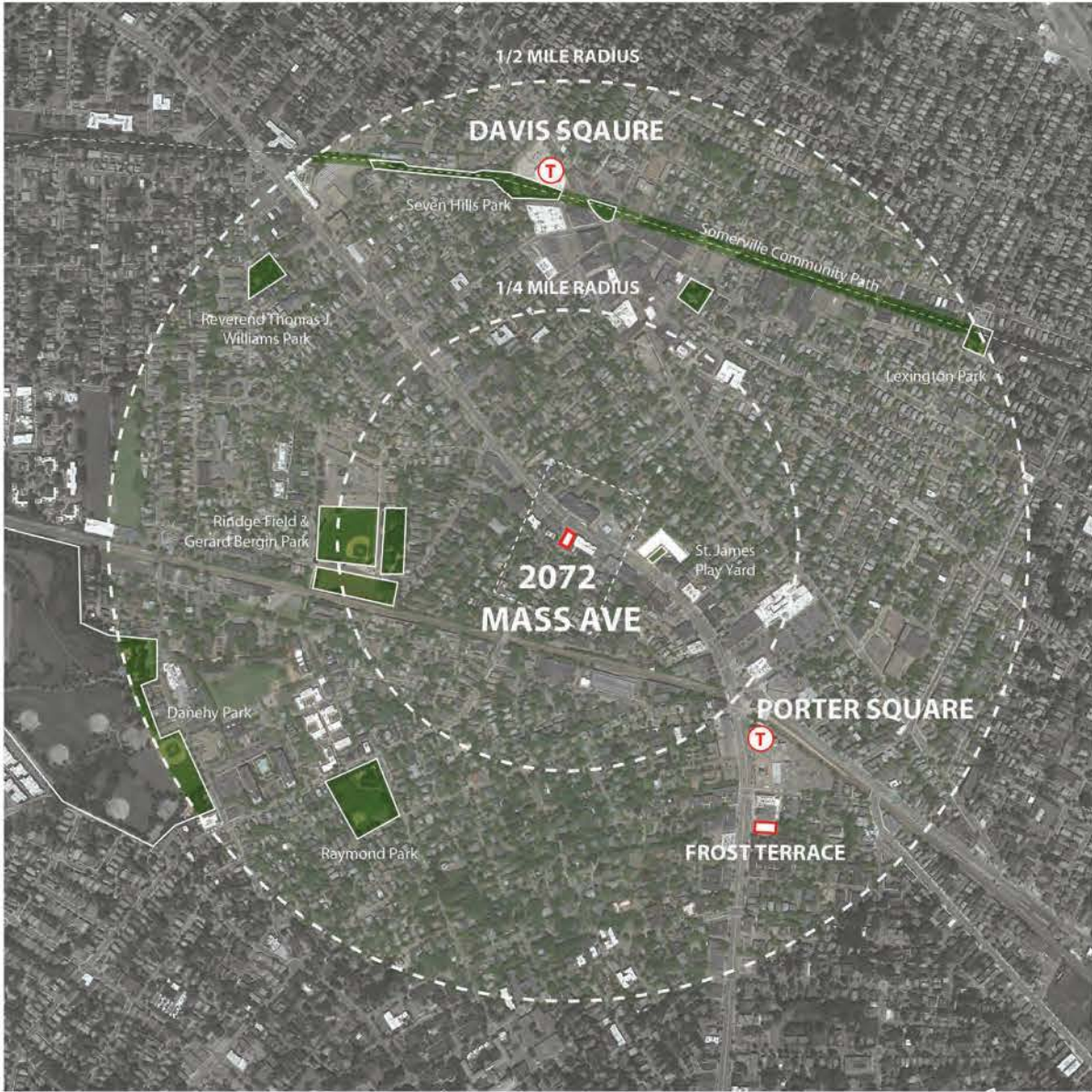
2072 MASS AVE

2072 Massachusetts
Avenue, Cambridge, MA
02140

COMPREHENSIVE PERMIT REVISIONS
NOT FOR CONSTRUCTION



NEIGHBORHOOD MAP/
SITE PLAN



NEIGHBORHOOD MAP



SITE PLAN

IF THIS SHEET IS NOT 24" x 36" IT HAS BEEN REDUCED OR ENLARGED.

12/4/2020 2:38:11 AM

DRAWING LIST	
GENERAL	
G-100	NEIGHBORHOOD MAP/ SITE PLAN
G-101	DRAWING LIST, UNIT MATRIX, ZONING SUMMARY
EC	EXISTING CONDITIONS PLAN OF LAND IN CAMBRIDGE, MA
CIVIL	
C-100	CIVIL SITE UTILITY AND LAYOUT PLAN
C-101	PROPOSED EASEMENT PLAN
ARCHITECTURAL	
A-100	BASEMENT FLOOR PLAN
A-101	FIRST FLOOR PLAN
A-102	TYPICAL FLOOR PLAN (FLRS 2-6)
A-107	FLOOR PLAN - FLR 7 & TERRACE
A-108	FLOOR PLAN - FLR 8
A-109	FLOOR PLAN - FLR 9
A-110	ROOF PLAN
A-300	EXTERIOR ELEVATIONS
A-301	EXTERIOR ELEVATIONS
A-302	EXTERIOR ELEVATIONS
A-303	EXTERIOR ELEVATIONS
A-304	EXTERIOR ELEVATIONS
A-305	PERSPECTIVES
A-306	PERSPECTIVES
A-307	PERSPECTIVES
A-308	PERSPECTIVES
A-309	PERSPECTIVES
A-310	PERSPECTIVES
A-400	STREET SECTIONS
A-401	BUILDING SECTION
A-402	BUILDING SECTION

2072 MASSACHUSETTS AVENUE, CAMBRIDGE, MA
BUILDING TABULATIONS
PREPARED BY BRUNER/COTT ARCHITECTS

SITE AND BUILDING

BUILDING GROSS FLOOR AREA (GFA) (PER ZONING)

BASEMENT	Excluded
FIRST FLOOR	4,580
SECOND FLOOR	7,545
THIRD FLOOR	7,545
FOURTH FLOOR	7,545
FIFTH FLOOR	7,545
SIXTH FLOOR	7,545
SEVENTH FLOOR	7,386
EIGHTH FLOOR	5,530
NINTH FLOOR	5,530
TOTAL GFA	60,751

BUILDING GROSS SQUARE FOOTAGE (GSF)

BASEMENT	5,465
FIRST FLOOR	4,580
SECOND FLOOR	7,545
THIRD FLOOR	7,545
FOURTH FLOOR	7,545
FIFTH FLOOR	7,545
SIXTH FLOOR	7,545
SEVENTH FLOOR	7,386
EIGHTH FLOOR	5,530
NINTH FLOOR	5,530
TOTAL GSF	66,216

TOTAL PARCEL SIZE (SF)	8,515
FAR	7.13
PARKING RATIO (EXCLUDES DROP-OFF/PICK-UP)	6%
LONG TERM BIKE PARKING RATIO	104%
OPEN SPACE	22%
CONSTRUCTION TYPE *	Type I(A construction (2-story podium, basement and first floor), Type II(A construction (above podium, second to ninth floors) * Alternative option of seeking a variance to permit the use of the new construction type classifications found in the 2021 IBC and classifying the tower as Type IV-B

PARKING

	HC	TOTAL *	HC UNIT : SPACE RATIO
COVERED PARKING	3	3	1:1

* Commercial parking is waived under Article 6.36 based on actual quantity required being below four (4) required spots

* Two (2) pick-up/drop-off spaces will also be provided

	LONG-TERM	TANDEM	SHORT-TERM	TOTAL
BIKE PARKING	48	3	0*	51

* Project team is seeking alternative public contribution option of satisfying short-term bike parking requirements per Article 6.104.2 (b)

UNIT MIX

	STUDIO	1 BR	2 BR	3 BR	TOTAL
SECOND FLOOR	0	2	3	2	7
THIRD FLOOR	0	2	3	2	7
FOURTH FLOOR	0	2	3	2	7
FIFTH FLOOR	0	2	3	2	7
SIXTH FLOOR	0	2	3	2	7
SEVENTH FLOOR	0	2	2	1	5
EIGHTH FLOOR	0	2	2	1	5
NINTH FLOOR	0	0	2	2	4
TOTAL	0	14	21	14	49

% PER UNIT TYPE	0%	29%	42%	29%
% 2 AND 3 BR COMBINED	71%			

	STUDIO	1 BR	2 BR	3 BR	TOTAL
TOTAL NUMBER OF BEDROOM	0	14	42	42	98

UNIT NUMBER	UNIT TYPE	UNIT AREA (SF)	UNIT NUMBER	UNIT TYPE	UNIT AREA (SF)
UNIT X01 *	1 BR	569	UNIT 701/801	1 BR	569
UNIT X02 *	1 BR	681	UNIT 702/802	1 BR	681
UNIT X03 *	3 BR	1144	UNIT 703/803	3 BR	1144
UNIT X04 *	2 BR	800	UNIT 704/804	2 BR	800
UNIT X05 *	2 BR	850	UNIT 705/805	2 BR	823
UNIT X06 *	3 BR	1031	UNIT 901	3 BR	1130
UNIT X07 *	2 BR	823	UNIT 902	3 BR	1249
* For second through sixth floors			UNIT 903	2 BR	823

UNIT TYPE	AVERAGE AREA (SF)
STUDIO	N/A
1 BR	625
2 BR	821
3 BR	1110

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Rev	Date	Remarks

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Scale	
Project Number	20.001
Drawn By	Author

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DRAWING LIST, UNIT
MATRIX, ZONING
SUMMARY

G-101

2072
MASSACHUSETTS
AVENUE

Cambridge, Massachusetts

PREPARED FOR:

CC HRE
2072
MASS AVE
LLC

1155 Walnut Street #31
Newton Highlands, Massachusetts 02461

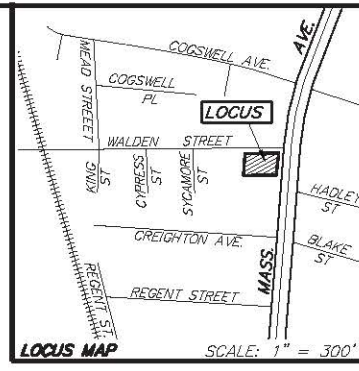
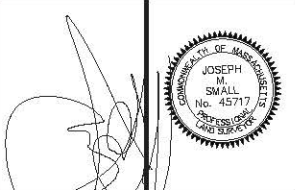
HANCOCK
ASSOCIATES

Civil Engineers

Land Surveyors

Wetland Scientists

185 CENTRE STREET, DANVERS, MA 01923
VOICE (978) 777-3050, FAX (978) 774-7816
WWW.HANCOCKASSOCIATES.COM

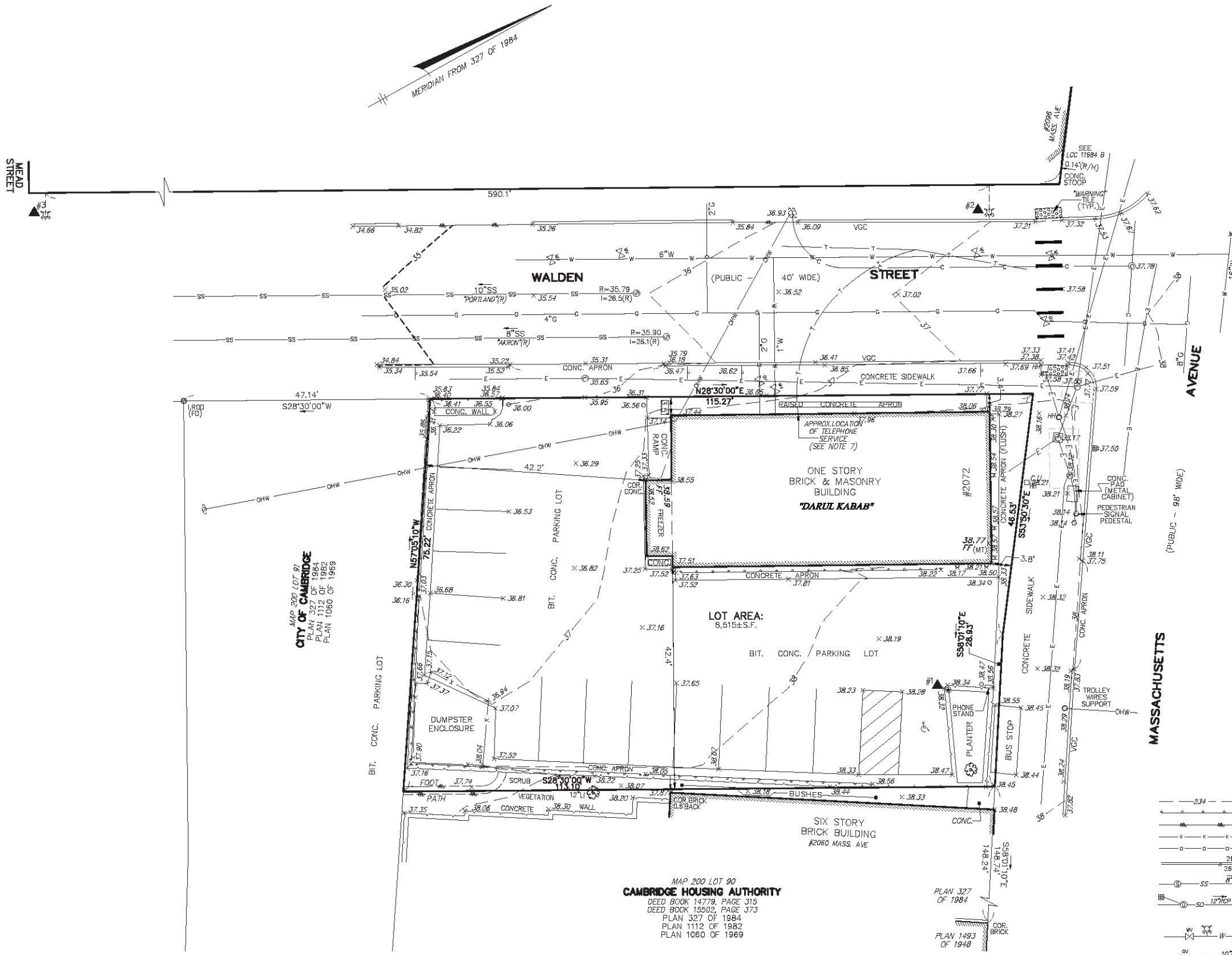


ASSESSORS: MAP 202, LOT 22

REFERENCES: DEED BOOK 70018, PAGE 247
PLAN 1060 OF 1969
PLAN 1112 OF 1982
PLAN 327 OF 1984
LCC 116B4 B

RECORD OWNER: 2072 MASS AVE LLC

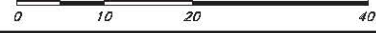
- NOTES:**
- 1) ELEVATIONS SHOWN HEREON REFER TO CAMBRIDGE CITY BASE.
 - 2) PROJECT SOURCE BENCHMARK IS A CHISELED "X" IN HYDRANT BONNET BOLT SHOWN HEREON AS BENCHMARK #3 AS SHOWN ON SKETCH PROVIDED BY CAMBRIDGE DEPARTMENT OF PUBLIC WORKS.
 - 3) UNDERGROUND UTILITIES SHOWN HEREON ARE COMPILED FROM FIELD LOCATIONS OF STRUCTURES AND FROM AVAILABLE RECORD INFORMATION ON FILE AT THE CAMBRIDGE D.P.W., MASS HIGHWAY DEPT. AND UTILITY COMPANIES. OTHER UNDERGROUND UTILITIES MAY EXIST. IT SHALL BE THE RESPONSIBILITY OF THE DESIGN ENGINEER AND THE CONTRACTOR TO VERIFY THE LOCATION, SIZE & ELEVATION OF ALL UTILITIES WITHIN THE AREA OF PROPOSED WORK AND TO CONTACT "DIG-SAFE" AT 811 AT LEAST 72 HOURS PRIOR TO ANY EXCAVATION, DEMOLITION OR CONSTRUCTION.
 - 4) BUILDING OFFSETS SHOWN HEREON ARE TO CORNER BRICK UNLESS OTHERWISE NOTED.
 - 5) SOME SNOW AND ICE WAS ON THE PREMISES AT THE TIME OF THE SURVEY; SOME SITE DETAIL MAY HAVE BEEN OBSCURED.



LEGEND

- | | | | |
|-----------|---|----------|---|
| — 234 — | SURFACE CONTOUR | 832.6 | SPOT ELEVATION |
| — M — | METAL GUARDRAIL | ☆ | LIGHT POLE |
| — X — | EDGE OF PAVEMENT | 205.24 | MONITORING WELL WITH TOP METAL CASING EL. |
| — X — | CHAIN LINK FENCE | ⊗ | D GRATE CATCHBASIN |
| — W — | WOOD FENCE | ⊗ | PROMINENT DECIDUOUS TREE |
| — 28.75 — | CURB WITH TOP AND BOTTOM CURB ELEVATION | 12" L | 12" LINDEN TREE |
| — S — | SEWERLINE WITH PIPE SIZE, MATERIAL & FLOW DIRECTION | ⊗ | BOLLARD |
| — D — | DRAINLINE WITH PIPE SIZE, MATERIAL & FLOW DIRECTION, CATCHBASIN, MANHOLE & ROUND CATCHBASIN | ⊗ | SIGN |
| — W — | WATER MANHOLE, WATER MAIN WITH SIZE, TEE, GATE VALVE & FIRE HYDRANT | ⊗ | MANHOLE (UNKNOWN UTILITY) |
| — G — | GAS MAIN WITH SIZE & GATE VALVE | RCP | REINFORCED CONCRETE PIPE |
| — U — | UTILITY POLE WITH DESIGNATION OVERHEAD WIRES AND GUY POLE | CI | CAST IRON |
| — E — | ELECTRIC MANHOLE & UNDERGROUND ELECTRIC LINES | CMP | CORRUGATED METAL PIPE |
| — C — | CABLE TV MANHOLE & UNDERGROUND CABLE LINES | ASB. | ASBESTOS PIPE |
| — T — | TELEPHONE MANHOLE & UNDERGROUND TELEPHONE LINES | VIC | VITRIFIED CLAY |
| — R — | RETAINING WALL WITH TOP AND BOTTOM ELEVATIONS | PVC | POLYVINYL CHLORIDE |
| | | (R) | RECORD |
| | | (C) | CALCULATED |
| | | (R/H) | RECORD AND HELD |
| | | (M) | FIELD MEASURED |
| | | DH | DRILL HOLE |
| | | IPIPE | IRON PIPE |
| | | LROD | IRON ROD |
| | | VGC | VERTICAL GRANITE CURB |
| | | 38.77 | ENTRANCE ELEV. (METAL THRESHOLD) |
| | | FF(MT) X | |

SCALE: 1" = 10'



ELEVATION BENCH MARKS		
DATUM: CAMBRIDGE CITY BASE		
NO.	DESCRIPTION	ELEV.
1.	FIRE HYDRANT; BOLT OVER MAIN OUTLET	38.98
2.	TOP CONC. WALL; TOP DRILL HOLE (SET)	38.95
3.	HYDRANT; CHISELED "X" IN BONNET BOLT	41.28

MAP 202, LOT 90
CAMBRIDGE HOUSING AUTHORITY
DEED BOOK 14779, PAGE 315
DEED BOOK 15502, PAGE 373
PLAN 327 OF 1984
PLAN 1112 OF 1982
PLAN 1060 OF 1969

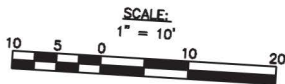
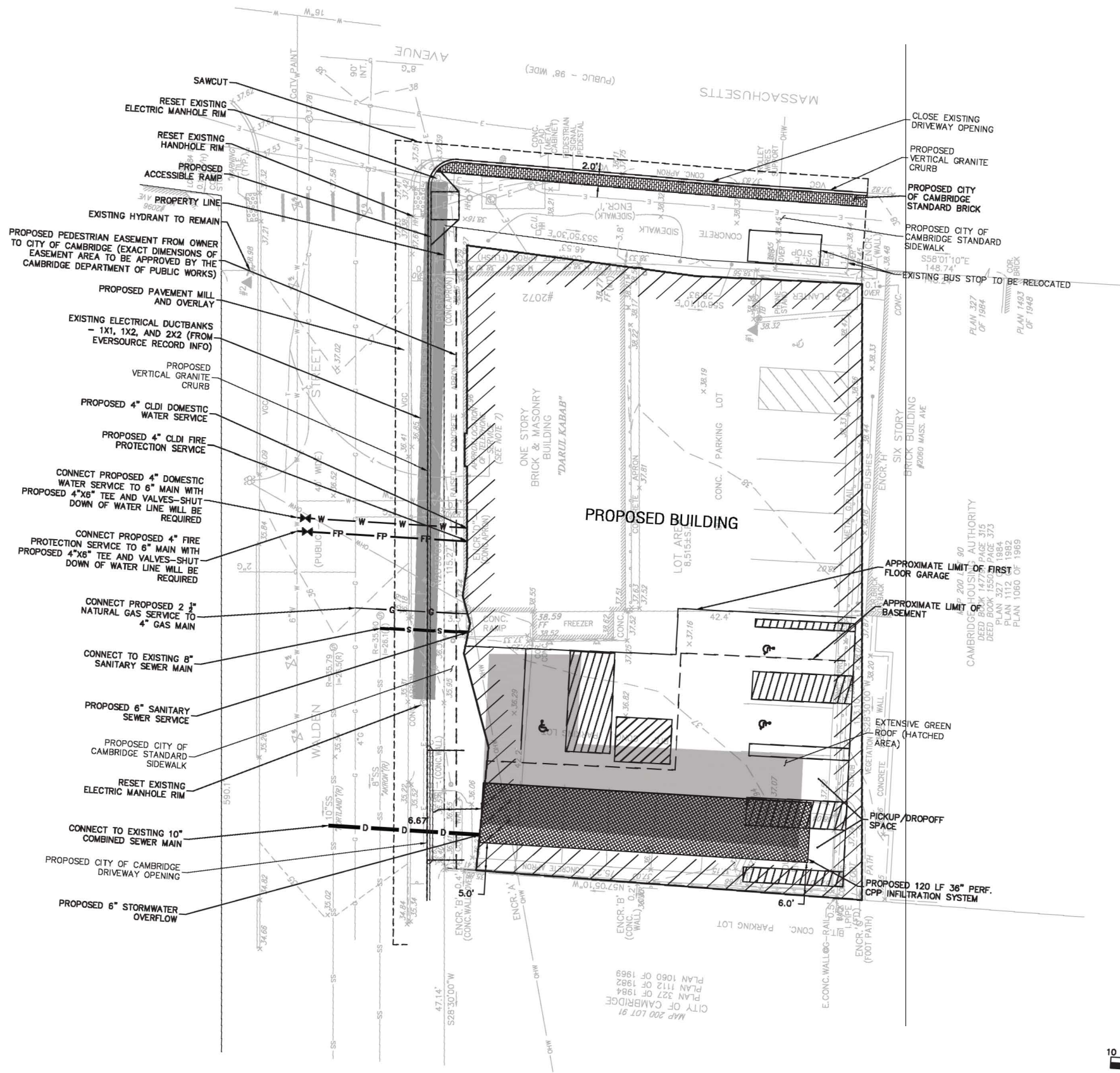
**EXISTING CONDITIONS
PLAN OF LAND
IN
CAMBRIDGE, MA**

PLT DATE: Oct 20, 2020 10:20 am
PLT: P:\CM 30 Projects\21130 - Carbons Communities - Cambr

DWG: 21130sv.dwg
LAYOUT: EC

SHEET: 1 OF 1

PROJECT NO.: 21130



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ARCHITECTS

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Boston, MA 02114
617.492.8400
www.brunercott.com

2 Center Plaza, Suite 430
Nitsch Engineering
www.nitscheng.com
Boston, MA 02108
T: (617) 338-0063
F: (617) 338-6472
Remainder of sheet
- Land Surveying
- Transportation Engineering
- Structural Engineering
- Green Infrastructure
- Planning

Date: NOVEMBER 10, 2020
Scale: 1"=10'
Project Number: NITSCH #14047
Drawn By: MLC

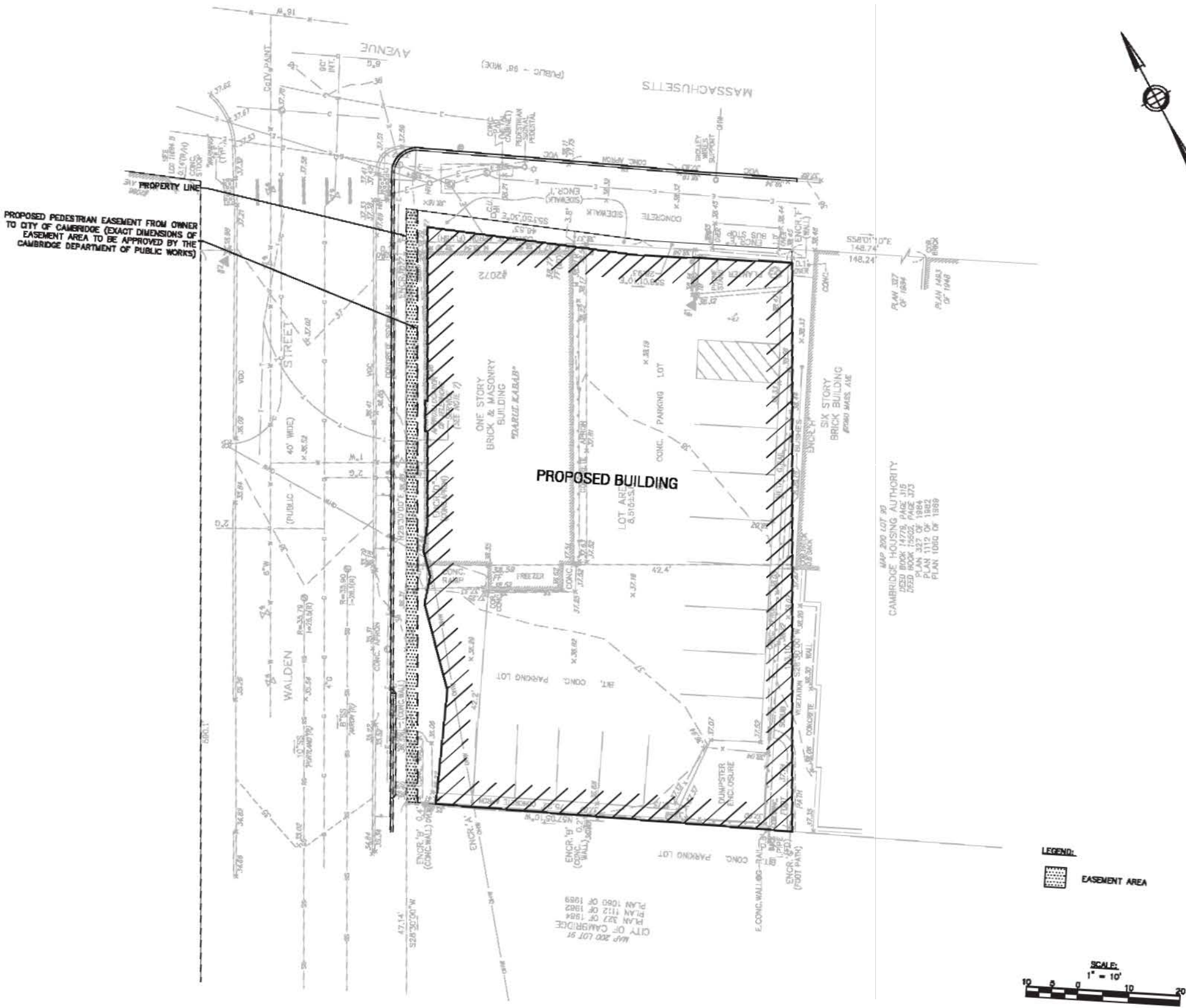
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NITSCH PROJECT #14047
MICHELLE L. CALLAHAN
CIVIL ENGINEER
REGISTERED
November 9, 2020
CITY OF CAMBRIDGE
FOR CONSTRUCTION
**CIVIL SITE UTILITY
AND LAYOUT PLAN**

C-100



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ARCHITECTS
225 Friend St., Suite 701
Boston, MA 02114
617.492.8400
www.brunercott.com

2 Center Plaza, Suite 430
Nitsch Engineering
www.nitschengineering.com
Boston, MA 02108
E: (617) 338-0083
P: (617) 338-6472
Rev: 10/20/20
Remarks:
• Land Surveying
• Transportation Engineering
• Structural Engineering
• Stormwater Management
• Planning

Date: NOVEMBER 10, 2020
Scale: 1" = 10'
Project Number: NITSCH #14047
Drawn By: MLC

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NITSCH PROJECT #14047
COMMONWEALTH OF MASSACHUSETTS
MICHELLE L. ELLAHHAN
REGISTERED PROFESSIONAL ENGINEER
November 5, 2020
FOR RECORD ONLY - NOT
PROPOSED
EASEMENT PLAN

C-101

Rev	Date	Remarks

Date	DECEMBER 3, 2020
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Project Number	20.001
Drawn By	Author

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BASEMENT FLOOR PLAN



1 FLOOR PLAN - BASEMENT
SCALE: 1/8" = 1'-0"

FLOOR AREA PLAN

BIKE RM	2130 SF
CORR	524 SF
ELEC RM	434 SF
ELEV MACHINE RM	58 SF
FIRE PUMP RM	211 SF
FUEL STORAGE FM	156 SF
STAIR 01	232 SF
STAIR 02	185 SF
STORAGE	214 SF
TRANSFORMER RM	738 SF
WATER ENTRY RM	122 SF



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12/4/2020 2:37:31 AM



1 FLOOR PLAN - FIRST FLOOR
SCALE: 1/8" = 1'-0"

FLOOR AREA PLAN

AMENITY SPACE	503 SF
LOBBY	200 SF
RETAIL SPACE	51 SF
FCC	924 SF
HC	254 SF
OFFICE	995 SF
STAIR 01	315 SF
STAIR 02	259 SF
STORAGE	99 SF
TRASH RM	433 SF
VESTIBULE	174 SF



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FIRST FLOOR PLAN

A-101

Rev	Date	Remarks

Date	DECEMBER 3, 2020
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Project Number	20.001
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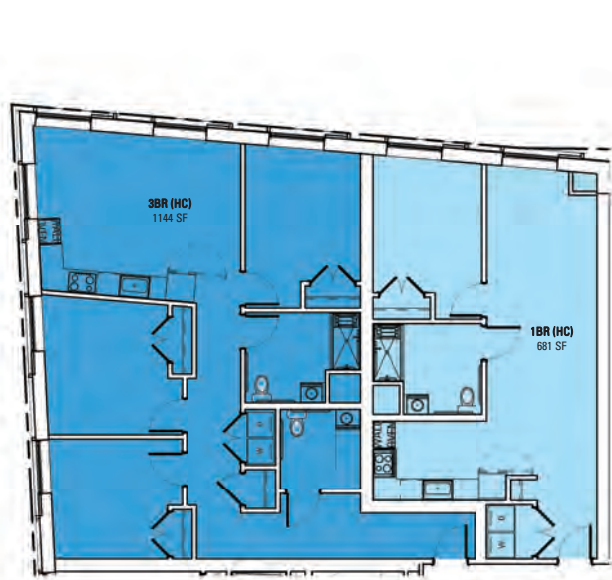
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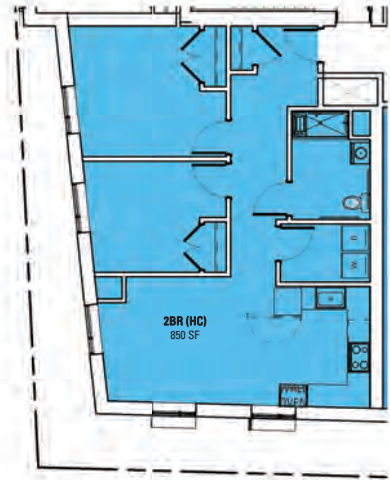


TYPICAL FLOOR PLAN
(FLRS 2-6)



*NOTE: 3 BR GROUP 2 UNIT LOCATED ON FLR 3
1 BR GROUP 2 UNIT LOCATED ON FLR 8

2 GROUP 2 UNITS - 1 BR & 3 BR
SCALE: 1/8" = 1'-0"



*NOTE: 2 BR GROUP 2 UNIT LOCATED ON FLR 4

3 GROUP 2 UNITS - 2 BR
SCALE: 1/8" = 1'-0"



1 FLOOR PLAN - TYP FLOORS (FLRS 2-6)
SCALE: 1/8" = 1'-0"

FLOOR AREA PLAN

1BR	1250 SF
2BR	1623 SF
3BR	1144 SF
CORR	386 SF
ELEC. CLOSET	15 SF
STAIR 01	186 SF
STAIR 02	197 SF



Rev	Date	Remarks

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FLOOR PLAN - FLR 7 &
TERRACE

A-107



1 FLOOR PLAN - FLR 7
SCALE: 1/8" = 1'-0"

FLOOR AREA PLAN

1BR	1250 SF
2BR	1623 SF
3BR	1144 SF
CORR	386 SF
ELEC. CLOSET	15 SF
STAIR 01	186 SF
STAIR 02	197 SF



Rev	Date	Remarks

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FLOOR PLAN - FLR 8

A-108



1 FLOOR PLAN - FLR 8
SCALE: 1/8" = 1'-0"

FLOOR AREA PLAN

1BR	1250 SF
2BR	1623 SF
3BR	1144 SF
CORR	386 SF
ELEC. CLOSET	15 SF
STAIR 01	186 SF
STAIR 02	197 SF



Rev	Date	Remarks

Date	DECEMBER 3, 2020
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FLOOR PLAN - FLR 9



1 FLOOR PLAN - FLR 9
SCALE: 1/8" = 1'-0"

FLOOR AREA PLAN

2BR	1623 SF
3BR	2379 SF
CORR	386 SF
ELEC. CLOSET	15 SF
STAIR 01	186 SF
STAIR 02	197 SF
STORAGE	48 SF



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1 ROOF PLAN
SCALE: 1/8" = 1'-0"

FLOOR AREA PLAN

MECH RM	363 SF
STAIR 02	132 SF



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ROOF PLAN



1 MASSACHUSETTS AVENUE ELEVATION - NORTH
SCALE: 1/16" = 1'-0"



2 WALDEN STREET ELEVATION - WEST
SCALE: 1/16" = 1'-0"

Rev	Date	Remarks

Date	DECEMBER 3, 2020
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Project Number	20.001
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EXTERIOR ELEVATIONS

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EXTERIOR ELEVATIONS

A-301

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EXTERIOR ELEVATIONS



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1 BUILDING ELEVATION - SOUTH
SCALE: 1/8" = 1'-0"

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EXTERIOR ELEVATIONS

A-303

MECH EQUIPMENT, BEYOND
MTL COPING
MTL PANEL W/ EXPOSED SEAMS

ROOF TERRACE, REF.
LANDSCAPE PLAN
MTL COPING
HALF ROUND FIBER
CEMENT SHINGLES, PTD
PERFORATED MTL SUN
SHADES, BEYOND

DASHED LINE INDICATES
EXTENTS OF ADJACENT 2050
MASS AVE BUILDING

CONTINUOUS MTL FLASHING AT
WINDOW HEAD & SILL, TYP.

HIGH PERFORMANCE UPVC
WINDOW
FIBER CEMENT
CLAPBOARD SIDING, PTD
FIBER CEMENT SHAKE
SHINGLES, PTD

GREEN ROOF, BEYOND

T.O.C. ROOF
137'-6"

T.O.C. NINTH FLOOR
127'-0"

T.O.C. EIGHTH FLOOR
116'-6"

T.O.C. SEVENTH FLOOR
106'-0"

T.O.C. SIXTH FLOOR
95'-6"

T.O.C. FIFTH FLOOR
85'-0"

T.O.C. FOURTH FLOOR
74'-6"

T.O.C. THIRD FLOOR
64'-0"

T.O.C. SECOND FLOOR
53'-6"

T.O.C. LOBBY/RETAIL
38'-6"

MEAN GRADE
37'-4 1/2"
(EL. 37.38')

T.O.C. BASEMENT
26'-6"

1 BUILDING ELEVATION - EAST
SCALE: 1/8" = 1'-0"



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EXTERIOR ELEVATIONS



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PERSPECTIVES

RENDERING - VIEW AT MASS AVE TOWARDS WALDEN ST LOOKING NORTH-WEST



Rev	Date	Remarks

Date	DECEMBER 3, 2020
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PERSPECTIVES

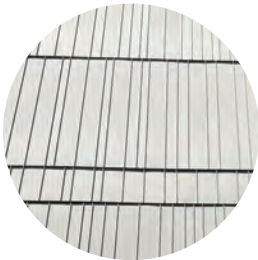
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RENDERING - VIEW OF MASS AVE & WALDEN ST INTERSECTION LOOKING SOUTH-EAST



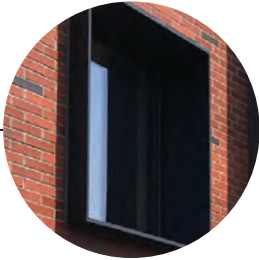
RENDERING - VIEW OF MASS AVE LOOKING SOUTH



METAL PANEL WITH EXPOSED SEAMS



LONG MASONRY AT STREET LEVEL



HIGH PERFORMANCE UPVC WINDOW WITH METAL FRAME



VERTICAL INFILL WOOD TEXTURE COMPOSITE PANEL

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PERSPECTIVES

A-307



RENDERING - VIEW OF WALDEN ST LOOKING WEST

Rev	Date	Remarks

Date	DECEMBER 3, 2020
Scale	
Project Number	20.001
Drawn By	Author

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PERSPECTIVES

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Boston, MA 02114
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PERSPECTIVES

A-309



RENDERING - VIEW OF WALDEN ST TOWARDS MASS AVE LOOKING NORTH



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RENDERING - VIEW ALONG MASS AVE LOOKING SOUTH-EAST

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PERSPECTIVES

A-310

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Drawn By	EP

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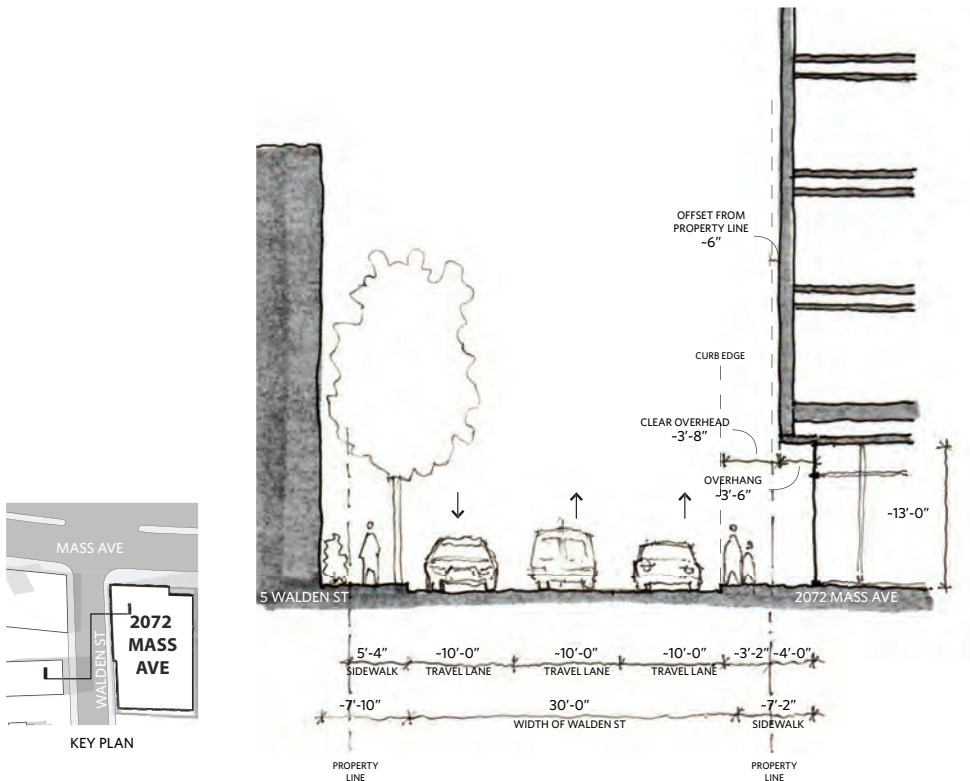
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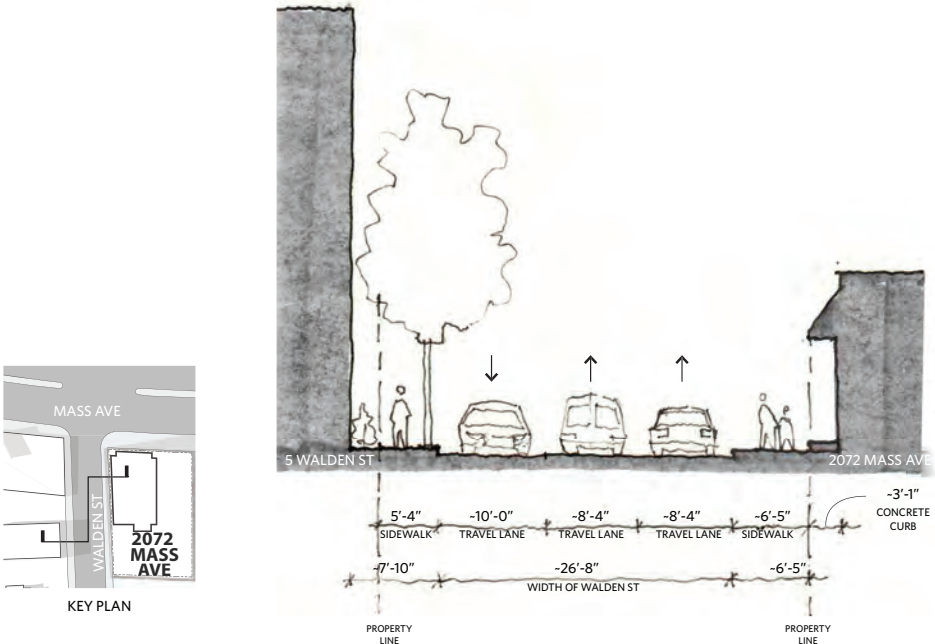


STREET SECTIONS

A-400



STREET SECTION - PROPOSED DEVELOPMENT WITH WIDENED WIDTH OF WALDEN ST
NTS



STREET SECTION - EXISTING CONDITIONS
NTS

Rev	Date	Remarks

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BUILDING SECTION

A-401



1 BUILDING SECTION - NORTH-SOUTH
SCALE: 1/8" = 1'-0"



Date	DECEMBER 3, 2020
Scale	1/8" = 1'-0"
Project Number	20.001
Drawn By	EP

2072 Massachusetts
Avenue, Cambridge, MA
02140

A-402



DIMENSIONAL INFORMATION

Project Address: 2072 Massachusetts Avenue

		EXISTING CONDITIONS	ORDINANCE REQUIREMENTS		REQUESTED CONDITIONS
			BA-2 / BUSINESS A-2	RESIDENCE B	
Lot Area (SF)		8,515 SF	No minimum	5,000 SF (min.)	8,515 SF
Lot Width (Ft)		~75.46' @ Massachusetts Avenue	No minimum	50' (min.)	~75.46' @ Massachusetts Avenue
Total Gross Floor Area (GFA)(SF)		1,860 SF	15,755 SF (max.)(g)	608 SF (max.)(g)	60,751 SF (h)
Residential Base		0	13,129 SF (max.)(g)	507 SF (max.)(g)	56,171 SF
Non-Residential Base		1,860 SF	0	0	4,580 SF (d)
Inclusionary Housing Bonus w/20% affordable		N/A	2,626 SF (max.)(g)	101 SF (max.)(g)	N/A
Ratio of Floor Area to Lot Area		Baseline: MAOD:	1.0 / 1.75 (max.)	0.5/0.35 for portions exceeding 5,000 SF (max.)	7.13
Residential Base		Baseline: MAOD:	1.75 for mixed-use / 1.0 for all other uses (max.)	1.75 for mixed-use / 1.0 for all other uses (max.)	7.13
Non-Residential Base		Baseline: MAOD:	1.75	0.5/0.35 for portions exceeding 5,000 SF	6.60
Inclusionary Housing Bonus - %		Baseline/MAOD:	1.75	1.75	6.60
			1.0	N/A	0.54
			N/A	N/A	0.54
			20% bonus = 2,626 SF (GFA)(g)	20% bonus = 101 SF (GFA)(g)	N/A
Total Dwelling Units		Baseline/MAOD:	16 (max.)	0	49
Base Units			600 SF / D.U. = 12	2,500 SF / D.U. = 0	49
Inclusionary Bonus units - 20%		N/A	2	0	N/A
Base Lot Area / Unit (SF)			625 SF / D.U. @ 12 UNITS	0 UNITS	174 SF / D.U. @ 49 UNITS
Total Lot Area / Unit (SF)			536 SF / D.U. @ 14 UNITS	0 UNITS	174 SF / D.U. @ 49 UNITS
Building Height(s) (Ft)		13'	45' (max.)(Baseline Zoning)	35' (max.)(Baseline Zoning)	
MAOD : Requirements:		N/A	50' max. (Massachusetts Avenue Overlay District) - Active non-residential ground floor use - Minimum ground-floor use depth of 40' - Ground floor located at mean grade of abutting sidewalk - Minimum 75% Mass Ave frontage occupancy - Minimum 15' ground floor height - Maximum 5,000 sf per ground floor tenant - No bank frontage > 25'		9 Stories ~102'/6 Stories ~69'-6" (j)
Front Yard Setback - Massachusetts Avenue (Ft)(a)		3.8'	Principal wall plane of an adjacent building facing the same street OR the BA-2 baseline requirement; whichever is less		0'
[Baseline Zoning - Article 5.33, Table 5-3, footnote (m)]					
Front Yard Setback - Walden Street (Ft)(a)		3.5'	5' (min.)(Baseline/MAOD)	15' (min.)(Baseline Zoning)	~0'-6" along Walden St (l)
Side Yard Setback - Abut City of Cambridge parking lot (Ft)(a)		42.2'	10' (min.)(Baseline/MAOD)	7'-6" (min.)(sum of 20)(Baseline Zoning)	0' (Abut City of Cambridge parking lot)
Side Yard Setback - Abut Cambridge Housing Authority (Ft)(a)		42.4'	10' min.	7'-6" (min.)(sum of 20)(Baseline Zoning)	0' (Abut Cambridge Housing Authority)
Open Space (% of Lot Area)		0.0%	No minimum		0
Private Open Space		0.0%	No minimum	40% Minimum Private Open Space to Lot Area =	0
Permeable Open Space		0.0%	No minimum	405 SF (min.)(g)	0 (m)
Other Open Space (Specify)		N/A	No minimum		0
Off-Street Parking Spaces		Baseline and MAOD:			3 accessible (b)(c)
Long-Term Bicycle Parking		15 (14 regular, 1 accessible)	1 per D.U. = 49 (min.)	N/A (Multifamily dwellings not allowed)	51 (Residential) + 0.4 (Commercial) = 51 (c)
Short-Term Bicycle Parking		0	1:1 first 20 D.U., then D.U. x 1.05 (min.)	1:1 first 20 D.U., then D.U. x 1.05 (min.)	0 (e)
Loading Bays		0	0.10 per D.U. (min.)	0.10 per D.U. (min.)	N/A
		0	N/A	N/A	
Allowable Uses		N/A	Multi Family Residential, Retail, Restaurant, Office, Institutional and Lab	Residential	Residential / Ground floor commercial / restaurant (f) and other uses as described on the Waiver List

(a) Lot is located on a corner. Project team assumed two front and side yards with no rear yard.

(b) Accessible parking requirement rounded up under UFAS (required for Section 504) to three (3) spaces.

(c) Commercial Parking is waived under Article 6.36 based on actual quantity required being below four (4) required spots.

(d) Garage and bicycle parking exempt from calculation.

(e) Project team pursuing public contribution approach for short-term bicycle parking per Article 6.104.2 (b).

(f) Along with other future possible uses as described on the Waiver List.

(g) ~1,013 SF of the total lot area is in Residence B, with the remainder in BA-2.

(h) Total GFA has increased due to the addition of roof terrace area (1,856 sf) and the net sf addition of the ninth floor on Mass Ave. (1,500 sf).

(j) 9 stories reference front volume along Mass Ave and front end of Walden St, 6 stories reference rear volume towards rear of lot along Walden St facing neighborhood.

(k) Building is sited to align with building next door which is right on the sidewalk.

(l) Along Walden Street for the length of the proposed building at the ground floor, Owners are granting City of Cambridge an easement.

(m) Roof terrace at 6th floor roof will contain combination of occupiable area and permeable open space.

**LIST OF REQUESTED EXEMPTIONS/
EXCEPTIONS/WAIVERS FROM THE APPLICABLE
CITY OF CAMBRIDGE ORDINANCES AND REGULATIONS
for CC HRE 2072 MASS AVE TENANT LLC
(Updated as of December 3, 2020)**

CAMBRIDGE ZONING ORDINANCE – CHAPTER 17 OF MUNICIPAL CODE

	<u>Section</u>	<u>Provision</u>	<u>Requested Exemption/Exception/Waivers</u>
1.	§ 4.30 Table of Use Regulations and §4.31(g)	Multifamily dwelling is Prohibited in Residence B Zoning District.	The Applicant seeks zoning relief to allow the proposed Multifamily Development.
2.	§ 4.21 Special Classification Rules (Accessory Uses)	Allowed accessory uses include, inter alia, off-street parking, customary home occupations and certain service establishments and eating establishments for residents of multi-family dwellings.	To the extent that the proposed building amenity uses are not enumerated in Section 4.21, the Applicant seeks zoning relief to allow the proposed amenity uses for the building occupants.
3.	§ 4.37(B) (2) Light Industry, Wholesale Business and Storage	Table of Uses in article 4.30 prohibits Catering Commercial kitchen as known as wholesale food products, including bakery, confectionery and dairy products	The Applicant seeks zoning relief to allow a Catering or Commercial Kitchen in the area shown on the plans as Retail.
4.	§ 5.11 Development Standards – General Regulations	No building or structure shall be built nor shall any existing building or structure be enlarged which does not conform to the regulations as to maximum ratio of floor area and lot areas, minimum lot sizes, minimum lot area for each dwelling unit or equivalent, minimum lot width, minimum dimensions of front, side and rear yards and maximum height of structures.	The Applicant seeks zoning relief to allow the proposed Multifamily Development as shown on the Plans. Specific requests are set forth below. A Comprehensive Permit may provide all local permits and approvals per M.G.L. c. 40B.
5.	§ 5.22.1 and § 5.22.3; §5.31 and Table 5-1 - Table of Dimensional Requirements – Residential Districts	Private open space shall be provided and shall be a percentage of the lot area as set forth in Section 5.31. An area designated as private open space must have both a width and a length of at least 15', except for balconies, and may not have a slope greater than 10%. With the exception of balcony areas, private open space shall be accessible to all occupants of a building; not less than ½ of the required private open space shall be provided at ground level or within 10' of the level of the lowest floor used for residential purposes. In the Residence B Zoning District, at least 50% of the required Private open space shall meet all of the requirements of Section 5.22.1. At least 50% of the required Private open space shall meet the definition of Permeable Open Space and shall not be subject to the dimensional limitations of Section 5.22.1 as applied to Private open space.	The Applicant seeks zoning relief to allow the proposed Multifamily Development as shown on the Plans. The proposed private open space has a width and length of less than 15', as shown on the Plans. All private open space is located at ground level. At least 50% of the provided private open space will be Permeable Open Space (as shown on the Plans); however, as described above, the proposed Multifamily Development does not meet the required private open space requirement. A Comprehensive Permit may provide all local permits and approvals per M.G.L. c. 40B.

	<u>Section</u>	<u>Provision</u>	<u>Requested Exemption/Exception/Waivers</u>
6.	§ 5.31 and Table 5-1 - Table of Dimensional Requirements – Residence B and Business A-2 District	Business A-2 Maximum Ratio of Floor Area to Lot Area = 1.0 for Non-residential Uses and 1.75 for Residential Uses. Residence B Maximum Ratio of Floor Area to Lot Area is .50. For those portions of any lot exceeding 5,000 sf, the applicable Maximum Ratio of Floor Area to Lot Area shall be 0.35 for all permitted residential uses.	To waive the requirement for dimensional variances and to allow the proposed Multifamily Development as shown on the Plans. The existing Ratio of Floor Area to Lot Area is approximately 0.22 and the proposed Ratio of Floor Area to Lot Area is approximately 7.13.
7.	§ 5.31 and Table 5-1 - Table of Dimensional Requirements – Residence B and Business A-2 District	Business A-2 Minimum Lot Area for Each Dwelling Unit = 600 sf. Per dwelling unit. Residence B Minimum Lot Area for Each Dwelling Unit = 2,500 per dwelling unit. For those portions of any lot exceeding 5,000 sf, the applicable Minimum Lot Area for Each Dwelling Unit shall be 4,000 sf.	To waive the requirement for dimensional variances and to allow the proposed Multifamily Development as shown on the Plans. The proposed 49 dwelling units cannot comply with the Minimum Lot Area for Each Dwelling Unit requirement.
8.	§ 5.31 and Table 5-1 - Table of Dimensional Requirements – Residence B and Business A-2 District	Business A-2 Minimum Front Yard = 5’ Residence B Minimum Front Yard = 15’	To waive the requirement for dimensional variances and to allow the proposed Multifamily Development as shown on the Plans. The smallest existing front yard setback is approximately 0’; the smallest proposed front yard setback will be approximately 0’.
9.	§ 5.31 and Table 5-1 - Table of Dimensional Requirements – Residence B and Business A-2 District	Business A-2 Minimum Side Yard = 10’ on both side yards. Residence B Minimum Side Yard = 7’6" (sum of 20).	To waive the requirement for dimensional variances and to allow the proposed Multifamily Development as shown on the Plans. The smallest existing side yard setback is approximately 42.4’; the smallest proposed side yard setback will be approximately 0’.
10.	§ 5.31 and Table 5-1 - Table of Dimensional Requirements – Residence B and Business A-2 District	Business A-2 Maximum Height = 45’ Residence B Maximum Height = 35’	Due to the need for a building height of approximately 102’/9 stories on Mass Ave and 69.6’/6 stories on Walden and considering the adjustments that may occur during development of the plans and drawings from design development to full construction drawings, the Applicant seeks a waiver of the height requirement not less than 68’ and not greater than 102’.
11.	§ 6.36.1 – Schedule of Parking and Loading Requirements	In Business A-2 District, there is a one parking space per dwelling requirement. In Residence B district, multifamily dwellings are not allowed; therefore, Section 6.36.1(g) states that there is no applicable requirement for off-street parking for multifamily dwellings in the Residence B district.	Although there is no technical requirement for off-street parking in the Residence B district for a multifamily use, the Applicant seeks a waiver to allow the proposed Multifamily Dwelling with three (3) proposed accessible off-street parking spaces and two (2) drop off spaces as shown on the Plans. A Comprehensive Permit may provide all local permits and approvals per M.G.L. c. 40B.

	<u>Section</u>	<u>Provision</u>	<u>Requested Exemption/Exception/Waivers</u>
12.	§6.42 – Design and Maintenance of Off-Street Parking Facilities – Dimensions for Off-Street Parking Spaces	Dimensions for off street parking spaces. Aisle Width of 22’ required.	To waive the requirement for a dimensional variance and to allow the proposed Multifamily Development as shown on the Plans. The proposed dimension of parking spaces shall be less than minimum required aisle width will be less than the required 22’.
13.	§6.43.4(c) – Design and Maintenance of Off-Street Parking Facilities – Driveways	Grade and design of driveway shall provide a clear view to the driver of any car exiting from the facility, or traffic on the street and of pedestrians.	Due to constraints of the driveway location and building, the clear view from the proposed driveway to Massachusetts Avenue when looking south may be compromised. The Applicant requests a waiver from the requirement for a variance and to allow the proposed Multifamily Development as shown on the Plans.
14.	Article 19 – Project Review	Establishes traffic and urban design standards for development projects exceeding 20,000 gross square feet that are likely to have a significant impact on abutting properties and the surrounding urban environment. Requirements include a Special Permit from the Planning Board including Traffic Impact Review (including a Traffic Impact Study), Urban Design Review, Tree Study, Sewer Service Infrastructure Review, Water Service Infrastructure Review, Noise Mitigation Review, Citywide Advisory Development Consultation and specific building and site plan elements.	To waive all of the applicable Article 19 requirements for a Planning Board Special Permit and other requirements and to allow the proposed Multifamily Development that exceeds 20,000 gross square feet. A Comprehensive Permit may provide all local permits and approvals per M.G.L. c. 40B.
15.	§20.100 Massachusetts Avenue Overlay District	Contains specific requirements for projects located within the Massachusetts Avenue Overlay District, including use regulations, dimensional requirements, and design standards. Projects are also required to comply with the Large Project Review process.	To waive all of the applicable requirements of Section 20.100 without the need for a Planning Board Special Permit and to allow the proposed Multifamily Dwelling without a Special Permit from the Planning Board. A Comprehensive Permit may provide all local permits and approvals per M.G.L. c. 40B.

MUNICIPAL CODE

	<u>Section</u>	<u>Provision</u>	<u>Requested Exemption/Exception/Waivers and Notes</u>
1.	Chapter 12.04.020 - Street Numbers	The City Council shall assign numbers to houses.	To waive the requirements of this provision of the Municipal Code so that no separate approval is required from the City Council. To allow the Board of Zoning Appeals to delegate the task of assigning house number(s) for the proposed Multifamily Development to the Building Department, to be completed prior to issuance of Certificates of Occupancy. A Comprehensive Permit may provide all local permits and approvals per M.G.L. c. 40B.
2.	Chapter 12.12.010 - Curb Cut	The City Council shall approve the Curb Cut modification to support the proposed multifamily development as shown on the plans.	To waive the requirements of this provision of the Municipal Code so that no separate approval is required from the Superintendent of Streets and/or the City Council for the modification to the curb cut on Walden Street as shown on the Plans. A Comprehensive Permit may provide all local permits and approvals per M.G.L. c. 40B.



LEGEND

- Existing Shadow
- New Shadow

SUMMER SOLSTICE | June 21, 9:00am

SHADOW STUDIES



LEGEND

- Existing Shadow
- New Shadow

SUMMER SOLSTICE | June 21, 12:00pm

SHADOW STUDIES



LEGEND

- Existing Shadow
- New Shadow

SUMMER SOLSTICE | June 21, 3:00pm

SHADOW STUDIES



LEGEND

- Existing Shadow
- New Shadow

SUMMER SOLSTICE | June 21, 6:00pm

SHADOW STUDIES



LEGEND

- Existing Shadow
- New Shadow

WINTER SOLSTICE | December 21, 9:00am

SHADOW STUDIES



LEGEND

- Existing Shadow
- New Shadow

WINTER SOLSTICE | December 21, 12:00pm

SHADOW STUDIES



LEGEND

- Existing Shadow
- New Shadow

WINTER SOLSTICE | December 21, 3:00pm

SHADOW STUDIES



LEGEND

- Existing Shadow
- New Shadow



EQUINOX | March 21 / September 21, 9:00am

SHADOW STUDIES



LEGEND

- Existing Shadow
- New Shadow

EQUINOX | March 21 / September 21, 12:00pm

SHADOW STUDIES



LEGEND

- Existing Shadow
- New Shadow



EQUINOX | March 21 / September 21, 3:00pm

SHADOW STUDIES

Middlesex South Registry of Deeds

Electronically Recorded Document

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Recording Information

Document Number	: 214319
Document Type	: LEASE
Recorded Date	: November 16, 2020
Recorded Time	: 01:39:06 PM
Recorded Book and Page	: 76174 / 595
Number of Pages(including cover sheet)	: 6
Receipt Number	: 2555550
Recording Fee	: \$105.00

Middlesex South Registry of Deeds
Maria C. Curtatone, Register
208 Cambridge Street
Cambridge, MA 02141
617-679-6300
www.middlesexsouthregistry.com

EXHIBIT C
NOTICE OF LEASE

Pursuant to Massachusetts General Laws, Chapter 183, Section 4, as amended, notice is hereby given of a ground lease (the "Lease") as follows:

EFFECTIVE DATE:	November 9, 2020
LANDLORD:	CC HRE 2072 Mass Ave LLC, a Massachusetts limited liability company
TENANT:	CC HRE 2072 Mass Ave Tenant LLC, a Massachusetts limited liability company
DATE OF EXECUTION OF LEASE:	November 9, 2020
DESCRIPTION OF PREMISES:	The parcel of land located in Cambridge, Massachusetts, more particularly described in Exhibit A attached hereto.
COMMENCEMENT DATE OF ORIGINAL TERM:	November 9, 2020
TERM:	99 years expiring on November 9, 2119

Deed Reference: Book 70850, Page 295

Signed, sealed and delivered as of November 9, 2020.

LANDLORD:

CC HRE 2072 Mass Ave LLC, a Massachusetts
limited liability company

By: Capstone 2072 Mass Ave LLC, its managing
member

By: 

Name: Jason Korb

Title: Managing Member

By: HRE 2072 Mass Ave LLC, its managing
member

By: 

Name: Sean D. Hope

Title: Managing Member

TENANT:

CC HRE 2072 MASS AVE TENANT LLC, a
Massachusetts limited liability company

By: Capstone 2072 Mass Ave LLC, its managing
member

By: 

Name: Jason Korb

Title: Managing Member

By: HRE 2072 Mass Ave LLC, its managing
member

By: 

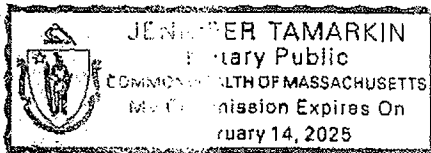
Name: Sean D. Hope

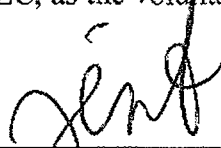
Title: Managing Member

COMMONWEALTH OF MASSACHUSETTS

COUNTY OF SUFFOLK

On this 9th day of November, 2020, before me, the undersigned notary public, personally appeared Jason Korb, managing member of Capstone 2072 Mass Ave LLC, as managing member of CC HRE 2072 Mass Ave LLC, proved to me through satisfactory evidence of identification, which was personal knowledge, to be the person whose name is signed on the preceding or attached document, and acknowledged to me that he signed it voluntarily for its stated purpose in my presence as managing member of Capstone 2072 Mass Ave LLC, as managing member of CC HRE 2072 Mass Ave LLC, as the voluntary act of CC HRE 2072 Mass Ave LLC.

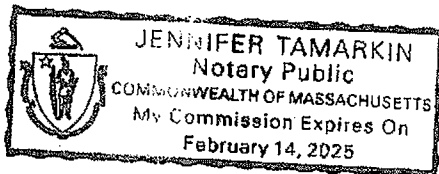


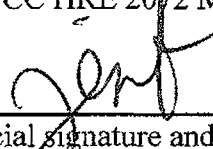

 (Official signature and seal of Notary)
 My Commission Expires:

COMMONWEALTH OF MASSACHUSETTS

COUNTY OF SUFFOLK

On this 9th day of November, 2020, before me, the undersigned notary public, personally appeared Sean D. Hope, managing member of HRE 2072 Mass Ave LLC, as managing member of CC HRE 2072 Mass Ave LLC, proved to me through satisfactory evidence of identification, which was personal knowledge, to be the person whose name is signed on the preceding or attached document, and acknowledged to me that he signed it voluntarily for its stated purpose in my presence as managing member of HRE 2072 Mass Ave LLC, as managing member of CC HRE 2072 Mass Ave LLC, as the voluntary act of CC HRE 2072 Mass Ave LLC.

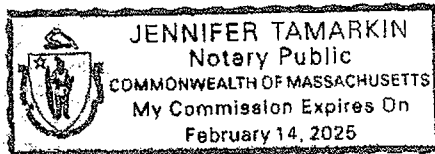


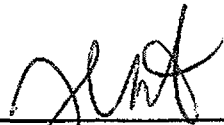

 (Official signature and seal of Notary)
 My Commission Expires:

COMMONWEALTH OF MASSACHUSETTS

COUNTY OF SUFFOLK

On this 9th day of November, 2020, before me, the undersigned notary public, personally appeared Jason Korb, managing member of Capstone 2072 Mass Ave LLC, as managing member of CC HRE 2072 Mass Ave Tenant LLC, proved to me through satisfactory evidence of identification, which was personal knowledge, to be the person whose name is signed on the preceding or attached document, and acknowledged to me that he signed it voluntarily for its stated purpose in my presence as managing member of Capstone 2072 Mass Ave LLC, as managing member of CC HRE 2072 Mass Ave Tenant LLC, as the voluntary act of CC HRE 2072 Mass Ave Tenant LLC.

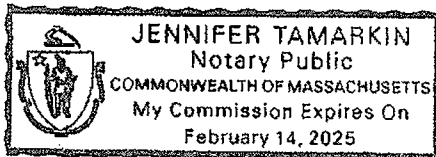



 (Official signature and seal of Notary)
 My Commission Expires:

COMMONWEALTH OF MASSACHUSETTS

COUNTY OF SUFFOLK

On this 9th day of November, 2020, before me, the undersigned notary public, personally appeared Sean D. Hope, managing member of HRE 2072 Mass Ave LLC, as managing member of CC HRE 2072 Mass Ave Tenant LLC, proved to me through satisfactory evidence of identification, which was personal knowledge, to be the person whose name is signed on the preceding or attached document, and acknowledged to me that he signed it voluntarily for its stated purpose in my presence as managing member of HRE 2072 Mass Ave LLC, as managing member of CC HRE 2072 Mass Ave Tenant LLC, as the voluntary act of CC HRE 2072 Mass Ave Tenant LLC.



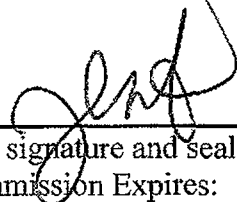

 (Official signature and seal of Notary)
 My Commission Expires:

EXHIBIT A TO NOTICE OF LEASE
Description of the Premises

2072 Massachusetts Avenue, Cambridge, Massachusetts

A certain parcel of land with the buildings thereon situated in said Cambridge on the Southeasterly corner of Massachusetts Avenue and Walden Street, and more particularly bounded and described as follows:

- NORTHWESTERLY: By said Walden Street, one hundred and fifteen and $\frac{27}{100}$ (115.27) feet more or less;
- SOUTHWESTERLY: By land of Ferguson, seventy-five and $\frac{22}{100}$ (75.22) feet more or less;
- SOUTHEASTERLY: By land of Daniel O'Connell, one hundred and thirteen and $\frac{10}{100}$ (113.10) feet more or less;
- NORTHEASTERLY: By said Massachusetts Avenue, seventy-five and $\frac{46}{100}$ (75.46) feet more or less in two lines.

Containing eight thousand five hundred and fifteen (8515) square feet of land more or less.



2072 Mass Ave

Frequently Asked Questions

(Updated 11/23/2020)

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[Parking and Transportation](#)

[Permitting and Next Steps](#)

Capstone Communities and Hope Real Estate Enterprises

Sean D. Hope, a third generation Cantabrigian, is the founder and principal of Hope Real Estate Enterprises LLC in Central

Square, Cambridge. Sean has leveraged his experience as a Land Use attorney and former member of the Cambridge Zoning Board in partnership with Capstone Communities LLC to develop Port Landing, a 20 apartment 100% affordable housing development at 131 Harvard Street in The Port and Frost Terrace, a 100% affordable development at 1791 Massachusetts Avenue in Porter Square (currently under construction).

Sean is deeply committed to improving the quality of life for Cambridge residents through his work with the Margaret Fuller Neighborhood house, Cambridge Children and Family Services (CFCS), and the Cambridge Historical Society.

Jason Korb is the principal of **Capstone Communities LLC** where he has developed market rate, mixed income, and 100% affordable housing. Since founding Capstone in October 2010, Jason has successfully completed a total of \$60,000,000 in development transactions in Cambridge, Somerville, Arlington, Newton and Brockton Massachusetts. Prior to forming Capstone, Jason was the Vice President of Acquisitions at Beacon Communities LLC, a developer, owner, and manager of over 10,000 apartment homes.

Jason is a Director of the Citizens Housing and Planning Association and was formerly a Director of Caritas Communities and the Vice-Chair of Preservation Massachusetts.

Capstone and Hope purchased the 2072 Mass Ave site in 2018 for \$3.6 million utilizing loan proceeds from the Cambridge Affordable Housing Trust.

General Information

What is the proposed mix of apartments?

The development proposal includes 49 apartments, including 14 one-bedrooms, 21 two-bedrooms, and 14 three-bedrooms. The apartments will be on floors 2-9. With over 71% two- and three-bedroom apartments, 2072 Mass Ave will provide desperately needed affordable housing to families.



Floors 2-6

Floors 7-8





Floor 9

What will be located on the first floor and basement?

The building's first floor will include retail and residential entrances fronting Massachusetts Avenue. The residential entrance will include a large lobby, a management office, and resident amenity space. The corner of Mass Ave and Walden Street will include approximately 1,000sf of retail space that will be programmed for a community-oriented retailer at modest rents. A vehicular entrance off Walden Street will provide access to three (3) handicap accessible parking spaces and two (2) short-term drop off/pick up parking spaces.





Ground

Floor Plan

The building's basement includes bicycle storage and mechanical rooms.



Basement Plan

What is the building's height and number of stories? Will someone be able to see the rooftop mechanical equipment?

The height to the top of the building's parapet facing Mass Ave is approximately 102' and the rear six (6) story portion is approximately 69.5'. The rooftop mechanical equipment will not be visible from the public way.

Since the building is oriented to families, where will kids be able to play?

The building includes a large indoor amenity space on the first floor that will be programmed for children. In addition, the sixth-floor roof will support a flexible outdoor space that may safely accommodate an open children's play area. There are also public playgrounds and parks in the area, including the play yard at St. James Church one block from the site, which will be available to the public during specific hours.

There is significant vacant retail space on Mass Ave, is retail the only way the ground floor can be activated?

The site is in the Massachusetts Avenue Overlay District (MAOD) which requires ground floor retail/active uses to help activate the streetscape and support the other small businesses along the Porter Square and north Mass Ave corridor. The size and layout of the proposed retail space at 2072 Mass Ave is intended to be consistent with the standards for ground floor retail spaces and will allow for flexibility to accommodate a range of allowed uses. Additionally, the project's Comprehensive Permit Waiver List

requests flexibility so that the retail space, if unsuccessful, may also be used for building amenity and or function space.

What are the plans for Darul Kabab, the current restaurant?

The development team has been working with Darul Kabab through COVID-19 and it is anticipated that it will be allowed to remain as a tenant until its lease expiration at the end of 2021.

**BACK TO FAQ
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Affordable Housing

What is the demand for affordable housing in Cambridge?

There has been overwhelming demand for affordable housing in Cambridge for many decades and COVID-19 has resulted in an even sharper increase in the need for affordable housing. As of September 28, 2020, the Cambridge Housing Authority has 20,703 unique applicants across all of CHA's waiting lists (32 total). 7,102 (34%) of applicants indicate they live or work in Cambridge or are a veteran. Per the CHA's mobile voucher waitlist only (12,081 applicants on waitlist) there are 5,657 applicants for one-bedrooms, 4,086 applicants for two-bedrooms, and 2,352 applicants for 3

bedrooms or larger. Of applicants that qualify for a local preference (applicants that live and/or work in Cambridge), there are 2,464 applicants for one-bedrooms, 1,325 applicants for two-bedrooms, and 732 applicants for 3-bedrooms or larger.

Homeowner's Rehab recently leased up The Finch and had 2,261 applicants for 98 affordable apartments. Capstone and Hope's Port Landing had 1,386 applicants for 20 affordable apartment units. The property management team at Frost Terrace, Capstone and Hope's newest development leasing up in Porter Square, estimates that the project will have 4,000 – 5,000 applicants for 40 apartments.

What is considered affordable housing in the context of 2072 Mass Ave?

Affordable housing typically includes apartments that limit household income to at or below 30%, 50% and 60% of the area median income (AMI). For 2020, the adjusted gross income limits in Cambridge for a family of four range from \$38,370 to \$76,740. For 2020, the adjusted gross income limits in Cambridge for a family of three range from \$28,770 to \$57,540. For 2020, three-bedroom monthly rents (including all utilities) would range from \$997 to \$1,995, and two-bedroom monthly rents would range from \$864 to \$1,728. HUD annually updates these rents and incomes.

If someone has very little income but significant assets, could they qualify to live at 2072 Mass Ave?

No. For purposes of qualifying residents to live in the building, income is imputed from assets in addition to being

calculated in more standard ways, such as from W2s, 1099s, etc.

How long will the apartments be restricted as affordable housing?

The apartments at 2072 Mass Ave will be affordable in perpetuity.

How would I apply for an apartment home at 2072 Mass Ave?

If the project is approved, approximately six months prior to the project being completed, the property management team will distribute applications for a public lottery for the apartment homes at 2072 Mass Ave. There will be a 60-day application period during which time applications for the lottery are accepted. If you are looking for an affordable apartment home now, the development team's Frost Terrace Apartments, also in Porter Square, is currently accepting applications. Visit www.FrostTerApts.com for more information.

Do Cambridge residents or household members who work in Cambridge have priority for any of the apartments?

Subject to state and City approval, 70% of the affordable homes would be set-aside for current Cambridge residents and those who work in Cambridge. The City of Cambridge requires the maximum amount of local preference which is capped at 70% by state and federal funders.

Why are all of the apartments rental and not ownership?

A significant portion of the development's funding sources include the Low-Income Housing Tax Credit, a federal subsidy that requires the development to be rental housing.

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Sustainability

What does it mean that the development will be Passive House (PHIUS+) certified?

Pursuant to phius.org:^[1] Passive building comprises a set of design principles used to attain a quantifiable and rigorous level of energy efficiency within a specific quantifiable comfort level. "Optimize your gains and losses" based on climate summarizes the approach. To that end, a passive building is designed and built in accordance with these five building-science principles:

- Employs continuous insulation throughout its entire envelope without any thermal bridging.
- The building envelope is extremely airtight, preventing infiltration of outside air and loss of conditioned air.
- Employs high-performance windows (double or triple-paned windows depending on climate and building type) and doors - solar gain is managed to exploit the sun's

energy for heating purposes in the heating season and to minimize overheating during the cooling season.

- Uses some form of balanced heat- and moisture-recovery ventilation.
- Uses a minimal space conditioning system.

Passive building principles can be applied to all building typologies – from single-family homes to multifamily apartment buildings, offices, and skyscrapers.

Passive design strategy carefully models and balances a comprehensive set of factors including heat emissions from appliances and occupants to keep the building at comfortable and consistent indoor temperatures throughout the heating and cooling seasons. As a result, passive buildings offer tremendous long-term benefits in addition to energy efficiency:

- Superinsulation and airtight construction provide unmatched comfort even in extreme weather conditions.
- Continuous mechanical ventilation of fresh filtered air provides superb indoor air quality.
- A comprehensive systems approach to modeling, design, and construction produces extremely resilient buildings.
- Passive building principles offer the best path to Net Zero and Net Positive buildings by minimizing the load that renewables are required to provide.

Visit phius.org for more information on Passive House.

In addition to PHIUS+, what other sustainability measures will the building incorporate?

The proposal includes significant and broad sustainability measures. The building will include a rooftop solar array, a green roof on the ninth-floor roof, EV charging stations, EnergyStar appliances, LED lighting throughout, building materials with fewer toxins and chemicals than traditional materials, solar shades at the rear façade, large fenestration for natural light, and raised gardens and native trees and shrubs on the sixth floor's roof that will be accessible to residents.

[1] <https://www.phius.org/what-is-passive-building/passive-house-principles>

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Exterior Design

How was the proposed height and density determined?

The development team based the proposed density and Floor to Area Ratio (FAR) on adjacent and nearby properties and its proximity to Mass Ave and Porter Square. The nearby properties include the adjacent six (6) story, 40' Russell

Apartments; the five story, 68' Henderson Carriage House directly across Mass Ave; and the eight (8) story, 71' building located at 2130 Mass Ave. It also noted the nine (9) story, 85' Northview Condominiums building located at 2353 Massachusetts Avenue. The development's architects designed the building in the context of the property fronting onto Mass Ave (the busiest street in Cambridge) and is 0.3 miles from Porter Square, which offers numerous amenities within a short walking distance to the building's future residents.

Where does the building cast its shadow?

The proposed building primarily casts its shadows to the north and northeast across Massachusetts Avenue throughout the year, having minimal impact on the residential neighborhood located behind the proposed site along Walden Street. Please refer to the supplementary shadow studies package for more details.

How was the building's design developed?

The development's design balances a complex set of contextual priorities, which have arisen from analysis, community meetings, and various meetings with the City of Cambridge Urban Planning, Community Development and Traffic and Parking teams. The design goals are:

Create a high-quality affordable housing apartment community for families in Cambridge

The design reflects its context, a strong corner on Cambridge's main street. The building massing is slender,

allowing for light and air for all apartment homes. The site is linked to amenities and services in this walkable neighborhood and the unit interiors are simple, but elegant.

Create significant transparency: Activate the streetscape

A primary design goal is to maintain transparency at street level to facilitate visual connections and to activate the streetscape. The first floor along Mass Ave will be wrapped with transparent storefront glazing and will include both the retail and residential entryway. The residential entryway will be pushed back to create a more pedestrian-scaled experience for residents. The storefront turns the corner onto Walden St and continues along with transparency into the retail and resident amenity spaces. An entrance limited to parking and a services area is hidden on the rear part of the site.

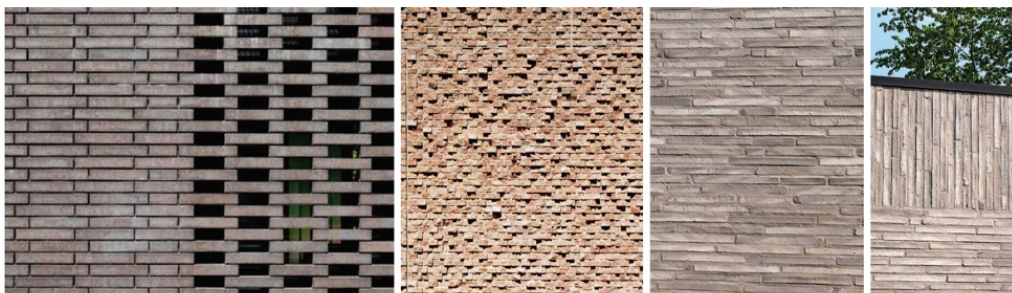
Shaping: Building Form

The building consists of two interconnected volumes – a nine (9) story volume on Massachusetts Avenue and a six (6) story volume at the rear. Along Massachusetts Avenue and West along Walden Street, the nine-story suspended aluminum clad volume is clad in light colored anodized aluminum / zinc vertical panels with a staggered pattern, creating a light and airy volume that floats above one story of recessed storefront glazing. The mass of the building has a strong corner presence that is visible from the street intersection and a grid of openings accentuates the volume, incorporating large windows which provide natural daylight into the interior units, as well as vertical infill composite panels that

resemble wood, bringing warmth and texture. The building steps down to a six (6) story residential anchor towards the rear of the site in response to the transition between the denser urban corridor along Massachusetts Avenue to the residential neighborhood lining Walden St. On the roof of the sixth floor, a landscaped roof terrace will provide amenity space for the enjoyment of residents, while also providing opportunities for green plantings that liven the building's appearance from the surrounding context.

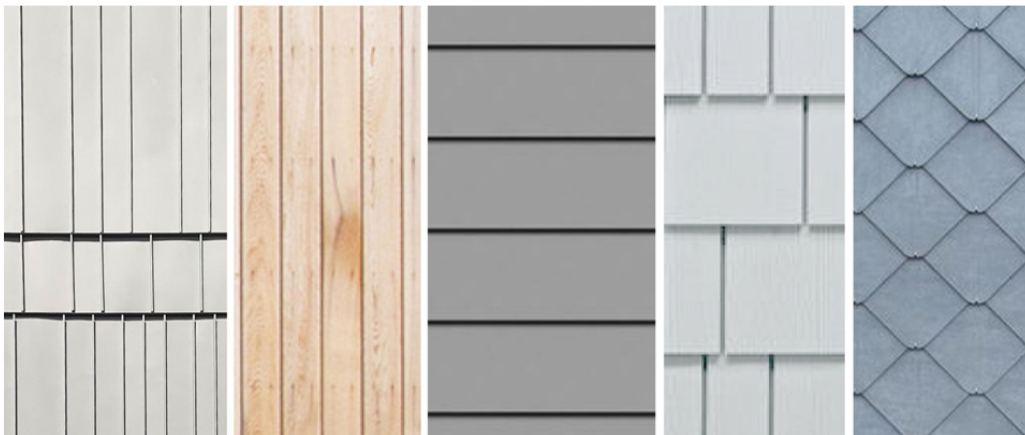
Human Scale: Material Palette

The residential anchor consists of a materials palette that relates to the surrounding residential buildings. The brick plinth / base is located on the ground floor, most prominently at the residential entry which is recessed along Mass Ave, adjacent to the retail space and bus shelter. The crafted long brick at street levels enlivens the pedestrian experience and allows for interactions as the wall angles back and forth. This culminates in a gradient of increasingly frequent openings that also serve as visibility into the parking area, as well as creating ventilation. Above the parking entry, the brick is oriented vertically as a contemporary response to the traditional lintel approach, while highlighting the playfulness of the material.



*Brick pattern material precedents*

Above the brick plinth, painted clapboard comprises the main field of the body. Proportionately sized and spaced windows bordered by a trim are located along all orientations of the residential anchor. On the south elevations, each window has an infill panel below, clad in a lighter colored shingle panel to provide an accent from the main field, as well as to give the perception of the same proportions compared to windows on the north and west facades. In addition, solar shades are proposed on the South façade that not only livens the rhythm of the façade, but also serves a functional purpose of providing shade to reduce the building's overall energy consumption. The top of the residential anchor is defined by a cornice band of half-round shingles.

*Suspended cube and residential anchor material precedents*

There is a mature existing tree on the property line between 2050 and 2072 Mass Ave. Will the development team attempt to save this tree?

The 12" diameter at breast height (DBH) Littleleaf linden

has been reviewed by the development team's master arborist, Daniel Cathcart, who believes the tree can be retained. Mr. Cathcart drafted a tree protection plan that will be incorporated into the development's construction plans.

Electrical transformers in new developments in Porter Square have been placed in conspicuous locations and have reduced visible green space and are unattractive. What steps are the development team taking to conceal the building's transformer?

Pursuant to discussions with Eversource, the electrical transformer has been allocated sufficient space in an enclosed vault under the parking area so that it will not be visible nor will it impede any of the building's amenities.

The development team was aware of this issue at Frost Terrace and located that project's transformer below the front yard and fully out of view.

Since the rooftop amenity space will be accessible to the residents, how will you ensure safety for all?

Safety from the distinct dangers presented by a recreational space on a rooftop is paramount for residents, operational staff, and pedestrians on the street. All current best-practices will be employed to create pleasing, yet effective barriers, for users of all ages and abilities, that meet or exceed building codes.

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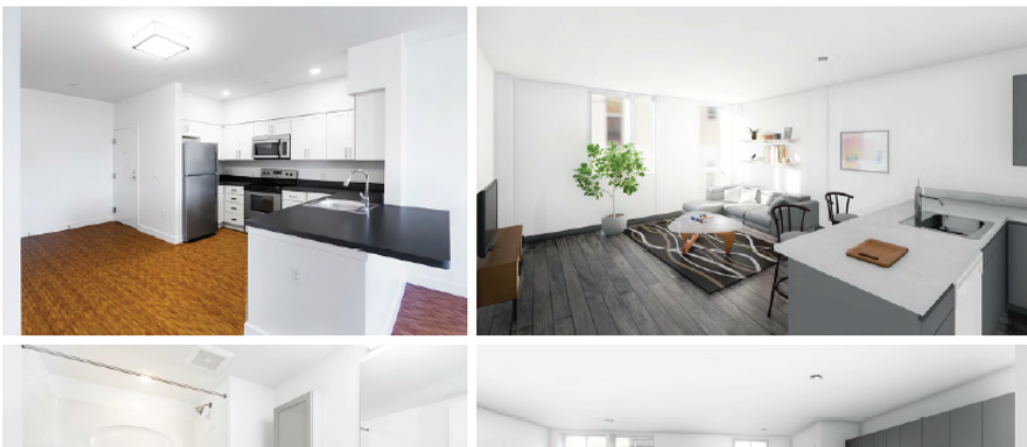
Interior Design

Will the apartments be duplexes, flats, etc.?

All of the apartments will be flats.

How will the interior of the apartments and common areas be finished?

The apartments and common areas will be modern with contemporary fixtures. Below are photographs from the development team's Port Landing development at 131 Harvard Street that was completed in 2016 and renderings from the development team's Frost Terrace development at 1791 Mass Ave that will be completed in spring 2021. The finishes at 2072 Mass Ave will be similar in quality and appearance but will also express the proposal's unique character.





Port Landing



Frost Terrace (renderings)

Will the apartments be accessible to individuals with mobility and sensory difficulties?

All of the apartments will be designed in accordance with the Massachusetts Architectural Access Board 521 CMR 5.00 Group 1 standards that require accessible routes throughout an apartment and include features that can be modified without structural change to meet the specific functional needs of an occupant with a disability. In addition, three (3) apartments will be fully accessible Group 2B units for individuals with mobility disabilities and one (1) apartment will be designed for individuals with sensory disabilities.

Will an elevator be provided?

One elevator will be provided.

What amenities will be in the building?

There will be an onsite management office that will be staffed part-time. In addition, there will be a resident community room where management will organize programming for families, seniors and children. The rear, sixth floor roof will support a flexible outdoor space that may safely accommodate an open children's play area, raised gardens, native trees and shrubs, and unique programming opportunities for residents. There will be enclosed bicycle parking in the basement.

Ownership and Management

How are residents selected? Do current Cambridge residents have any priority?

Current Cambridge residents and certain others will have priority to apply to 70% of the apartments.

Resident screening will follow all federal and state fair housing laws in addition to the requirements specified by the Cambridge Community Development Department and the Massachusetts Department of Housing and Community Development.

Prior to construction completion, there will be a random public lottery that will determine the order in which future residents will be screened. The development team's Frost Terrace apartment community is currently accepting applications for its lottery, visit www.FrostTerApts.com for more information.

Who will manage the community once it opens and for how long will you own the building?

Capstone and Hope will be hiring a professional property management company that specializes in multi-family affordable housing. Capstone and Hope opened the 100% affordable **Port Landing** apartment community in The Port (Cambridge) in 2016. Port Landing is currently managed by Trinity Management Company, a highly respected

management company that currently manages over 6,000 market rate and affordable apartments and over 538,500 square feet of commercial space. Trinity Management is also managing the lease-up and management for Frost Terrace. Under the various affordable housing subsidy programs, Capstone and Hope are required to own 2072 Mass Ave for a period of no less than 10-15 years after the development is completed. This long-term ownership requirement ensures that Capstone and Hope have a vested interest in constructing a high quality and sustainable apartment community.

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Parking and Transportation

The proposal includes only 3 onsite parking spaces and 2 short-term drop off spaces, will residents have cars and where will they park?

The development's parking consultant, VAI, estimates that the building's residents will have approximately 25 vehicles. VAI surveyed the surrounding streets within ¼ mile of the site and determined that there are a total of 928 parking spaces. At the peak demand time of 12pm, there were 281

available parking spaces. [Click here to review the parking study.](#)

What will the developer do to reduce residents' dependence on cars?

Management will employ traffic demand measures (TDM) that include 1) designating an on-site employee as the site's Transportation Coordinator to oversee marketing and promoting of transportation options at the site, 2) providing new residents transportation information packets with information on getting around Cambridge sustainably, 3) installing real-time transit display screen in the lobby to make it simpler for residents, visitors, and employees to access real-time transit and BLUEbikes availability information in the area. The screens will also post other useful information on single occupancy modes of travel, such as carpool/vanpool to supermarkets, etc., 4) subsidizing 100 percent of the cost of a MBTA T pass for employees (building property managers/maintenance staff) or \$240 annual reimbursement for bike maintenance for employees who choose to commute by bike, 5) organizing orientation sessions with residents to teach biking rules, safe biking measures, basic maintenance and repairs and help identify bike routes to various locations, 6) Bicycle racks and a bicycle "Fix-it" station will be provided on-site, 7) annually, upon initial move-in and lease renewal, residents will be offered the choice of: 1. annual BLUEbikes membership (including one-time discounted helmet through bluebikes), 2. \$90 credit for ride share service, 3. 1-month adult MBTA Monthly LinkPass, and/or 4. 3-month Student or Senior

Monthly LinkPass. This will be provided PER RESIDENT (not per household) on an annual basis.

The above strategies will encourage non-auto travel by the residents.

Will bicycle parking be provided?

Covered and accessible bike parking will be provided in the building's basement. 48 long-term and three (3) tandem bicycle parking spaces will be provided in a separate bicycle room, which is fully compliant with the City's zoning code.

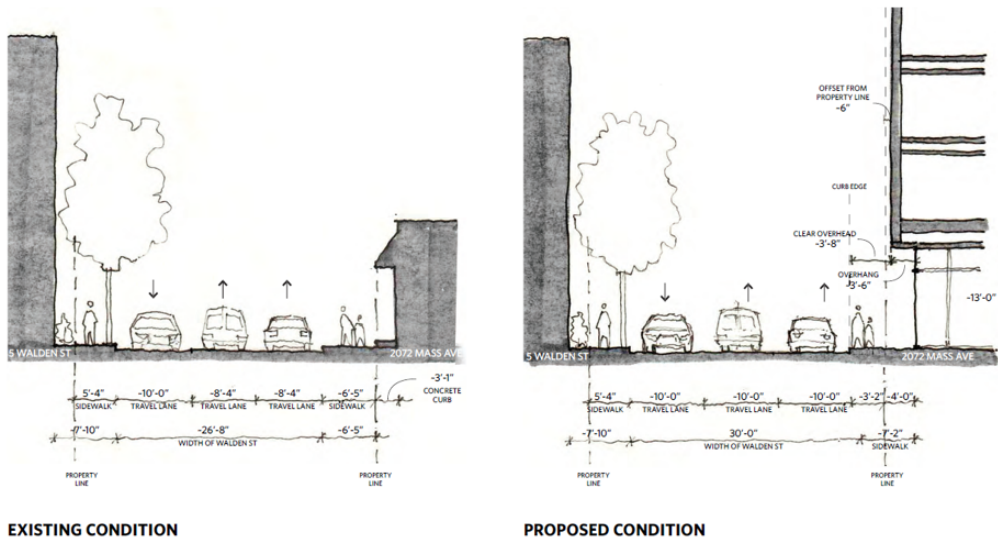
The intersection at Mass Ave and Walden Street is congested and difficult to navigate. What will the proposal do to improve the intersection?

The portion of Walden Street adjacent to the 2072 Mass Ave is unusually narrow for a three-lane street. Currently Walden Street is ~26'-8", with a ~10'-0" travel lane outbound from Massachusetts Avenue and two ~8'-4" travel lanes inbound. This constriction slows traffic moving through the intersection, makes turns difficult, and does not align with best practices and guidelines.

The proposed design includes the widening of Walden Street from ~26'-8" to 30'-0". This is achieved by locating the exterior walls of the ground floor ~4'-0" inside the property line and then granting an easement to the City for a sidewalk in that area, resulting in three (3) 10'-0" travel lanes and making a more spacious street condition. The proposed design also improves the sidewalk by widening it from its current ~6'-5" width to be ~7'-2" wide, both measurements

are inclusive of the curb. The proposed upper floors of the new building overhang the sidewalk by ~3'-6" with an overhead clearance of ~13'-0".

The opportunity to widen Walden Street and significantly relieve its congestion is only possible because the development team is providing a portion of the 2072 Mass Ave land (via an easement) to the public way.



The building will add additional vehicles to the currently constrained adjacent roadways, will this further exacerbate traffic?

The site is currently occupied by a fast-casual restaurant with curb cuts on Mass Ave and Walden Street. The restaurant generates more vehicle trips than the proposal, so the net impact to the roadways is reduced vehicle trips.

How will the short-term drop-off parking spaces be monitored so that residents do not stay for longer than the designated period?

The drop-off spaces will properly function only if they are constantly monitored. The development team is exploring the installation of devices that will alert management and a local towing company if a vehicle stays longer than permitted. The garage has been designed so that a tow truck can access the parking area and tow a vehicle.

Why are no street trees included in the proposal?

The development team had hoped to add as many street trees as feasible; unfortunately, several electrical duct banks under the Mass Ave and Walden Street sidewalks prevent the planting of any street trees at these locations.

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Permitting and Next Steps

What mechanism is being proposed to permit 2072 Mass Ave?

2072 Mass Ave is being proposed to be permitted through a Comprehensive Permit under MGL Chapter 40B. A Comprehensive Permit application permits an applicant to request all local permits through the Board of Zoning Appeal, including a request for any waivers from the underlying zoning.

The Affordable Housing Overlay (AHO) was recently enacted, why is 2072 Mass Ave not permitting the project under the AHO?

The AHO is one of a number of tools to permit an affordable housing development in the City of Cambridge. The AHO would require setbacks and a lower height than currently proposed at 2072 Mass Ave. The development team believes that setbacks are not appropriate along Mass Ave (most of the adjacent buildings have no setbacks) and believes the proposed nine (9) stories along Mass Ave is appropriate and warranted at this location.

What steps have the development team taken to solicit feedback from the elderly residents at the adjacent Russell Apartments located at 2050 Mass Ave?

The development team has twice met with the Cambridge Housing Authority (CHA), the owner and property manager of the Leonard J. Russell Apartments, to present the proposal and recent modifications and solicit feedback. The development team, in conjunction with CHA property management, have displayed renderings and other project information in the Russell lobby along with comment cards for residents to provide feedback. A Russell resident is also a formal liaison and is providing feedback to the development team on behalf of the Russell residents.

How has the development team solicited comments and feedback from the community?

By the time the proposal is presented to the Planning Board on December 1, the development team will have presented and solicited feedback at two (2) large virtual community meetings, two (2) Porter Square Neighbors Association virtual meetings, and many private virtual and phone meetings with direct abutters. In addition, the development team has responded to comments posted on this website and emails it has received from abutters.

What changes to the proposal have resulted from the community process?

The current plans reflect significant feedback from abutters, the neighborhood, and the City. These changes include but are not limited to:

- Allocating of a portion of the 2072 land to the public way in the form of an easement so that each Walden Street vehicular travel lane can be widened to 10' and so that the sidewalk adjacent to 2072 Mass Ave along Walden Street can also be widened;
- Relocating the residential entrance from Walden Street to Mass Ave so that package delivery, ride shares, and pedestrian traffic will be oriented to busier Mass Ave;
- Setting back the first floor of the building along Walden Street;
- Angling the wall adjacent to the driveway along Walden Street back toward the building to provide additional pedestrian safety;

- Adding street level landscaping planters at the garage entrance;
- Reducing the height of the rear residential anchor from eight (8) stories to six (6) stories so that the building steps down to the residential community (the front floating cube increases to 9 stories along Mass Ave); and,
- Adding a flexible outdoor space on the sixth-floor roof that may safely accommodate an open children's play area, raised gardens, native trees and shrubs, and unique programming opportunities for residents.

When are public hearings scheduled for this proposal?

The Planning Board is scheduled to hear the proposal on December 1, 2020 and the Board of Zoning Appeal is slated to hear the proposal on December 10, 2020. Visit <https://www.2072massaveapts.com/updates> for details on these hearings.

If 2072 Mass Ave is approved by the Board of Zoning Appeal, when is the earliest construction could start?

If the proposal is approved by the Board of Zoning Appeal, the earliest the project could start would be in spring/summer 2022. The development would need to apply for and receive additional City and state financing resources before commencing construction. This process takes a minimum of 6-9 months but could take significantly longer.

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Comments



Comments (75)

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Ann Ferraro 15 hours ago · 0 Likes

This is a terrible idea ! This corner is congested to begin with but a giant building blocking your vision without any parking ? You got to be kidding . First it's taller than any other building , how did you get zoning from a one story building to 9?????? Also, you say it's affordable units , but so did the project at upland and the project at porter rd . They are now hotels . It's not noticeable right now , because of covid, but normally it's impossible to navigate that neighborhood during rush hour traffic - it takes me 15 minutes to get out of my driveway some days . You are doubling down on the inconvenience of the residents , and it's just horrible .



Ted Hoff 3 days ago · 0 Likes

My family and I have lived in the Porter Square neighborhood for over 33 years. My wife and I love this neighborhood and Cambridge overall. Our two sons, now adults, grew up here and love this area as well and would like to live here. We know first hand in our family how the housing shortage in Cambridge is preventing all kinds of people from living in this area. We are all so fortunate to have the T stop in Porter Square. Many people can live and work without having to own a car or drive all that frequently. The Porter Square Shopping Center is also a wonderful resource and permits us all to buy almost anything we need without driving far or even driving at all. All of this provides context for why I so enthusiastically support the proposed housing project at 2072 Mass Ave. For years in fact I have thought that this corner would make a great site for an affordable housing project, primarily because of its proximity to Porter Square. I am not a developer so I just kept my thoughts to my wife and myself. But I am delighted that such an impressive project is now being proposed. The project will bring additional vibrancy to our whole area, including for all the terrific local businesses and restaurants we enjoy. I will enable many people to be able to live here, and it seems designed in a way to minimize the environmental impact of the new housing. Thank you.



Jason Korb 3 days ago · 0 Likes

Thank you Ted for your support of the project and for articulating so well all of the positive aspects you see in our proposal. We greatly appreciate your feedback and thoughtful comments.



Hi! This seems like a great project and I look forward to welcoming high-density transit-focused affordable housing to the neighborhood. One question: I am not seeing in-building or in-unit laundry facilities in any of the design materials,

which are often a huge boon to residents. Are you planning to provide that, or is there a nearby laundromat facility?



Jason Korb 3 days ago · 0 Likes

Hi Grace - thank you for supporting the project! Great question - each apartment will have its own washer and dryer hook-up and will be provided with a full size washer and dryer.



Nancy Phillips A week ago · 0 Likes

I wholly support this project, despite its height, because of the critical importance of adding as many affordable units as possible to Cambridge's overwhelmingly UNaffordable housing stock. I do need to mention, though, that the design of the 3-bedroom units is sub-par: far too much circulation space -- which is essentially wasted space -- and much too cramped common space: a combined kitchen-dining-living area. Surely the architects can make these units more liveable!



Jason Korb A week ago · 0 Likes

Nancy,
Thank you for your comments and your support. Regarding the unit layouts, we 1,000% agree with you. We plan to relook at each unit layout and reconfigure where necessary. The units as currently laid out are initial layouts, but as you astutely point out, the 3 beds especially need more work to incorporate the changes you suggest. Since unit layouts are not directly considered in the zoning process, we have focused on

other aspects of the project, but internally have already discussed how we can improve all of the layouts, recognizing the 3 beds need the most work.

We'll make sure to provide updated unit plans as they evolve.

Best,
Jason



James Zall 2 weeks ago · 0 Likes

I've just reread all of the comments posted here so far. Of the 21 people who shared their views of the proposed housing, 15 declared themselves in favor of the project (71%). The most frequently given reasons were the design of the building (10 people), the housing shortage (6) and environmental benefits (5). For the 6 opposed (29%), the most frequent issues were size/density (7 people), traffic (5) and parking (4). (These numbers reflect the fact that most people, not surprisingly, gave more than one reason for their opinion).

I also notice that a slight majority of the opponents declared themselves in favor of affordable housing, some specifically in connection with this proposal and others in a more general way. This indicates that 90% of the people concerned enough to post their thoughts here seem to recognize the harmful effects that the ongoing, worsening housing shortage is having on our city and its residents, on our economy and our environment.

As we're approaching the inauguration of a new, less-divisive administration in Washington, I would like to take the opinion sample here as a local sign of hope. Unlike most new housing developments in Cambridge, this one aims to provide housing for people in the lowest 30% of the overall income distribution. Unlike some of the alternative suggestions that have surfaced in public meetings about this proposal -- like closing Walden St. to traffic at the overpass, thus reducing traffic for the North Walden area but increasing it in surrounding neighborhoods while making it harder for residents to get around the city, or calling for housing to be

housing plan seems to have substantial community support. Can we yet manage to focus on what unites us and not on what divides us?



Young Kim 2 weeks ago · 0 Likes

Hi James,
Thank you for such an excellent, deep dive analysis of the public comments. However, one key factor is missing from your analysis which unfortunately you could not have come up as a community commenter - the residential makeup of proponents and opponents into immediate abutters; residents of North Cambridge (say within 1/2 mile of 2072 Mass Ave); Cantabrigians outside North Cambridge and non-residents. I agree with you 100% on focusing on what unites us and I think I am not too far off the mark when I say that should be reasonable development that respects the rights of the immediate abutters and that preserves the character of Cambridge we will be proud to leave as our legacy to our children and future generations to come.



Jenny Tamarkin 2 weeks ago · 0 Likes

James: thank you for your feedback and for taking the time to read through all of the comments - we are happy to hear that 71% of the commenters support our proposed development! It makes it much easier to digest when you break it down the way you did, thank you, and we definitely understand that those who oppose our proposed development do not necessarily oppose affordable housing in general. We look forward to hearing additional comments and feedback from the neighborhood as this project advances and hopefully at the end of the day we can find a way to stand in unison as advocates for safe, quality, affordable housing. - Jenny

**Daniel Smith** 4 weeks ago · 0 Likes

Jenny,
Thank you for your response.
I can tell you categorically that the parking difficulties now are less severe than under normal non-Covid conditions, notwithstanding what Traffic and Parking may guess the situation is. (It seems Cambridge Traffic and Parking examiners work 9:00 to 5:00 and give diminished attention to parking concerns outside those hours.) The parking problem in our neighborhood is not during the day, but during the evening and weekends when residents return from work and local businesses (e.g.; yoga studio, gyms), are open. Many of these businesses are barely operating now. Many residents have headed for the hills during the pandemic, leaving the streets comparatively empty. Furthermore, the 26 unit apartment building 70-80 Porter Road is currently undergoing renovation (as affordable housing). This building has no dedicated parking. Because it is now vacant during construction there are currently no residents from there vying for parking spaces. All of these things will come to an end. People will return along with their automobiles and the parking situation will revert to a scarce situation not reflected by the recent parking study.

**Jenny Tamarkin** 2 weeks ago · 0 Likes

Hi Daniel,
I apologize for the delay in my response. We hear your concern and we appreciate your feedback, we definitely understand that our new development will impact street parking in both the covid and post-covid worlds. However, we maintain our position that this is an appropriate location to build dense housing within close proximity to transit and other services. Our proposal aligns with the City of Cambridge's goal to provide environmentally friendly housing that will allow residents to be less dependent on their cars. We believe that the addition of 25 cars parking in the neighborhood is a reasonable tradeoff to allow 49 families access to high quality housing in a city that has seen an extreme increase in the cost of living over time.

Thank you,
Jenny



Young Kim A month ago · 0 Likes

I have asked Jason to save a large, mature tree on the northwest corner of Russell Apt that overhangs onto 2072 Mass Ave lot if at all possible as even one mature tree is a valuable urban asset. He responded by locating that tree on their property survey and has engaged an expert to generate a preservation plan. I want to thank Jason for listening to my concern and taking these steps to preserve that tree. I look forward to continue to work together to bring about a reasonable 100% affordable housing development that will integrate into the residential neighborhood harmoniously.



Jason Korb 4 weeks ago · 0 Likes

Thank you Young for your comments and for pointing out the tree to us. As you mentioned, we hired a master arborist to develop a tree preservation plan for this tree.



Daniel Smith A month ago · 0 Likes

While I support the aspirations of this project, I have concerns about the density and parking impact. No parking or traffic study done during this time of Covid can accurately reflect the typical conditions that are normal for this area. It just can't – these are not normal times. Current conditions do not reflect the usual reality on the streets in this neighborhood. Though you may shun anecdotal information, I can tell you, observationally, that under normal circumstances parking is frustrating and challenging, especially in the evening when residents return home after work. Parking is not bad today,

usual the difficult parking situation will return too. An additional 25 or so cars, by the developers' own estimate, will compete for the same scarce parking spaces (along with St. James Place and Frost Terrace – all new to Porter Square). For a development project to be successful it must address all issues, not some issues. The parking demand will inevitably have a negative impact on the surrounding neighborhood and residents unless addressed in the design of the building. No validity can be ascribed to a traffic and parking study conducted under the current pandemic conditions. In my opinion, the current project design raises problems of density and parking in particular.
Daniel Smith - Regent Street



Jenny Tamarkin A month ago · 0 Likes

Daniel,

Thank you for your comment. We are aware that the current COVID-19 health pandemic is impacting the data and had spoken to the Cambridge Traffic and Parking department to express these concerns before commencing the study. They felt, and we agree, that imperfect data is better than no data at all. COVID could be impacting parking availability in a different direction than you suggest - for example, many people are working from home so their cars are parked on the street, rather than driving to work every day, so there would be less parking available during the day time and into the evening because people are not community in or out of the neighborhood for work. This is why we felt it was important to engage a professional firm who specializes in this type of data and research. If there were an end in sight to the current health pandemic, we may have been able to wait a few months before commencing the parking study, but unfortunately that is not the case. COVID-19 cases are on the rise again and the demand for affordable housing continues to grow.

I would like to point out that the estimated addition of 25 cars was from the parking and traffic report created

data. However, we do understand that there are a few new developments in the neighborhood that will increase parking demand and that the Walden Street and Mass Ave intersection can be a huge headache and safety hazard even during COVID times, so we are exploring a few different measures that should help to combat the congestion on Walden Street and the surrounding neighborhood which we hope to reveal to the community in the next few weeks.

We understand that our development is going to have an impact on the neighborhood, but we hope that the benefits of providing affordable housing, ethnic and socio-economic diversity and families will outweigh the impact on available street parking.

Thanks,
Jenny



Young Kim A month ago · 0 Likes

I would like to offer a comparison of the proposed 2072 Mass Ave 100% Affordable Housing development (Development) with four existing/under construction 100% affordable housing developments (AHD) within ½ mile of Porter Square:

1. Frost Terrace at 1791 Mass Ave/1 & 2 Frost Terrace under construction
 - a. CC HRE 1791 Mass Ave LLC C/O Capstone Communities LLC
 - b. 40 units with 3 accessible parking
 - c. Multi -stories with 5-story being the tallest.
 - d. The Floor Area Ratio (FAR; gross floor area/lot size) = 2.3.
2. Russell Apt next to 2072 Mass Ave, affordable housing for the elderly
 - a. Cambridge Housing Authority (CHA)
 - b. 51 units with 10 parking spaces one of which is accessible; garden in the rear
 - c. 6-story building
 - d. FAR = 2.9.
3. 78 Porter Road
 - a. Cambridge Affordable Housing Corp, a subsidiary of CHA and currently undergoing renovations.

vouchers after construction is complete. No parking but is set back from property boundaries.

c. 4-story building

d. FAR = 3.3.

4. 1713-15 Mass Ave

a. CHA

b. 16 units; 8 parking spaces one of which is accessible parking; a large backyard;

c. 3-story building

d. FAR = 0.5.

The total number of units in these AHDs is 133 with average FAR of 2.1. In comparison, the Development will be an 8-story building with 49 units; 3 accessible parking and 2 drop off spaces; no setback and no ground level greenspace; and the densest with FAR of 6.7. The proposed number of units is 36.8% increase from the existing number of units and FAR is 315% of the average FAR. Looking at another way, the Development's lot size of 8515 sq. ft. is only 4.7% larger than 78 Porter Road. But the proposal calls for an 8-story building, twice as tall, and twice the FAR occupying the entire lot.

Many people have commented that the Development is a vast improvement over the existing drab commercial buildings in that neighborhood. But the Development is all residential except for the ground level retail so it should be compared to the residential units in the neighborhood and it should provide the quality of life amenities of a residential development. At the very least, it should be set back from the property boundaries and should have ground level greenspace.

Finally, the impact of the Project should be assessed in context of the St. James Church development which is nearing completion (Planning Board's Special Permit PB-241) that will add another 46 dwelling units just 0.1 mile from 2072 Mass Ave. Together these two projects will add 95 dwelling units creating tremendous new demand on City's resources and potentially worsen the traffic and parking problems in the Porter Square area.

I know Capstone Communities/Hope Real Estate Enterprises, proponents of AHDs and residential property owners in this



risk of coming with the aggressive turns from Mass Ave (provided one can pass through the two lanes of traffic first). The consequences for pedestrians and bicycles are similarly dangerous. The development team isn't responsible for correcting the existing traffic problems but should make every effort to lighten the impact.

I urge the design team to re-evaluate the ground floor plan circulation and consider using the existing curb cut along Mass Ave—it is offered modest protection by the traffic signal and would still provide adequate ground floor retail space (evidenced by the existing restaurant). The orientation of traffic toward Mass Ave would be consistent with the scale of the building.



Jason Korb A month ago · 0 Likes

Thank you Cyrus for your thoughtful feedback. We greatly respect your opinions and the time you put into your comments as well as our previous meeting. We will convey your comments regarding the scale and the relationship to Walden to our architects to see if they have any further ideas. We do note that the City parking lot provides a large buffer between our property and our side of Walden Street (we also acknowledge you are across the street, not on our side). In addition, most of the shadow cast by our building does not cast onto the residential neighborhood but rather onto Mass Ave and to the south. That being said, we understand your concerns and will continue to consider them.

With regard to traffic, we are working on a number of mitigation measures that we hope to publicly reveal in the next 1-2 weeks that we believe will address many of your concerns. Pursuant to your feedback and our previous discussions, we are also working to reorient the pedestrian entrance onto Mass Ave instead of Walden Street.

We would be glad to have another Zoom meeting with you and your neighbors at 5 Walden to continue discussing these issues and to present you with revised plans. Thank you again for engaging with us in



Matt Goldstein A month ago · 0 Likes

I live in the neighborhood, pass through this intersection 2-3 times a day (my kids are at Graham and Parks). We're excited about this neat looking project to bring more good looking ,affordable housing to Cambridge! Also really interested in the proposed wood construction. Please let us know how we can help keep this project moving forward!



Jason Korb A month ago · 0 Likes

Thank you Matt for your support, we will continue to keep you in the loop as the public process proceeds.



Seymour Kellerman A month ago · 0 Likes

Thanks for your comment, Michael. I also live nearby, on Cogswell, and have experienced the same congestion and backup that you have. At times it was hard to drive my car out of the driveway. I wanted to add a note about the meeting held this February regarding this problem.

Responding to complaints from our neighbors, two members of the Cambridge Community Development Department convened a meeting to discuss these concerns. The CDD people were surprised at the size of the turnout - at least 40 people by my count. They proposed a solution: allow only a right turn at the corner of Cogswell and Mass Ave. That is a good idea for Mead St. and Cogswell Ave, but it would not improve matters on the corner of Walden and Mass Ave. Construction of an eight-story building at that corner would not help either.

**Michael Kennedy** 2 months ago · 0 Likes

Hi,
I'm more than a little dubious. Let me share a few reasons. First, the positive comments seem overly enthusiastic. I've only read a few so far and they sound the same--hyperbolically positive (as if paid for or written by one person with people's names attached) in order to support the project.

Second, I live a stone's throw from this--and I notice not one of these overly enthusiastic commenters lives in this neighborhood though somewhat nearby--, and while I am not against affordable housing this really isn't affordable housing; it is something else, unfortunately: a rental property to make money for someone/group's investment portfolio or private bank account. Why rent to people who are low-middle income when it does not help them get ahead? If this project were truly interested in helping the neighborhood, the era (green buildings and alternative transportation), and low-middle income Cantabridgians, it would help low-middle income residents realize home ownership, not rentals...'into perpetuity' as this slick piece of advertising states. Apparently, someone or group of investors stand to make and continue to make a pretty penny off the backs of renters who are 'being offered a deal.' And what's to keep it from becoming a big hotel of Air-B-&Ber's? The drop off and pick up spaces (carport) is more like a hotel drop off than an apartment building.

If this were to go forward, maybe about half the size at best? Where's the green space? This building would take up the entire footprint of the lot; it would expand beyond the footprint of the commercial space that it would replace. Does Cambridge Zoning Board allow for this? Seems that it takes advantage of the spirit of any subsidy to encourage low income affordable housing.

Next, because I live nearby and have been watching this once great little residential neighborhood (we are not Brooklyn) erode through overdevelopment and traffic overflow, I know that this extremely congested corner (and dangerous) cannot stand this size project: 49 apartments will bring owners with cars regardless of the projects design to be 'commuter

encouraging. And, more pedestrians means more potential accidents at the corners of Walden-Mass-Cogswell-Mead, which has become a big drag race, daily, in the mornings and afternoons when the traffic backs up over the bridge on Walden street and cars go careening through Mead to Cogswell to Mass Ave in order to try to beat the light at

Walden and Mass Ave to avoid traffic and get across Mass Ave from Cogswell to Russell and into Davis.

And , I did see one woman's comment on here, Susan, and agree with her comments whole-heartedly.

(I will check back to see that my comment remains: the rules state that all germane comments will be post and remain live. I consider this comment in the community 'community feedback' section absolutely germane as I live in this community and in the shadow of this project and would like to hear, primarily, from others who live here and what they believe and feel about this development.)

Thank you for reading, everyone.



Sean Hope A month ago · 0 Likes

Hi Michael,

Thank you for your comments. While this forum is designed for questions, answers and feedback about our proposed redevelopment many of your statements/questions appear to be misleading (intentional or not) and could be easily be answered/disputed by reading the material provided for on the <https://www.2072massaveapts.com/about> website. That said, I will respond to some of what I believe are actual questions and not the editorial comments although I would gladly discuss offline the reasons why my partner and I have dedicated years of our lives, taken substantial risk and feel so rewarded by this important work.

Now to your questions. First, as you can see, we require that all of the commentors to the website provide real names and anyone could easily fact check who wrote

the comment. It is unfair and contrary to a fruitful neighborhood discussion to disparage the motives and opinions of other commentors that express feelings different than your own. This type of rhetoric has dominated our national discourse and resulting in alternative facts and often unnecessary finger pointing.

We are choosing to be transparent and honest about the work we do and hope that in return our motives wouldn't be unfairly called into question.

Secondly, your comment "Why rent to people who are low-middle income when it does not help them get ahead" to me highlights how deep the equity and income gap has become in Cambridge that the day to day struggles of the working poor and middle-income families in our city are just missed. Historically, for many working families in Cambridge, multi-family rental housing was often the primary housing option for low and middle-income families and immigrants like my grandparents who emigrated from the Caribbean to Cambridge in the 1950's. From living in rental housing as a tenant in Cambridge my grandparents were able to save enough money and with the assistance of the G.I. Bill purchase a multi-family dwelling. As an owner they were able to provide housing for other immigrant families for over 30 years while living in one of the units and raising a family. My mother and her two brothers were the first in our family to even graduate high school. Due to the schools, close community in the Port and faith-based institutions in Cambridge all three went on to earn graduate degrees and changed the trajectory of our family. This is just one example of how "rental" housing has helped my family get ahead and prosper but there are countless others I could share with you. It should also be no surprise that the number of children in the Cambridge school system has dropped dramatically since the end of rent control in 1997 as well as the number non-white Cambridge residents. Further, if by "get ahead" you mean that our renters will likely never experience the exponential appreciation of real estate values that you and many other property owners in Cambridge now benefit from having purchased homes in 90's, then you are likely correct. Our residence won't likely have the 1st world privilege

of resting on a Million dollar asset and the economic mobility to move where the jobs are most prevalent but living in this great location will allow our residents immediate access to many of the vital services for families at the Porter Shopping Center (grocery store,

hardware, pharmacy), well-funded schools and religious institutions all within walking distance.

Additionally, the website clearly states that our proposed building is required by statute to remain affordable in perpetuity which means no big hotel or air-b&ber's. The City of Cambridge and State will have invested significant funds into this redevelopment so if ever the affordable covenants were breached there would be numerous mechanisms to correct any improper use. Jason and I will provide personal guarantees to the construction and permanent lenders some of which last 15 years.

Lastly, we are fully aware of the existing hazardous conditions on Walden Street and have committed to working with the neighborhood and Cambridge Traffic & Parking to improve Walden street for pedestrians, cyclist, automobiles and our future residents. We have hired a traffic consultant and a host of engineers to determine what improvements are possible. We recently mentioned at the Porter Square Neighborhood Association (PSNA) meeting a few possibilities we are researching that will potentially involve rethinking the ground floor plan, increasing the width of Walden Street and other traffic calming measures.

I respectfully ask for your continued patience as we incorporate neighborhood feedback into the design and present alternatives that will improve the building for our residents and the Walden Street and Mass Ave intersection.

"If you want to touch the past, touch a rock. If you want to touch the present, touch a flower. If you want to touch the future, touch a life." – Author Unknown

**Susan Frankie** 2 months ago · 0 Likes

What is the maximum capacity of the building?

**Jason Korb** A month ago · 0 Likes

Hi Susan - your question is answered pursuant to 105 CMR 410.400: Minimum standards of fitness for human habitation (State Sanitary Code, chapter II) which applies to housing of all types, affordable and market rate, and states: (A) "[E]very dwelling unit shall contain at least 150 square feet of floor space for its first occupant, and at least 100 square feet of floor space for each additional occupant...." and (B) "In a dwelling unit, every room occupied for sleeping purposes by one occupant shall contain at least 70 square feet of floor space; every room occupied for sleeping purposes by more than one occupant shall contain at least 50 square feet of floor space for each occupant."

Therefore, for the 2072 Mass Ave proposal the MAXIMUM number of potential occupants would be 217. All of the occupants would also need to be related to each other or be a caregiver for one of the other occupants. We have never seen a building occupied to the maximum number of occupants. A better determination of the projected occupancy is from HUD, which estimates that in affordable housing there are an average of 1.5 individuals in a 1 bed, 3 individuals in a 2 bed, and 4.5 individuals in a 3 bed. This would result in an expected occupancy of 147 individuals. This is also more consistent with what we observe in other similar developments.

**Young Kim** 2 months ago · 0 Likes

Thank you for posting the Parking and Traffic Assessment report by Vanasse & Associates (V&A).

...and the new proposed one way, 1-way street for each way,, but it has several flaws and it didn't go far enough. I am not a parking or traffic expert by any means but offering my comments as a retired system engineer and a long-time resident of North Cambridge ½ mile north of 2072 Mass Ave.

The most glaring flaw is that the effects of COVID-19 on parking and traffic were not factored in. I live on Norris Street and the normal, pre-COVID-19, morning rush hour traffic heading into Porter Square and beyond regularly backed up to Norris Street making it very difficult to exit the street to get onto Mass Ave in either direction. The reverse was true in the evening on the northbound side heading towards Alewife Brook Parkway compounded by two traffic lights separated only by a block at Cameron Ave and Cedar Street intersections. And there were hardly any parking spaces available on Norris Street after 6 PM. But since COVID-19 shut-down began, there have been plenty of parking spaces available and I hardly have to wait to make the turn onto Mass Ave, just enough time to come to full stop and observe the oncoming traffic.

The second flaw is that the study was not done from the point of view of residents in the neighborhood nor did it consider the effect of the business establishments in the area. It is impressive that V&A surveyed parking availability within a quarter-mile, 1320 feet, radius. That is almost 4 football-field lengths, including the end zones! Can you imagine yourself circling around to find a parking space and then trudging home in a pouring rain or blinding snowstorm that far? Mr. Korb stated that the proposed project was planned with family in mind. Can you imagine dragging your child even half that distance with groceries? When you purchased your house, I am sure your expectation was to be able to park on your block if you didn't have a driveway.

The meter and unrestricted parking spaces around the project will be taken up by the customers of the existing businesses as well as the proposed 1040 square feet on-site ground floor retail during the business hours. The stay-at-home parents or caregivers of children/elderly, let alone the customers of the proposed retail space, will have a hard time finding parking spaces during these hours.

Another example of this flaw is that the study was conducted for just one weekday, namely Tues, ignoring the bustling social activities around that neighborhood on weekends starting from Fri evening.

The traffic study didn't go far enough in only considering the estimated trip generated by the project. The study should have considered the road configuration at that intersection and the impact of the project on the traffic flow. Walden Street at the intersection is a 3-lane road; one westbound lane and two eastbound lanes with one for right turn only and the other left turn only. The solid lane marking separating the turning lanes starts well west of the property; and the two turn arrows start just about even with the curb cut into the current Darul Kabab. It is not hard to imagine the gridlock a delivery truck could cause when it attempts to turn into the property after turning into Walden Street during the rush hour from Mass Ave. It will immediately get stuck trying to cut across two lanes of eastbound traffic waiting to turn left or right onto Mass Ave.

The study should also have counted the current traffic volume at the intersection and counted the number and duration of cars waiting to make turns either onto Mass Ave from Walden Street or vice versa during the peak commuting hours. This could have been done easily by using strategically placed traffic counters. These counts should then have been extrapolated into normal, pre-COVID estimates to be used in assessing the impact of the new vehicle traffic. Furthermore, the study should have addressed the issue of bicycle traffic and pedestrian foot traffic at this very busy intersection for people trying to get to Porter Square T station or to their destinations. Finally, the study should have assessed the social issue I mentioned above. When we return to "normal" after COVID-19, I envision a vibrant neighborhood with area restaurants and other businesses running at full capacity creating a heavy congestion at this intersection during peak social gathering times.



Jason Korb 2 months ago · 0 Likes

Young - thank you for your comments, our response is as follows:

Paragraph 1 - your comments are anecdotal and perceptual and do not provide data pre-Covid, if you have actual data to present regarding pre-COVID traffic and parking counts please provide those. Our proposal will add 25 cars to on-street parking. The parking study indicates at the peak time (8pm) there are 316 on-street parking spaces available within 1/4 mile of the site. After the 25 cars associated with the proposal park on the street there will still be 291 available remaining on-street parking spaces.

Paragraph 2 - walking 1/4 mile to your home is not unreasonable in the City. This is approximately a 5 minute walk, people do this across all cities all the time, including Cambridge. The proposal includes 2 short-term drop off spaces to allow residents to drop off groceries or other packages prior to parking their car. In addition, the study provides data on a street by street level, so you can extrapolate any distances you would like. We also disagree regarding your expectation that you should be able to park on the street where your home is located. It is a public street paid for by the entire City's taxpayers and to the best of our knowledge no guarantee has ever been provided to anyone regarding parking expectations. Homeowners without parking consciously chose to purchase that home without parking without any guarantees whatsoever regarding availability of on-street parking in front of their home. On-street parking spaces are for the benefit of all Cambridge residents with a parking permit, not just those who reside adjacent to those spaces.

Paragraph 3 - the peak parking demand is at 8pm when most of the retail stores are closed, residential is a perfect shared use with commercial metered parking spaces. The Urban Land Institute and others have done multiple data-driven studies indicating this fact.

Paragraph 4 - weekdays are by far busier than weekends when people travel, businesses are closed, etc. so it is not best practices to survey weekend activity.

Paragraph 5 - we are working on potential solutions to make Walden Street safer overall and address

personnel, materials, and so, the concerns, the issues will follow.

Paragraph 6 - the parking study indicates that the building will generate a TOTAL of 98 vehicle trips on an average weekday, 6 of which will occur during morning peak hour and 9 of which will occur during evening peak hour. Considering the current high volume of traffic on Walden Street and Mass Ave, this is an immaterial impact. In addition, the existing restaurant on the site generates multiple vehicle trips per day as is.

Lastly, part of creating a "vibrant neighborhood" is providing housing opportunities for residents of all incomes, including lower income residents, which this proposal accomplishes.



Taylor Pratt 2 months ago · 0 Likes

I live about a quarter mile from this address, and increased density of affordable housing is sorely needed in our neighborhood. I'm very excited and heartened by the scope of this proposal and amount of housing it intends to add. Taking into consideration some of the other surrounding (and much less visually appealing) buildings, I simply don't understand the comments here that want to cap this building at 3 stories. I very much hope the project goes forward as-is and does not decrease the number of units.



Jason Korb 2 months ago · 0 Likes

Thank you Taylor for your support of the project.



Sarah Klein 2 months ago · 0 Likes

I live around the corner on Russell St. and I'm very excited to see this proposal. I'm very happy to see this focus on affordable housing, particularly for families and during this very difficult time for many, financially. Visually, it will be an improvement relative to some of the surrounding buildings.



Jason Korb 2 months ago · 0 Likes

Thank you Sarah for your support of the project and for your thoughtful comments.



Susan Frankle 2 months ago · 0 Likes

Can you please give as much information as possible on the finances of this venture? Who will profit from it now and over time, how much is taxpayer money, and how is the rest of it getting funded? Why are these rental units vs. home ownership. If this information is already on the website could you point me to it? I think posting it here in the comments section for all who are commenting would be helpful. As much transparency as you can give would be appreciated and I'll post anything I find out with the City as well. Thank you.



Jason Korb 2 months ago · 0 Likes

Susan,
Current and projected funding for the project includes the following:

- The Cambridge Affordable Housing Trust (CAHT) provided a \$3.8M loan in 2018 for the site's acquisition and early predevelopment costs (additional CAHT funding will be requested after zoning);
- Net income from the restaurant;
- Funding from the Commonwealth of Massachusetts Department of Housing and Community Development ("DHCD") will be requested in the form of 9% low income housing tax credits, state low income housing tax credits and subordinate debt;

- Construction and permanent financing most likely from a local or regional bank;
- Project based Section 8 financing either from the Cambridge Housing Authority or DHCD for 8 apartments;
- We are also proposing to defer a material portion of our eligible developer fee and contributing that as a funding source;
- Sean and Jason will provide personal guarantees to the construction and permanent lender and low income housing tax credit investor. Some of these guarantees continue for 15 years.

The development team receives a developer fee in accordance with the guidelines set forth by DHCD. You can find the developer fee calculation in DHCD's 2020-2021 Qualified Allocation Plan on pages 61-62.

<https://www.mass.gov/doc/2020-2021-qap-low-income-housing-tax-credit-qualified-allocation-plan-qap/download>

Non-profit and for-profit developers receive the same fee. At the completion of construction an independent audit and cost certification is completed by licensed CPA's and submitted to DHCD and the City for their review. Annual independent audits are also completed.

These projects typically generate little ongoing cash flow once the building is in operation. The development team receives a modest annual asset management fee to asset manage the property's operations, but only receives this fee to the extent there is available cash flow to pay it after all expenses and debt service is paid. If there is material ongoing cash flow that cash flow is split 50/50 between the development team and the City/State.

In addition to the development team managing the entire development process (acquisition, design, permitting, financing, construction, lease-up) over the course of many years, the development team is responsible ongoing operations and are personally liable for ensuring the development is properly maintained and occupied by qualified households for 15 years. Any failure to do so results in the "recapture" or

clawback of the tax credits, which results in the tax credit investor calling Jason and Sean's guarantees.

From start to finish these projects take +/- 5-6 years minimum. Until the financing closing, the development team receives almost no compensation. The financing closing typically does not occur for at least 3-4 years from when the development team first starts working on a project.

Please advise if you have further questions.



Young 2 months ago · 0 Likes

Great question re finances. To my offline follow up question after the Community Meeting Mr. Korb informed me they plan to file a comprehensive permit zoning application to the BZA. Now that the 100% AHO Amendment has passed the final vote at the Monday's City Council meeting, it would be great if the answer to Susan's question include an explanation of funding difference between comprehensive permit project vs 100% AHO Amendment project.
Thank you



Jason Korb 2 months ago · 0 Likes

The financing and the zoning are separate issues. As we discussed on the community presentation, the AHO would cap the height at 6 stories. We believe that from an urban design perspective this site can support 8 stories due to its location on Mass Ave, near services, transit, etc. I would like to remind everyone that a 6 story building has 14 fewer apartments for low and very low income households and would result in 35 apartments instead of 49 apartments. Considering the incredibly high demand for affordable housing right now in Cambridge, reducing the proposal to 6 stories would frankly be a lost opportunity to house these individuals and families who so desperately need quality housing, now more than

ever.



Young Kim 2 months ago · 0

Likes

Thank you for pointing out that financing and the zoning are separate issues. I realize I should have asked what is the difference between zoning (permitting) difference between comprehensive permit project vs 100% AHO Amendment project. Will the decisions of Planning Board or the Zoning Board be binding or advisory only?
Thank you



Jason Korb

2 months ago · 0 Likes

Hi Young. The Planning Board will review the plans in an advisory capacity with a focus on the design and provide a recommendation to the Zoning Board. The Zoning Board hearing will follow the Planning Board hearing and the Zoning Board be responsible for providing the project's comprehensive permit approval, which is in its sole discretion. Both the Planning Board and Zoning Board hearings will be public with public comment. Thanks.



Susan Frankle 2 months ago · 0 Likes

Seymour thank you for posting this well articulated and well

neighbors (more and more each day that I speak with) who live in the immediate neighborhood. Your suggestion of 3 floors, 20 units is much more in line with what is size appropriate for the neighborhood. Adding some green space which is sorely needed in our area would be a good idea too.

Your question if this is family appropriate is also a great question. There is nowhere for children to play and it's a very dangerous intersection for adults, much less children.

Thank you for speaking up in the interest of our small residential neighborhood.



Sean Hope 2 months ago · 0 Likes

Hi Susan,

Thank you for comments and I look forward to our offline conversation later this week. I did want to direct you to the response that I sent to your neighbor Seymour Kellerman in the comment section. Many of your issues were similar and I believe my responses covers your concerns as well.



Seymour Kellerman 2 months ago · 0 Likes

On Sept. 29, Capstone Communities presented their proposal for an affordable housing project at the corner of Massachusetts Ave and Walden Street. This was the first time i heard about this project.

In the small space adjacent to the Cambridge Senior Center, Capstone plans to construct a 49-unit, 8-floor building. No parking.

The building will impact our neighborhood negatively, crowding further an already densely-populated area. The parking needs of the 150 residents of the proposed building will crowd nearby streets such as ours.

The corner at Walden and Mass Ave is a traffic flow problem.

February of this year, responding to numerous complaints about traffic congestion on Walden St., Mead St., and Cogswell Ave, the Cambridge Community Development Department hosted a well-attended neighborhood meeting. The CDD officials acknowledged the pedestrian safety and traffic flow problems, and they proposed a solution.

An 8-story apartment building on a small space at this busy corner is not a solution. Both during construction and after project completion, traffic flow will be significantly impeded. We cannot imagine that safety will be improved. Is this an appropriate location for family living?

i support affordable housing. However, we oppose the construction of a building this size in such a small and busy area. At the very least, cut the size to 20 units and 3 floors. Or build 4-6 affordable townhouses. Or move the Victory Gardens from Pemberton Street here and build the housing there.



Sean Hope 2 months ago · 0 Likes

Seymour, thank you for comments and for taking the time to express your views. We have heard similar feedback about the existing congestion issues near the corner of Massachusetts Avenue and Walden Street. Please know that we are taking this condition seriously and will continue to work with Cambridge Traffic and Parking and our consultant to try and implement measures to improve this condition both for our future residents and the neighborhood as a whole. In my experience often public infrastructure improvements occur when there are new projects being proposed and this is no different just because this is 100% affordable. We respectfully request your patience as work with our traffic engineer and other consultants to explore and propose solutions that may improve the pedestrian, cyclist and vehicle circulation on Walden Street.

That said, I think we all can agree that no one development can fix a decades old urban congestion traffic issue and hopefully we can similarly agree that it's unfair to place the burden for "solutions" for Walden

we have an obligation to be thoughtful about all the issues surrounding the development and evaluate the inevitable trade-offs keeping front of mind the health and safety of the abutting neighborhood and our future residents.

As a life-long Cambridge resident, I have sadly watched the disappearance of children from our communities especially middle/lower income children and families of color. Housing affordability and equity has become a crisis throughout the commonwealth and environmentally responsible projects such as this that are located close to transit, with the adjacent family services (Porter Square Shopping mall) are a uniquely rare opportunity. Your question about the appropriateness of family living on this site seemingly fails to appreciate the large numbers of families that are in desperate need of high-quality family sized affordable housing. Additionally, the benefits to our residents, especially school age children, will be much more than simply housing. The access to Cambridge's schools, churches, community centers and neighborhoods can alter the trajectory of an entire family and have generational impact. I have personally experienced this and would gladly share stories with you offline. This building has the potential to be occupied by a multitude of families in my lifetime and I believe that working through challenges of where children will play and how best to navigate a challenging existing traffic corner is worth the hard work that it will take to make this development great. As for the appropriate height(s), reasonable minds can disagree although for context the abutting building on Massachusetts Avenue and across the street are both 6 stories with an 8 story building further down Massachusetts Avenue. We do understand that what we are proposing is a substantial change from the existing condition and look forward to continuing the dialogue as we work to improve this project in the weeks to come.



Jeanine Pearson 2 months ago · 0 Likes

I live on Cogswell Ave (1 block away) and I'm very excited to see this proposal. I fully support the design as-is. It's an aesthetic improvement over the existing building on the lot and visually I think it will make our neighborhood more appealing. I'm not concerned about the height as there are many other buildings of a similar size nearby. Cambridge desperately needs more affordable housing (as evidenced by the demand laid out in the presentation). I'm much happier to see new affordable housing than luxury housing in our area.

I also like the focus on sustainability in the design with the Passive House certification. Similarly, I think the location is great because of its access to public transit which helps to reduce the total number of cars on the road. W.r.t parking, I'm interested to see the study, as I live one block away and I never have trouble finding parking even on street sweeping days.



Jason Korb 2 months ago · 0 Likes

Thank you Jeanine for your support of the project! We'll circulate the parking study as soon as we receive it.



Noah Sawyer 2 months ago · 0 Likes

I like the project design, and appreciate how it relates to the neighboring building towards Porter Sq. It creates a nice continuity along the block.

I'm interested in the retail dimensions and what kind of tenants might make sense for the space. Please focus on street visibility, good signage, and room for front of the house/back of the house operations. This opens up the space for more kinds of small businesses.



Jason Korb 2 months ago · 0 Likes

Thanks Noah for your feedback. We are still working on the retail component, recognizing that retail is

moment in time too. The details you point out about retail are much appreciated. We are not retail developers so suggestions such as these are very welcome. We will look into all of these ideas.



Jefferson Smith 2 months ago · 0 Likes

As a Cambridge resident, and the Executive Director of the non-profit Massachusetts Housing Coalition (MHC), I'd like to offer my full support, as well as the full support of our coalition for this project. I believe that Capstone and Hope Real Estate have set the standard for housing not just in Cambridge, but in Massachusetts, and that every resident in our city can and should stand behind this project.

Our coalition is the voice of the grassroots pro-housing movement in the Commonwealth. Instead of building barriers by using zoning regulations to stop homes from being built, MHC believes that we should encourage building homes for every income level, for every family, and in every neighborhood in Massachusetts.

It's been said many times that we are in a housing crisis. Our organization is responding directly to this crisis by advocating for brand new homes just like the ones being proposed at here. Our mission is to reduce the harmful influence of restrictive zoning regulations that have slowed production and increased housing inequality and insecurity. MHC recognizes that we must do better for our residents and for the people that need housing regulations to work for them, not against them.

As far as parking for this project, the next generation of tenants and homeowners have little use for single occupancy vehicles and therefore little need for parking spaces. Our environment is also desperately in search of relief from single occupancy vehicles - we should be helping in any way that we can to make residents LESS dependent on cars. When you allow more parking, more pavement, or any other way to squeeze in more cars, you only succeed in increasing traffic. More parking spaces equal more cars. As a result of these well-known and well-documented facts we should be doing

just what 2072 Mass Ave is proposing — reduce parking spaces. Parking requirements are a costly and outmoded infrastructure requirement. We can make affordable housing more affordable by lowering costs for great projects like this and helping our environment, too.

Honestly, a very sincere thank you to Sean Hope, Jason Korb, and the Capstone Communities team for their vision and persistence in making this project a reality and prioritizing affordable housing for our community. There is no doubt that our organization and many others like it will be holding this as a model for future 100% affordable development and encouraging more like it throughout the state. Thank you again and congratulations on a very important project for our community!



Jason Korb 2 months ago · 0 Likes

Thank you Jefferson for your thoughtful comments.



Lee Farris 2 months ago · 0 Likes

I like that 71% of the units in the building 2 and 3 bedroom apts., which are much needed so families can stay in Cambridge, and that 8 apts. will be for very low income people, which is rare.



Jason Korb 2 months ago · 0 Likes

Thank you Lee, creating family and deeply affordable housing is of utmost importance to our development team and the City Council.



Larry Field 2 months ago · 0 Likes

This project checks critical boxes for me: 1) 100% permanently affordable, 2) in a suitable location for density,

rail and steps from bus, 3) Passive House, so cutting edge on climate, and 4) elegant design.

Even though the preliminary plan is suggesting the kind of use the City usually wants on the street level, I think the development team and the City should look at alternatives. We all know that retail was hard even before COVID and its

future is more uncertain now. Vacancies don't help the owner or neighborhood.

I live on the other side of Porter Square (Mt Vernon), 2 blocks from the Frost Terrace project. Jason and Sean listened to community input and improved the design/lessened impact on abutters, while keeping the original vision. The traffic and parking study done for that project was very useful in assessing the neighborhood impact and I look forward to seeing the findings for 2072 Mass Ave.



Jason Korb 2 months ago · 0 Likes

Thank you Larry for your feedback and support.



Young Kim 2 months ago · 0 Likes

Although this is a very well thought out design for much needed affordable housing, it is far too out of scale with no setbacks from the property boundaries and no ground level greenspace at all. The rooftop greenspace is a great concept but unless the tenants have access to it, it will be of little benefit to them. The proposed 8 story height (reaching almost 100 ft to the roof of the mechanical room) is even taller than what would be allowed by the 100% Affordable Housing Overlay Amendment if it is ordained.

The property is at a very busy intersection of Walden Street and Mass Ave. The design is based on residents' using bicycles and public transit for transportation with no car ownership as only 3 accessible parking spaces and 2 drop-off spaces are planned. The reality is that you can't mandate car-ownership and will create a tremendous parking issue in the neighborhood. Furthermore, cyclists will face very dangerous entry/exit competing with cars especially during rush hours.

**Jason Korb** 2 months ago · 0 Likes

Thank you for your feedback Young and all your thoughtful questions offline. With regard to your assertion that the project is out of scale with the neighborhood and setbacks, I would encourage you to compare it with the setbacks on both sides of Mass Ave near the site. I believe every other building has no setbacks along the Avenue, so this proposal is in line with those. The side of the building facing Walden Street is setback and the building above overhangs that setback.

Your comment regarding the 100' height is misleading. While the mechanical equipment may reach that high, almost none of it is visible from Walden or Mass Ave in any direction. The renderings posted on our website have the mechanical equipment built into them - you just can't see them because they are not visible from the ground level. This is very different than a 100' building where you can see all the way up 100'. This is an 89' building.

Regarding your other points, please see my responses to Susan and others. Our conversations with the Traffic department and our transportation engineer will continue to focus on bicyclist safety as well as vehicular and pedestrian safety. We appreciate that bicyclist safety is of paramount concern.

As I mentioned at the community presentation, I believe we can all agree that there are tradeoffs to everything in life. We plan to further address the traffic issues and will ensure that the intersection meets all safety standards for bicycle, pedestrian and vehicular traffic. Assuming those safety issues will be addressed to the satisfaction of the City's Traffic and Parking

Department (they must, otherwise the development cannot be approved), the debate then becomes one of: (1) slightly fewer on-street parking spaces for the neighborhood's existing residents, and (2) height. We believe the height is appropriate in this location and do not believe it is out of scale with being on the Mass Ave

corridor. Until we have our parking study we cannot assess the on-street parking situation. Some neighbors have said it is not an issue, others have indicated it is an issue. Only the actual independent data can inform our decision making process.

As you know, you can reach out to us anytime. We genuinely want to work with you and your neighbors to make this proposal as beneficial as possible to the neighborhood and to our building's future residents.



Young Kim 2 months ago · 0 Likes

Your architects should be commended for their innovative, elegant and forward-looking design of the building such as hiding the rooftop mechanicals in a room setback so that "almost none of it is visible from Walden or Mass Ave in any direction" and "The side of the building facing Walden Street is setback and the building above overhangs that setback." These are really excellent design features and I would like to request that you go one step further by staggering the top 2 floors (perhaps breaking up the building into 7-story southern half and 8-story northern half) and even set back the top two floors so that the added height of the building would blend in more harmoniously with the surrounding buildings as the Cambridge Housing Authority's Leonard J. Russell Apartments next door shown in the RENDERING Mass Ave | View Towards Walden St. Intersection in your presentation package.



Young Kim 2 months ago · 0 Likes

I fully agree with you that other buildings do not have setbacks along Mass Ave. However, most of the buildings have setbacks on the rear; with parking and/or green space, however modest they may be. Also, I believe we need to compare your plans against other residential buildings, not

commercial buildings, in evaluating your plans. What I love about, and what I am trying to preserve, is the suburban-like residential neighborhoods in this vibrant urban city with so much amenities and so close to a major metropolitan city that Boston is. Almost everywhere you look in Cambridge, residential houses are set back from their property lines with nice greenspace of trees, plants, and lawn. Just walk north along Mass Ave and take a look at 2130 and 2192 Mass Ave. 2130 Mass Ave is an 8-story building like your proposed building but it is set back from Mass Ave and Cogswell Ave and has a large backyard as well. 2192 Mass Ave is a 4-story building and is well set back on all sides with greenspace all around. It would be great if your architects could superimpose your proposed building on the 3D satellite view of Existing Site (page 17 of the presentation package) to contrast the difference. One might argue that the need of affordable housing is more urgent than niceties such as the building set back away from the property line.

I grew up in Manhattan in a 16-story high (if my memory serves me right) corner building with no private space except for the service area around it. One of the development goals as stated in your 2072 Mass Ave Community Presentation is to "(c)reate a High-quality Affordable Housing apartment community with a large percentage of homes for families". This will mean there will be many children and I hate to see them growing up in a building with no outdoor private space. They should be able to come home from school to a nurturing, inviting family neighborhood and not just to a jungle of commercial buildings along Mass Ave.

... ..

planters but you have told me offline that the roof will not be accessible to the residents. So, these greeneries will be of no benefit to the children. The presentation also included an excellent Neighborhood Map - Quarter and Half Mile highlighting the greenspace near the site.

However, according to googlemaps, the three nearest recreational area are Rev. Thomas J. Williams Park (0.6 miles walk along Mass Ave), Gerard Bergin Park/Pemberton tennis courts/basketball courts (0.4 miles walk via Mass Ave & Pemberton St) and Raymond Park (0.4 miles walk via Walden St); all along very busy streets and none within a quarter mile of walking distance. Children need and deserve recreational areas closer to home. Providing them with an indoor play area/gym along with atrium/plant walls (as in the Harvard Holyoke Student Center) would do them a world of good.



Jason Korb 2 months ago · 0

Likes

Hi Young - the play yard at St. James Church will be open to our residents as well as the public from 8am to dusk every day of the week with few exceptions. As you probably know, it is one block from 2072 Mass Ave. at 1991 Mass Ave.



Susan Frankle 2 months ago · 0 Likes

Hi, I'm wondering if you can encourage people who are commenting on this website to mention if they live in the immediate neighborhood that will be impacted by this project (Mead, Cogswell, Walden near Mass Ave). Not a half mile away, not in other parts of Cambridge, but in the direct path of this project and who have experienced the day to day challenges of our neighborhood. Our small residential neighborhood has suffered through congestion and

dangerous traffic conditions for bikers, pedestrians, and cars to the point where we had an overfilled community meeting with the city because of concerns. These conditions will likely return once we are on the other side of the pandemic. The size and scope of this project (49 apartments, roughly 150 people, and likely 25-30 cars) where the building fills almost the entire footprint, is out of zoning, and offers no parking only furthers the crowded conditions and dangerous traffic / road situation. Why wouldn't you consider proposing affordable housing that meets current Cambridge zoning requirements?



Jason Korb 2 months ago · 0 Likes

Thank you Susan for your feedback, we certainly respect your opinion and sympathize with the challenges residents endure as the City grows and evolves. We appreciate your suggestion requesting commenters to indicate if they live in the immediate area and have made that change to the website.

I think James did a good job of explaining some of the reasons why the proposal is not meeting current zoning.

We are continuing to work on the parking and traffic issues and have meetings scheduled with the City's transportation department and our traffic engineer. We are taking those concerns very seriously.

With regard to the proposed density, we respectfully disagree with your assertion that this site is not appropriate for height and density. The demand for affordable housing in Cambridge is overwhelming. As indicated in our presentation, there are over 7,000 unique households on the waitlist who either work or live in Cambridge or are a veteran that need affordable housing. We and others (including most of the urban planning community) believe that in order to meet that demand, sites such as 2072 Mass Ave, being located on Cambridge's busiest street (Mass Ave is four lanes in front of the site as you know), its adjacency to public transit (Red Line and bus), and its proximity to services such as healthcare, education, retail (including a larger

our proposal.

All of that being said, we respect your opposition to the project and are committed to working with you and your neighbors - regardless of whether you support or oppose the project - to design the project in a way that

minimizes adverse impacts to the neighborhood and maximizes benefits to the community.



James Zall 2 months ago · 0 Likes

Also, given Cambridge's high land costs, limiting the number of units to what zoning allows makes the housing no longer affordable to many if not most in Cambridge.



James Zall 2 months ago · 0 Likes

The reason that affordable housing proposals usually do not meet current Cambridge zoning requirements is that those zoning requirements have been designed to prevent affordable housing in particular, and to limit housing in general. About 65% of Cambridge housing does not meet current zoning requirements. That includes many buildings that seem perfectly fine to most Cambridge residents. My home is included in that 65%; odds are that yours or your neighbors' homes are too.



claire silvers 2 months ago · 0 Likes

This seems to be a very well thought through project and there is no doubt that this kind of housing is sorely needed. (Anyone who doubts that should spend 5 minutes looking at available rentals within a mile's radius.) We live a block away on Mead St, and have been pondering the fate of this parcel for years. First choice would have been what used to be called a vest-pocket park, esp. so the senior housing residents would have an outdoor place within reach that

didn't require getting in a van. That is no doubt unrealistic. Then we are concerned about the fate of the restaurant's staff. But if the space is to be devoted to housing, this project is far & away better than the "luxury" overpriced block blot that we had assumed would materialize. The 1st floor treatment set-back makes it more appealing and our 1st impression of the design is that it is distinctive without being jarring, and the apts are well-laid out. Interesting materials. You will get many complaints about adding to parking. That doesn't worry us. The drop-off spots are essential--but the only thing we wonder about is the difficulty of making left turn off south-bound Walden into those spots. As you know, that intersection can become crazed at times, & even during calmer periods complex dynamics can develop when someone tries to left-turn into the restaurant parking lot. Don't know what the solution would be.



Jason Korb 2 months ago · 0 Likes

Thank you Claire for your thoughtful feedback and support as a neighbor of the project. We agree regarding the left turn into our building, as well as a left turn out of our building could also snarl traffic on Walden. We are working with the City's transportation department and our traffic engineer to see if we can prevent and/or limit these movements. We will definitely keep you in the loop as to our progress.



Deb Gallagher 2 months ago · 0 Likes

I live on Cogswell and support the project, as we do need a lot more affordable housing in Cambridge. I'm a little worried about parking, as Cogswell may be the closest street parking that the residents will be able to find. But the need for the housing, I think, is the predominant factor.



Jason Korb 2 months ago · 0 Likes

Thank you Deb for your support of the project. We will be receiving our parking study this week and will post it

here when we receive it. If the parking study shows a significant lack of on-street parking near the site we will look at other possible options for off-street parking.



Michael Salib 2 months ago · 0 Likes

This project looks absolutely amazing! We desperately need more housing, and the building looks beautiful. I think it will fit in very well with that area. I live about a half mile away in North Cambridge and it is so wonderful to see projects like this coming online; I only wish we could get more and that the city would allow an extra floor or two. I hope that permitting and construction go smoothly so we can welcome our new neighbors!



Jason Korb 2 months ago · 0 Likes

Thanks Michael, we look forward to working with you and your neighbors!



James Zall 2 months ago · 0 Likes

I'm glad to see such a well-designed building and well-planned project coming to our neighborhood. With many stores and amenities within walking and biking distance and so close to subway and buses, this is an ideal location for the kind of affordable housing that is in short supply and so badly needed in Cambridge right now.

Thanks for your detailed presentation earlier this week. I hope you'll continue to keep the neighborhood informed as the project progresses.



Jason Korb 2 months ago · 0 Likes

Thanks James! We'll continue to keep the community in



Christopher Schmidt 2 months ago · 0

Likes

This project is super exciting! I really enjoy how this project connects to the context on Mass Ave: while it's slightly taller than some of the surrounding buildings, it flows well in the overall street, and doesn't "overpower" any of the nearby buildings.

The first floor amenity and retail space should provide a good frontage on Mass Ave, and I appreciate the thought that's gone into pulling back from the sidewalk on the corners of the lot to create more walking space -- I know that intersection can be narrow right now.

The location's strong access to transit -- both Davis and Porter, as well as the bus access -- make it a really good candidate for lowered parking; it's so important we move away from the induced demand of creating parking, for climate and other reasons.

I'm also really excited to see that the team is considering mass timber construction! I'd love to know more details about what we can do to support that, since I believe this is also an important climate mitigation element and will require additional state or city rules before it can be done, so I hope you'll share more on that as well.

I'm sure that there are some elements of this project that will raise concerns in the community, but I am super excited by the possibilities here and really look forward to it moving forward.



Jason Korb 2 months ago · 0 Likes

Thank you CHHS for your feedback and support for the project!



Eric Herot 2 months ago · 0 Likes

This project is very exciting and I would love to see it move forward. Zero parking projects are the future of Cambridge and I hope to see more projects like this go forward in the future. I strongly support this project as-is.



Jason Korb 2 months ago · 0 Likes

Thanks Eric!



Tom Chase 2 months ago · 0 Likes

Thank you Capstone, HRE, and Bruner/Cott for bringing this extremely well thought-out, beautiful, and deserving proposed project to the neighborhood. As a resident of the Porter Square area for six years, the last three of those on nearby Pemberton Street, I'm thrilled to see this level of design brought to the dual challenges of affordability and sustainability. We live in a fairly large city right next to an even bigger city. Providing extremely energy efficient housing close to transit, jobs, schools, and services is a big step towards making Cambridge an even more welcoming place, and towards addressing the local and global crises brought on by climate change. Well done. - Tom Chase



Jason Korb 2 months ago · 0 Likes

Thanks Tom, we are looking forward to working with you and the entire NEI team on implementing the Passive House design goals and many other sustainable and energy efficient measures!

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December 5, 2020

Dear Members of the Board of Zoning Appeal,

I am writing to support the proposal of Capstone Communities LLC for a 100% affordable housing development at 2072 Massachusetts Avenue. I have lived in North Cambridge since 1987, and I have watched in amazement the huge increase in the cost of housing, both rental and homeownership in our neighborhood. The addition of 49 units of permanently affordable, family-sized apartments will constitute an important step forward in the City's effort to enable low- and moderate- income families to continue to live in Cambridge.

The site is conveniently located close to public transportation. There is regular bus service along Massachusetts Avenue. The Porter Square T station and the adjacent commuter rail stop are no more than a quarter mile from the site. Residents will be able to easily walk to grocery stores, pharmacies, etc., and will be able to access Porter Square's broad variety of retail and commercial entities without reliance on cars.

As such, the site is a "main corridor" site. Main corridor sites are sites at locations close enough to public transit to significantly decrease car use. Main corridor sites like 2072 Mass can accommodate buildings with additional height, and, in my opinion, should be allowed to do so, especially when the height will enable construction of much-needed housing for low- and moderate- income residents.

Because of its location on Massachusetts Avenue it will provide the kind of walkable and convenient urban site that enables the families who live there to provide their children with the best of urban living in an environment that does not overwhelm the occupants. This is not a proposal for a 300 ft high-rise building; nor is it a proposal for a location right beside an expressway with no way to cross the street. The City has done a lot of work on the pattern of traffic signals at that corner to develop a safe, albeit busy, intersection. The plans for the building show an attractive building that will fit well into the overall built environment on that part of Massachusetts Avenue.

For the above reasons, and most importantly, for the addition of much-needed affordable housing for low- and moderate-income families, I urge the Board of Zoning Appeals to act favorably on this proposal. Thank you for your consideration.

Sincerely,

Margaret Drury
1 Dudley Court
(North) Cambridge

Pacheco, Maria

From: Sam Seidel <seidel.sam@gmail.com>
Sent: Friday, December 4, 2020 10:23 AM
To: Pacheco, Maria
Subject: Letter in support of 2072 Massachusetts Avenue

Dear Board of Zoning Appeal:

I am writing in support of the proposed project at 2072 Massachusetts Avenue, brought forward by Capstone Communities and Hope Real Estate Enterprises in conjunction with Bruner/Cott Architects. I live in the Agassiz Neighborhood, so I view this project as impacting my broader neighborhood.

There are many aspects that recommend this project's approval by the BZA. Let me take a few paragraphs to outline a some of the ones that I see:

From a planner's perspective, Massachusetts Avenue near Porter Square still needs work.

Massachusetts Avenue from Walden Street to Roseland Street needs continued planning attention. The arrival of the Red Line in Porter Square in the 1980s created the possibility for transit-oriented development, increased density, an improved street wall and heightened retail, but actual changes to the avenue have lagged decades behind. Only now, following one of the hottest real estate markets in memory, are we seeing the creation of new housing units — at Beech Street and at Roseland and now the possibility of additional units at Walden. From an urban design perspective, this is a good thing. With regard to the proposed building at Walden, a taller building at that corner (Walden and Mass Ave) fills in a design gap along the avenue and creates a needed visual marker for the beginning of Porter Square. The proposed project at 2072 Mass Ave serves that function well and it does so with design sense, thankfully forgoing red brick in favor of a new material that introduces a new visual cue to Porter.

The team behind the proposed project at 2072 Mass Avenue is the right team.

I mentioned the new housing going up at Roseland — that project is in construction, with attractive design and good contextual layout. It is being developed by this exact same team, working along with the city to create more affordable units on Mass Ave. The HRE/Capstone track record is exemplary and their ability to work with the neighborhood and deliver on their commitments is well established. You couldn't ask for a better, more committed group of people shepherding this process.

The need for affordable housing hasn't diminished in the past decade — it has only increased. With Covid, that need continues to expand.

I won't go through Chapter and Verse of the affordable housing story — the BZA is too familiar with the regional pressures on the housing market and the specific pressures that Cambridge is facing. I will only note that opportunities to create new affordable units in the city is rare and to be able to create them near a transit node is rarer. This site provides both — the opportunity for a significant number of new affordable units in close proximity to a T stop. Additionally, the development team's commitment and ability to deliver is second to none.

In each of their previous projects, they have fostered a good working relationship with all the parties involved to make a better project in the end. This project will be no different, I'm certain.

Finally, I'll close with a list of particulars — the Capstone-HRE-Bruner/Cott list of "Community Process Design Improvements."

This list shows the team's engagement with the local community, their willingness to on-board feedback and comment, and shows their significant [and in my view, very relevant] design improvements to improve their project — such as setting back the building's ground floor along Mass Ave and stepping the building down as it recedes back into the neighborhood. This active dialog will continue as the project moves forward. Of that I'm certain. Here is their list:

- Relocated residential entrance from Walden St. to Mass Ave.
- Widened Walden St to provide three 10' wide driving lanes
- Setback building on ground floor and increased sidewalk width along Walden Street
- Tapered wall at garage entrance to provide additional vehicular/pedestrian visibility
- Added sidewalk planter at garage entrance
- Increased height along Mass Ave from 8 stories to 9 stories
- Lowered height from 8 stories to 6 stories towards residential neighborhood
- Added upper level terrace resident amenity space (~1,850 SF)
- Developed tree protection plan with master arborist
- Updated traffic and parking study
- Updated Transportation Demand Management (TDM) measures

I hope the Board of Zoning Appeal will look favorably on this project and help the creation of much-needed new housing in the city of Cambridge.

Sincerely,

Sam Seidel
43 Harris St.
Cambridge

Pacheco, Maria

From: Seymour Kellerman <seymourkellerman@gmail.com>
Sent: Friday, December 4, 2020 11:51 AM
To: Pacheco, Maria
Subject: Capstone 2072 Mass Ave Comprehensive Permit application

Members of the Board of Zoning Appeal:

On Sept. 29, 2020, Capstone Communities LLC presented a proposal for a mixed-use housing project at the corner of Massachusetts Ave and Walden Street. This was the first time I heard of this project. Were there any negotiations with CDD before public disclosure?

I ask you, members, to visit the Russell Apartments and Senior Center, a few feet away from the proposed tower site, and speak to the affordable housing residents (one is 101 years of age). They will tell you in detail how the project will diminish the quality of their lives. Do you support decent affordable housing for the elderly and disabled?

I ask you to visit this corner and visualize 49 dwelling units crammed into the 8 residential floors with no adjacent open space and with significant traffic hazards on two sides of the building during much of the day. Is that a model for community zoning?

I ask you to reject the rash decision of the Planning Board on Dec. 1 to approve the Capstone comprehensive permit application.

I ask you to take part in amending the current plan by requesting a decrease in its size and density and by adding open space for the building's residents in order to improve health, safety and quality of life for the residents, while reducing the project's impact on the neighborhood. Isn't that kind of oversight the responsibility of BZA?

I ask you to demand a Traffic Impact Study to analyze the impact of the proposed development on the Mass. Ave./Walden St. intersection as well as the surrounding streets (taking into account pre-pandemic conditions).

Respectfully,
Seymour Kellerman
21 Cogswell Ave
617.833.3963

Pacheco, Maria

From: Norma Wassel <normawassel@yahoo.com>
Sent: Tuesday, December 1, 2020 8:27 PM
To: Pacheco, Maria
Subject: BZA-017326-2020

To Members of the Board of Zoning Appeals:

I am writing to urge the board to grant the request for zoning variances in order for the proposed residential building on **2072 Mass. Ave.** to move forward. **(BZA-017326-2020)**

I live @ 175 Richdale Ave. between Walden and Raymond Streets and support this project. I do not own a car and routinely bike through the corner of Walden St. and Mass. Ave. as well as take the bus directly in front of 2071 Mass. Ave. I do not feel that this location would be unsafe for children if families were to live on that corner as some people had said.

I also am a social worker who consults with a number of social service agencies in Cambridge. Lack of affordable housing for families, including many of those who are at risk, is a crisis in our city and I feel that the city should do everything possible to provide more units. Although homeless families are more hidden than the individuals that many of us see in many neighborhoods in Cambridge, the need is there.

I have had a chance to review the project design and I feel very positive about the specifics - the ground floor design that would improve the sidewalk visibility and the focus on being environmentally responsible which will be a prototype for other cities considering similar projects.

I urge you to support this housing project. The variances requested are not significant enough to prevent this critical housing from being built.

Please feel free to contact me if you have any questions.

- Norma Wassel 175 Richdale Ave. unit 214 617-306-4916

Pacheco, Maria

From: Ann Fleck-Henderson <afleckh@gmail.com>
Sent: Tuesday, December 1, 2020 9:32 PM
To: Pacheco, Maria
Subject: for the Board of Zoning Appeals

I am a resident of 113 Richdale Avenue. This note is to express my support for the building proposed by Capstone Communities for the lot at the corner of Walden and Massachusetts Avenue where the Indian restaurant is, on the site of the former KFC. I understand the issues raised by some of my neighbors about congestion at that corner, parking, and the height of the proposed building. I trust that the transportation and parking people will do due research on the first two issues. While the height of the building is somewhat out of scale with the surrounding buildings, I believe that the building may need that height to accommodate enough units to make the whole project feasible. The value to the area of economic diversity outweighs, for me, the aesthetic issue of the proposed building's height.

Ann Fleck-Henderson

Pacheco, Maria

From: Marian Swain <marianswain@gmail.com>
Sent: Tuesday, December 1, 2020 9:16 PM
To: Pacheco, Maria
Cc: Clerk
Subject: Comment on BZA-017326-2020

To the BZA:

I am a Cambridge resident writing in support of the affordable housing development at 2072 Mass Ave (BZA-017326-2020). Cambridge needs more affordable housing built near public transit, which this development offers, and it is also designed to the passive house energy standard, which will contribute to the city's climate goals as well as reduce energy bills for the residents. I urge you to support this development, which will contribute to a more just and sustainable city.

Thank you,

Marian Swain
988 Memorial Dr. #181
Cambridge, MA 02138

BZA Case # 017326-2020

To The Members of the Cambridge Zoning Board of Appeal,

My name is Margaret Rueter. I live at the Leonard J Russell Senior and Disabled building at 2050 Mass Ave. I have communicated with many of my neighbors. I speak for a group of residents that are afraid to give voice for they are tenants of the city of Cambridge.

Our building is just 3 feet from the planned new affordable housing project to be located next door at 2072 Mass Ave. Indeed the new building as planned is not just tight next to us, adding 150-200 more people at a very unsafe corner just feet from us, but also does include adding a proposed additional anchor building as a part of the design. The anchor building, as proposed, is to run along Walden Street and is to be a six-story windowed facade to run across the entirety of our open space and just three feet away from us. The building will tower over and cast shadows along our sunny residential landscaped terrace where gardening has been encouraged. The new neighboring windows and rooftop play space will change the environment from a restful, restorative and quiet area to one of an experience of shared sound and space, with a much more active population. This has seismic implications to this Russell Apartments population which includes some of the city's oldest, most needy and vulnerable citizens. We are people who were vacated for two years due to construction and who have only JUST moved back (spring 2019, during the pandemic) into a completely renovated building which also houses the North Cambridge Senior Center.

The city of Cambridge is experiencing an urgent housing crisis. Cambridge as always, is at the forefront - working to solve this crisis. Further, it is my belief that all of us as citizens give a little something to help the community meet the goal of additional affordable housing. However, the building as

proposed is asking those of us next door to sacrifice our health, safety, and quiet environment.

This is too big a demand on us, if this city really cares to protect their very own most vulnerable tenants. I ask you to imagine it is your grandparent with Alzheimers, your sister with PTSD, your brother with debilitating clinical depression, or maybe your adult child disabled by breathing issues living here at Russell - for this is who we are. Now imagine, the loss of our outdoor enjoyment, neighbors just feet away, seeing into our windows, and then the worst – constant noise, completely appropriate to a family housing environment, and all of this of course after several years of intolerable construction noise. Perhaps many of us will need to move again. Essentially, we are being asked to sacrifice our own mental and physical health by this design. Is this really what we want to do, and is it really what the City wants to do?

Beyond the very real implications to a vulnerable population there are separate but severe safety concerns at the building site's corner. The intersection of Walden and Mass Ave is fraught with issues. Others will speak to them, but there is one area going to be made far worse because of the proposed Massachusetts Avenue entrance. Seniors at both Russell and the North Cambridge Senior Center park behind our building. There is an entrance through an arch between sections of the building. It is already unsafe and dangerous to approach Russell for those of us who use this lot. As one comes through the Mass Ave light or makes the right turn at Walden to approach Russell there is danger. One must IMMEDIATELY have quick reaction times to watch traffic speeding behind us just after the light. We must signal right, hug the right lane while watching for pedestrians, bikes, runners, delivery vehicles, and the bus and The Ride used by our population. All of this happens within feet of the intersection. The addition of an entrance at Mass Ave will bring additional Ubers, delivery vehicles, and drop off and pick ups pulling up at the new front residential entrance just after this corner. Again, I ask you to consider your parents and grandparents trying to navigate this short shared distance to

our building. A death will occur. It is only a matter of when and how many. How many are we willing to accept?

We are all very grateful tenants. We are grateful to both the city of Cambridge and to the Cambridge Housing Authority for their compassionate and steady care.

What I ask, for all of us here at Russell, is that you complete a proper and thorough review of this project before any approval. I also ask that you consider the context of location and ask for a design review. The developer has delivered a beautiful architectural design. However, at this location and given the intersection so near to our senior and disabled community, and the North Cambridge Senior Center, I would further ask for a “reimagining” of design. The building could be shorter, cast less shadow on our only place of outdoor recreation. It could have a population of tenants more commensurate with the lower density we need for safety. It could be set back from the property line – especially from our own walls.

Please don't sacrifice us in the name of Affordable Housing. Let's work together in the best, generous and humane spirit of Cambridge.

Sincerely,

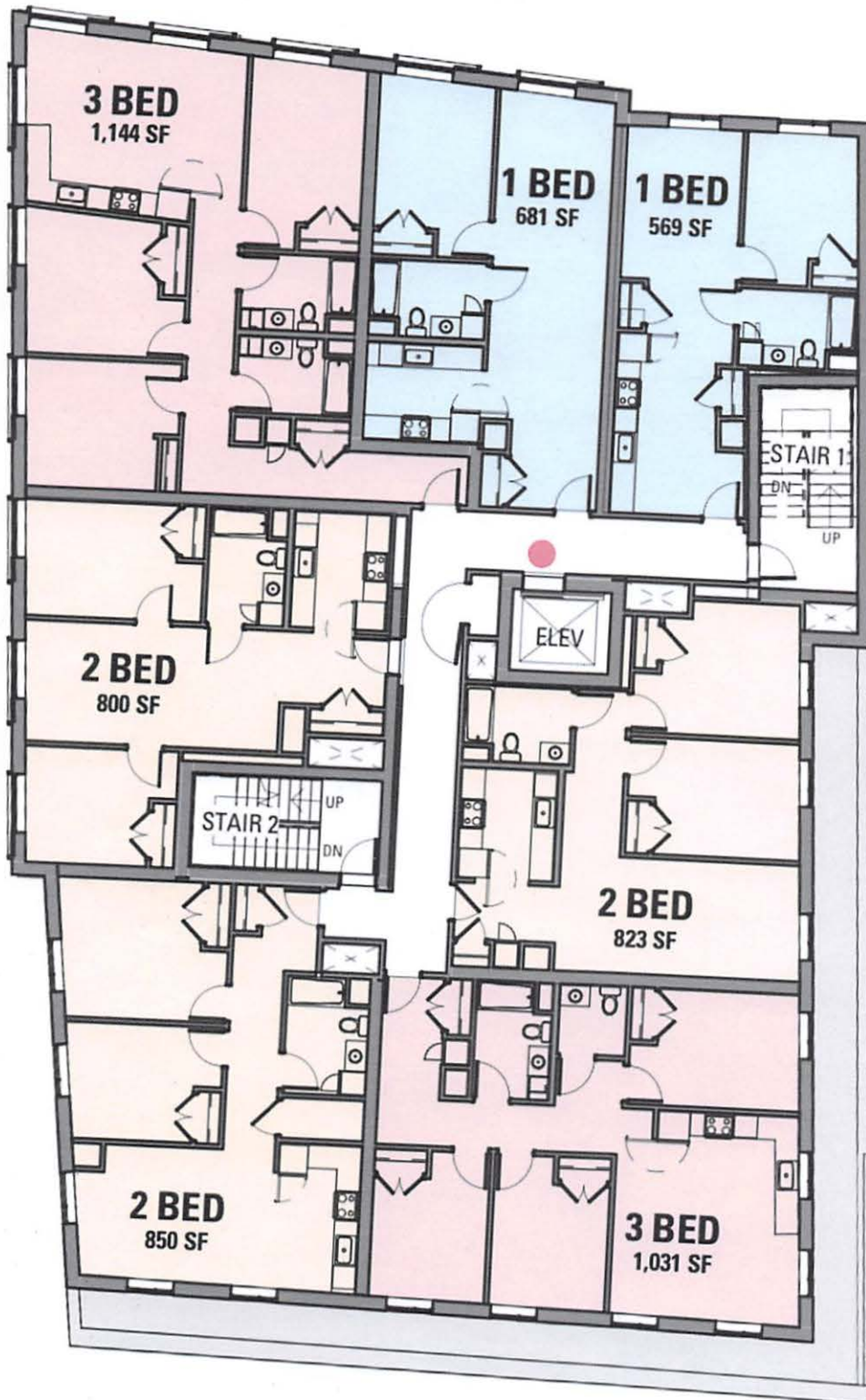
Margaret B Rueter
2050 Mass Ave
Cambridge, MA 02140

Darul Kabab
2072 Mass
Ave

Russell Apartments.
2060 Mass Ave



MASS AVE



2050 MASS AVE

WALDEN ST



ELEVATIONS North | View from Mass Ave

BremerKohl
 architects



HRE
 Hines Real Estate







SW

EE | June 21, 6:00pm

STUDIES

WY 101 HIL 2012 MASS AVE TENANT LLC

2072 MASSAC
N

Pacheco, Maria

From: Kelsey Harris <kelsey.m.k.harris@gmail.com>
Sent: Monday, December 7, 2020 9:48 AM
To: Pacheco, Maria
Subject: CASE NO. BZA-017326-2020

Hello,

I am writing in support of BZA-017326-2020.

I think it's a lovely-looking building that will provide much-needed subsidized housing mere steps from the red line, right on a major bus corridor. I know the area well; I live down the block, abutting the still-under-development Frost Terrace.

Although construction is always a little disruptive (perhaps more so right now, as so many of us are almost always at home), that is a much easier cost to bear than the huge rents we ask working families to bear every single day.

I would like to add that I think the parking planned for the building is entirely adequate, if not even excessive. The 42-unit apartment building I live in has 0 dedicated spots, and it works just fine. Additionally, our planet is warming by the day because of human emissions, causing destruction across the country and globe. Anything we can do to reduce emissions, including reducing parking and including solar, is a step in the right direction.

Please approve the permit,
Kelsey Harris
8 Newport Road

Pacheco, Maria

From: Singanayagam, Ranjit
Sent: Monday, December 7, 2020 11:54 AM
To: Pacheco, Maria
Subject: FW: What did the Planning Board vote to recommend favorably to BZA on 12/1?
Attachments: Height Map 2072 Mass Ave.xlsx

From: Young Kim <ycknorris@gmail.com>
Sent: Thursday, December 3, 2020 12:01 PM
To: Farooq, Iram <ifarooq@cambridgema.gov>; Singanayagam, Ranjit <ranjits@cambridgema.gov>
Cc: Siddiqui, Sumbul <ssiddiqui@cambridgema.gov>; Mallon, Alanna <amallon@cambridgema.gov>; Carlone, Dennis <dcarlone@cambridgema.gov>; McGovern, Marc <mmcGovern@cambridgema.gov>; Nolan, Patricia <pnolan@cambridgema.gov>; Simmons, Denise <dsimmons@cambridgema.gov>; Sobrinho-Wheeler, Jivan <jsobrinhowheeler@cambridgema.gov>; Zondervan, Quinton <qzondervan@cambridgema.gov>; Toomey, Tim <ttoomey@CambridgeMA.GOV>; DePasquale, Louie <ldepasquale@cambridgema.gov>; Glowa, Nancy <nglowa@cambridgema.gov>; Barr, Joseph <jbarr@cambridgema.gov>; City Clerk <CityClerk@CambridgeMA.GOV>; Paden, Liza <lpaden@cambridgema.gov>; Joseph, Swaathi <sjoseph@cambridgema.gov>
Subject: What did the Planning Board vote to recommend favorably to BZA on 12/1?

Dear Assistant City Manager Farooq and Commissioner Singanayagam,

I would like to call your attention to the Dec. 1 Planning Board (PB) hearing of the Board of Zoning Appeal (BZA) Comprehensive Case 2072 Mass Ave Project (2072 case). I would also like to beg your pardon for a rather lengthy email and ask your patience in reading it in its entirety. Then you will understand the subject line.

I should have known but I was surprised to find out that the PB hearings are now scheduled weekly which seems to be very aggressive given that the members are volunteers with full time jobs and families. I don't know how they could possibly read through all the material associated with complex agenda items such as the two cases on Dec. 1.

The 2072 CP case will have a profound impact not only on the Porter Square neighborhood but throughout the City as it sets very dangerous precedence going **well beyond even the limits of the recently ordained 100% Affordable Housing Amendment**. It was heard as a BZA Case and I would like to point out two major flaws in this hearing. To the PB's credit, I must thank them for allowing public comments albeit at a reduced time limit of 2 minutes. The public comment lasted about an hour but the debate and decision took less than half that.

In the past, it was my understanding that the public comments and developer's material have to be in by close of business the Thursday before the hearing. However, because the City offices were closed for Thanksgiving holiday, the comments were due by close of business on Wed (which PB couldn't have seen anyways since the offices were closed) with additional comments accepted on Monday through noon on the day of the hearing. Getting this submittal schedule was very difficult and I am sure the Board members didn't have adequate time to review all the comments we submitted. I want to thank the staff for their tireless work in assembling all the comments we sent in and getting them to the PB members.

In particular, I submitted a slide presentation package on behalf of the North Walden Neighbors to voice the **concerns for/by the senior and disabled residents of Russell Apt** and to make a case why the 2072 Mass Ave project **does not fit in that neighborhood**. We were very disappointed that we could not even make a request to pool our time to present the case more cohesively because we could not raise our hands.

But the most alarming flaw was that the Comprehensive Permit - 2072 Massachusetts Avenue - [Materials](#) in the Planning Board's Meeting table (<https://www.cambridgema.gov/CDD/zoninganddevelopment/planningboard/planningboardmeetings>) was a link to Comprehensive Permit Application received by the City Clerk on Nov 12 which stated: DIMENSIONAL INFORMATION Project Address; 2072 Massachusetts Avenue Building Height(s) (Ft) - REQUESTED CONDITIONS 8 Stories/ appx 89'-8". There was no updated material available and the new design of 9 stories/102 ft. facing Mass Ave and 6 stories facing Walden neighborhood was presented to the PB and the staff only at the hearing. This would mean that all the [CDD, TP+T, DPW Staff Memos](#) were based on the old design. The hearing should have been called off since the developer team did not submit the most up to date material in time. Once again, we could not raise point of order as we could not raise our hands.

One of the PB members said that the height of the building is not much taller than other buildings near the proposed site including the Henderson Carriage House. Perhaps he was referring to the 8 story original design. The only other building taller than 100 ft. that I am aware of near 2072 Mass Ave is 2373 Mass Ave which is 0.5 mile north of 2072 Mass Ave. Please see the attached height map as an excel spreadsheet with graphical representation of heights of various buildings we have used for comparison

In view of the failure of CC HRE to submit the revised application to the City Clerk to be distributed to the PB and BZA, there is a serious question as well as legal implications as to what the Planning Board had voted unanimously (with one absentee) to recommend favorably to the BZA. Therefore, I beg you to reschedule the Dec. 10 BZA hearing of this case and direct the applicant, CC HRE, to submit revised documents in accordance with established data submittal guidelines, not presenting them at the hearing. Furthermore, PB should rehear this case after all the City had the time to review the revised documents and resubmit their updated memos and the public and PB members had adequate time to review the material.

Going one step further, a project that exceeds the threshold of Article 19 Project Review Special Permit by the PB by more than factor of 2.5 (57391 sq. ft, living space waiver requested for 20,000 sq. ft. threshold) should NOT be granted a waiver even for a "friendly 40B". If this waiver is granted for this project, I shudder to think where this will lead to.

Thank you very much for your attention to this matter and looking forward to your response

Respectfully yours,

Young Kim

17 Norris Street

		Frost Terrace (1791 Mass Ave under construction)	Russell Apts (2060 Mass Ave)	1713-15 Mass Ave	Total Existing/under construction
Land (sq. ft.)	78 Porter Rd 8,130	22000	15,258	14,499	59,887
No of Stories (highest)	4	5	6	3	
Living Area (sq. ft.)	26,970	49,772	43,864	7,485	128,091
Floor Area Ratio	3.32	2.26	2.87	0.52	2.14
No of Units	26	40	51	16	133
			10 total with	8 total	
No of Parking Space	0	3 HP	1 HP	with 1 HP	
Distance (miles) to Porter Square	0.1	0.2	0.2	0.4	

Note - FAR for 2072 Mass Ave is from Email response from Mr. Korb dated 9/20/2020

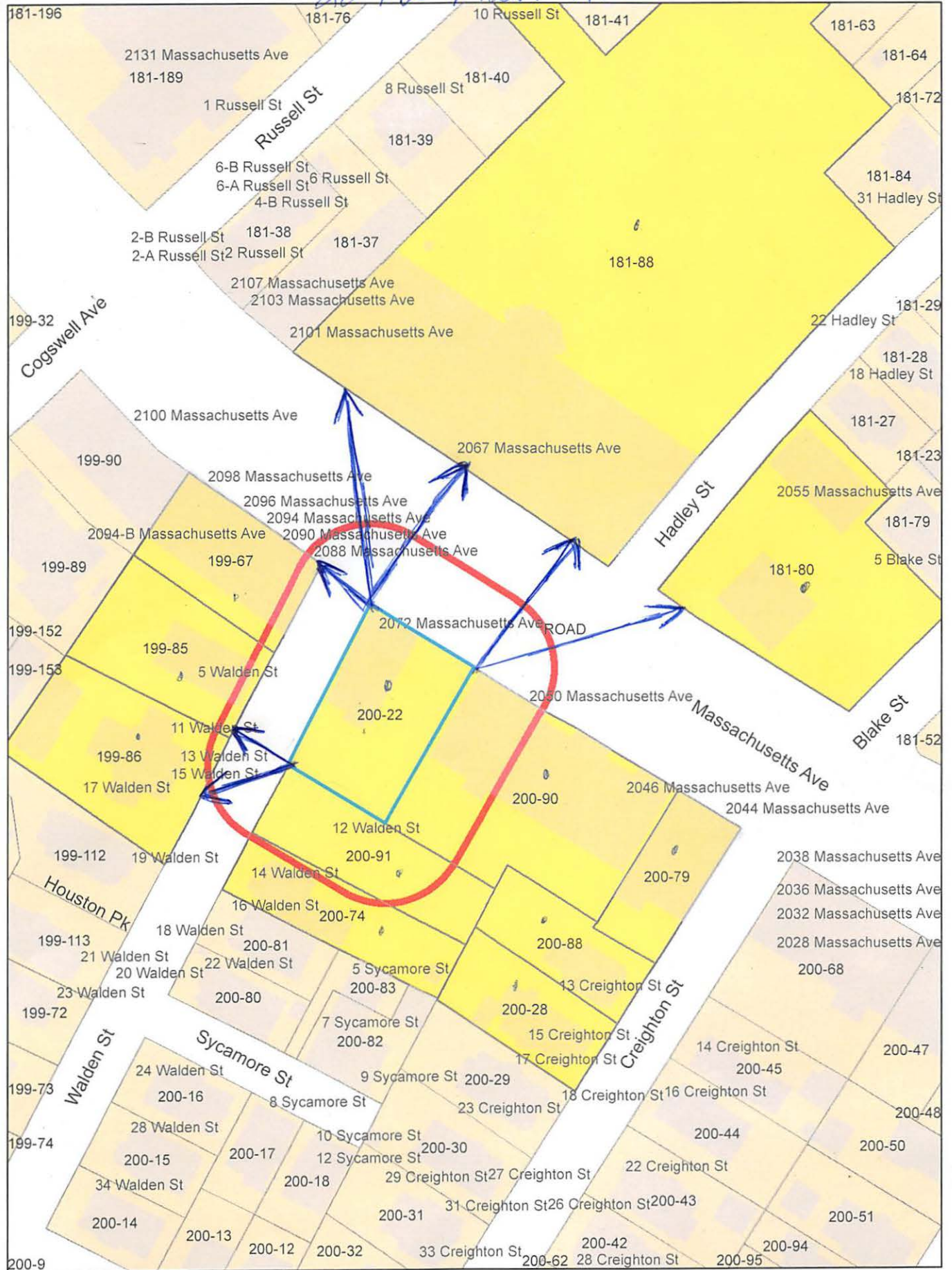
2072 Mass Ave (Proposed) 8,515	Total incl 2072 Mass Ave 68,402	Increase above existing
--------------------------------------	--	-------------------------------

9/6		
57391	185,482	44.8%
6.74	2.71	315.1%
49	182	36.8%

3 HP; 2 drop off

0.3

2072 Mass Ave



2072 Mass Ave

Petitioner

181-80
CHRISTOS POUTAHIDIS MANAGEMENT, LLC.
C/O HESS REALTY LLC, PROPERTY TAX DEPT.
539 SOUTH MAIN ST
FINDLAY, OH 45840

200-22
CC HRE 2072 MASS AVE LLC
C/O CAPSTONE COMMUNITIES LLC
PO BOX 610083
NEWTON HIGHLANDS, MA 02461

HRE 2072 MASS AVE LLC
C/O HOPE REAL ESTATE ENTERPRISES LLC
ATTN: SEAN D. HOPE
907 MASS AVE, SUITE 300
CAMBRIDGE, MA 02139

200-90
CAMBRIDGE HOUSING AUTHORITY
675 MASSACHUSETTS AVE
CAMBRIDGE, MA 02139

181-88
HENDERSON CARRIAGE LIMITED PARTNERSHIP
C/O H.J. DAVIS DEVELOPMENT TRUST
125 HIGH STREET 21ST FL
BOSTON, MA 02110

CAPSTONE 2072 MASS AVE LLC
C/O CAPSTONE COMMUNITIES LLC
1087 BEACON STREET, SUITE 302
NEWTON, MA 02459
ATTN: JACOB KORB

200-91
CAMBRIDGE CITY OF COMMUNITY DEV
57 INMANN ST
CAMBRIDGE, MA 02139

200-91
CITY OF CAMBRIDGE
C/O NANCY GLOWA
CITY SOLICITOR

200-91
CITY OF CAMBRIDGE
C/O LOUIS DEPASQUALE
CITY MANAGER

199-85
STROUD, MARY F.
5 WALDEN ST., UNIT # 6
CAMBRIDGE, MA 02139

199-85
LI, XINGFANG
5 WALDEN ST., #3
CAMBRIDGE, MA 02140

199-85
LIN, PEI-YU
5 WALDEN ST., UNIT #5
CAMBRIDGE, MA 02140

199-67
COLANNINO, JOSEPH A., FRANCIS X.,
ROBERT ANTHONY J., MARIAN L. & MARIA C.
C/O RIVERSIDE MANAGEMENT
P.O.BOX 440317
W.SOMERVILLE, MA 02144

199-85
LEE, TIEN-YI
4 IVY STREET
CAMBRIDGE, MA 02138

199-85
CAMBRIDGE AFFORDABLE HOUSING
675 MASS AVE
362 GREEN STREET
CAMBRIDGE, MA 02139-3306

199-86
FOLEY, MARIA HOTTELET
17 WALDEN ST
CAMBRIDGE, MA 02140

199-86
RUBIO, ELAISA E. & ELAISA S. RUBIO
15 WALDEN ST
CAMBRIDGE, MA 02140

199-86
HYDE, SAMUEL MARCELLA HYDE
11 WALDEN ST UNIT #13
CAMBRIDGE, MA 02140

199-86
EGAN, MARYBETH L.
C/O MARYBETH L. EGAN
11 WALDEN ST
CAMBRIDGE, MA 02140

199-85
DOCHOW, CYRUS AIDAN
JENNIFER YESSUE CHUONG
5 WALDEN ST UNIT 8
CAMBRIDGE, MA 02140

199-85
JAS HOMEOWNERSHIP LLC,
ATN: JUST A START CORPORATION
1035 CAMBRIDGE STREET#12
CAMBRIDGE, MA 02141

199-85
KWAKU, INGER MARIE & KEVIN FORREST
43 STEVENS RD
HANOVER, NH 03750

200-79
FITZSIMONS, CHRISTOPHER
TR. THE 2046 MASS AVE. REALTY TRUST
17 REYNOLDS STREET
NORTH EASTON, MA 02356

200-28
MCMANUS, HUGH L. & LISA M. MCMANUS
17 CREIGHTON STREET
CAMBRIDGE, MA 02140

200-74
KEBEDE, MEKONNEN & ALMAZ ABEBE
14 WALDEN ST
CAMBRIDGE, MA 02140

200-88
MCINTOSH, LINDA G.
TRUSTEE OF THE LINDA MCINTOSH 2017 TRUST
80 PARK ST UNIT #72
BROOKLINE, MA 02446

Pacheco, Maria

From: SUSAN E FRANKLE <susanfrankle@comcast.net>
Sent: Monday, December 7, 2020 12:39 PM
To: Pacheco, Maria
Cc: Carlone, Dennis; Nolan, Patricia; north-walden-neighbors@googlegroups.com; Mitzi Fennel; Singanayagam, Ranjit; Barr, Joseph; Baxter, Patrick; Farooq, Iram; City Manager
Subject: CASE NO. BZA-017326-2020 - 2072 Mass. Ave. Proposed Development
Attachments: 2072 Mass Ave Objectives v3.pdf

To the Cambridge Board of Zoning Appeals:

We, the undersigned, all Cambridge residents who live in close proximity to the intersection of Mass. Ave. and Walden Street, are writing to you in regard to the Proposed Development of 2072 Mass. Ave. CASE NO. BZA-017326-2020. The signatories to this letter represent a broad coalition of the North Walden Neighbors leadership team, neighbors from Creighton Street, and the Russell Apartments. We are long-term neighborhood residents, homeowners, and affordable housing residents.

We welcome affordable housing in our community.

We are supportive of the 2072 Mass. Ave. site being developed for affordable housing and applaud the mission of developers who build affordable housing units in Cambridge. There is an urgent need in Cambridge for affordable housing and we are committed to helping the City further this mission.

But the project is fundamentally flawed.

SIZE: We have significant concerns about the height, density, and footprint of the proposed building, particularly in light of the small lot size (8,514 square feet), the lack of setback, the community context, and the dangerous, congested intersection that is the corner on which this site is located. The building is dramatically out of scale given the lot size and context of the neighboring structures.

TRAFFIC: We have experienced, especially pre-pandemic, safety issues that exist for pedestrians, bicyclists, and vehicles at this corner and in the surrounding streets. The City is well aware of these issues, having hosted a community meeting in February 2020 to listen to neighborhood concerns during which the City voiced a commitment to solve these issues. Additionally, there is a plan by Capstone to widen Walden leading into the corner. However, there is no scientific evidence that traffic and safety will be improved due to the extra width. There has been no objective, comprehensive study of the actual traffic measures to accommodate this project.

VULNERABLE ABUTTERS: Finally, and importantly, the proposed building directly abuts Russell Apartments, an affordable housing community of vulnerable residents including seniors and people with disabilities; and also houses the North Cambridge Senior Center. Having this vulnerable population next door creates additional challenges that must be considered to protect the health and well-being of these residents. The City has not incorporated appropriate measures to safeguard one of the area's most precious, valued, and respected City institutions.

We believe these important issues have not been adequately addressed to date, and the community consultation around this project has been incomplete and rushed (many nearby residents are only

now finding out about the proposed development). As a result, community members have strong concerns and opposition to certain aspects of the project plans which the Board of Zoning Appeals (BZA) will consider in its December 10 meeting.

We request that the BZA require a Project Review, as required by Zoning Ordinance Article 19, to allow sufficient analysis, consultation and co-design of the project plans to benefit both new residents and the existing neighborhood.

Every great urban project requires careful planning and coordination.

The attached Objectives Document outlines our specific requests. These objectives, once acted upon, will help mitigate the safety issues related to the location and size of the proposed development.

We urge the BZA to require adequate review of this project. We look forward to working cooperatively with the City of Cambridge and Capstone Communities to ensure that the development is a true win-win for the new residents who will live there, as well as the larger community.

Respectfully yours,

Christin Cleary, 51 Creighton Street
Bryan Cook, 23 Creighton Street
Lisa Dreier, 38 Cogswell Avenue
Mitzi Fennel, 37 Creighton Street
Pauline Fennel, 35 Creighton Street
JaneAnn Fisher, 16 Creighton Street
Susan Frankle, 3 Houston Park (19 Rear Walden)
Cheryl Gault, 47 Creighton Street
Doug Gault, 47 Creighton Street
Kristen Graves, 28 Creighton Street
Anna Jeffers, 37 Creighton Street
Douglas Jeffers, 37 Creighton Street
Peter Katz, 41 Creighton Street
Ailish Keating, 41 Creighton Street
Seymour Kellerman, 21 Cogswell Avenue
Michael Kennedy, 8-B Cogswell Avenue
Elizabeth Kenney, 33 Creighton Street
Elizabeth Kon, 23 Creighton Street
A. M. Kubilius, 21 Cogswell Avenue
Hugh McManus, 17 Creighton Street
Lisa McManus, 17 Creighton Street
Lucie Prinz, 31 Creighton Street
Margaret B. Rueter, Russell Apartments, 2050 Mass Avenue, #210
Lou Soltys, 26 Creighton Street
Lein Tung, 28 Creighton Street
John Uzzolino, 3 Houston Park (19 Rear Walden)
Gatewood West, 63 Creighton Street
Merry White, 6 Cypress Street

Objectives: 2072 Mass Ave Proposed Development

**Representing Consensus of North Walden Neighbors Leadership and Representatives from
Creighton Street and Russell Apartments**

1. TRAFFIC / PARKING

Require full compliance with Article 19 of Zoning Ordinance, specifically:

- **Project Review Special Permit (19.20): Traffic Impact Review and Urban Design Review**
- **Citywide Urban Design Objectives (19.30)**
responsive to the existing or anticipated pattern of development
pedestrian and bicycle-friendly, with a positive relationship to its surroundings
mitigate adverse environmental impacts of a development upon its neighbors
enhance the urban aspects of Cambridge as it has developed historically.
Enhancement and expansion of open space amenities in the city
- **Development Consultation Procedures (19.40) to provide**
“the opportunity for City staff and the general public (1) to review and comment on
development proposals prior to the formulation of final plans and before the issuance of a
building permit and (2) to determine compliance with the zoning requirements” and conduct
Large Project Review

Provide at least 0.5 offsite and/or onsite parking spaces per unit

- Mitigate the significant impact on streets in the immediate area
- Provide specifics as to location to be determined by the above requested Traffic Impact Review

2. SIZE OF BUILDING

Reduce building size

- Reduce Mass Ave-facing height to 6 stories in accordance with the Affordable Housing Overlay provisions for building height and with input from the Project Review conducted under Article 19
- Reduce the Walden side of the building to 3 stories to reduce impacts on Russell Apartments and 5 Walden as well as other adjoining neighbors

Reduce building footprint to allow setbacks and provide green space

- Setbacks per zoning ordinance: 10' on Walden St and to City parking lot
- Developer provides detailed professional landscape plan before construction
- Green space to protect Russell Apartment residents and North Cambridge Senior Center visitors

Change composition of units to serve neediest families

- Increase the percentage of units for the lowest income families from 16% to 25%

3. CONSTRUCTION MITIGATION

Develop a construction mitigation plan with abutters to include:

- On-site rep with cell phone who can respond immediately to resident concerns
- Community liaison and regular communication
- Community review of construction staging and construction schedule
- Provide parking for project workers
- Noise abatement
- Provide air conditioners and air filters for Russell Apartment residents and payment for extra electricity, to shield residents from construction dust and fumes
- No work on weekends
- Compensation plan for any and all damage to surrounding structures
- Compensation for residents who will be moved
- Sound-proofing windows and other sound-proofing mitigation measures as needed for abutting properties
- Third-party safety supervisor
- Conformity to demolition and construction regulations
- No smoking on site
- Plan for rats and rodent control
- Clean site at end of work day

4. COMMUNITY AMENITIES

Provide additional safety measures and neighborhood enhancements to include:

- Traffic calming on Walden St., Mead St., and Cogswell Ave.
- Street improvements for Walden, Cogswell, Sycamore, Cypress, and King: repaving, sidewalk maintenance, trees
- Green median and trees on Mass Ave from Regent to Rindge
- Crosswalk and traffic box at the end of Cogswell and Mass Ave
- Added services for elderly in Russell Apartments and Senior Center
- Improvements to permit parking lot on Walden Street, such as repaving with arable top, gardening plots at borders, stacked wall of solar panels, bike rack, and electric power charging stations

December 6, 2020
Cambridge Planning Board
344 Broadway, Third Floor
Cambridge, MA 02139

Re: 2072 Massachusetts Avenue Comprehensive Permit

Dear Members of the Planning Board,

This letter is written on behalf of Creighton Street residents regarding the proposed development at 2072 Massachusetts Avenue. Creighton Street is located one block southeast of the development site.

First, to be clear, we are unanimous in our strong support of affordable housing on the proposed site. We are heartened that it is 100% affordable, appreciate many aspects of the design, and believe in the good will of the developers. Our concerns are strictly a question of scale and the resulting impact to residents of the area. We appreciate that some zoning restrictions may on occasion need to be relaxed in the interest of providing housing access, and also appreciate the need for due diligence in making intelligent choices and exceptions. These building decisions are not reversible.

This communication is focused on parking issues. Other issues, addressed in concert with the North Walden Neighbors group, also have the support of many Creighton residents.

Our objective with this letter is to see provision made for resident vehicle parking. We understand that zoning specifies a 1.0 space per unit requirement for new construction, but we accept the developers' estimates of the number of resident vehicles, which would limit the need to .5 space per unit. We believe that a parking solution can be found without jeopardizing the project.

Below is our reasoning regarding the parking issues.

We understand that Article 19 of the Zoning Ordinance was not strictly followed, and we believe that the parking study commissioned by Capstone was insufficient in the following areas:

- It was limited to a census count of available spaces on a single day during the pandemic, which does not reflect an average situation, or post-pandemic realities.
- It did not take into account unique geographical considerations of the adjacent dead-end streets: Creighton Street, Regent Street, and Porter Road (see details below).
- The study's count differs from that taken by a resident on the same date and time (October 20 at 6:00 p.m.); the study showed 8 spaces while the resident counted only 4.
- The resident conducted a count showing 5 out of 10 mornings with no spaces available (reflecting the lack of overnight capacity).

Other relevant issues:

Creighton Street (along with Regent Street and Porter Road) challenges:

- It is a single block long, only one lane, and is bounded by the commuter rail tracks; if parking can't be found, one can't just continue to the next street, but rather has to either back out or turn around to exit Creighton.

- It is used for commercial parking by those visiting stores and restaurants on Mass Ave.
- It has traffic issues with those looking for parking or reversing direction on Mass Ave, then traveling the length of the street and turning around in driveways or at the end of the street.

Creighton Street is a logical first place for residents of the new construction to seek parking:

- It is the next street closest to the building and the easiest to access coming from Walden or Massachusetts Ave.
- Residents will likely prefer not to cross the busy Walden/Massachusetts Avenue intersection.
- Creighton Street will fill up every day, with spillover filling up Regent Street and Porter Road.
- Regardless of whether the developers count is correct, all available spaces would be filled by the, possibly underestimated, 25 building residents with cars.

Capacity:

- Historically, Creighton Street has had very few to no spaces in the evenings. Currently, two 3-family homes have been vacant while undergoing renovations, so there has been less of a challenge.
- Prior to these homes being vacant, residents had difficulty parking upon return from work or going out in the evening, on street cleaning days, and when accumulated snow reduced the number of spaces.

Being able to park is a quality of life issue:

- Creighton Street has a high number of senior residents and families with young children; parking blocks away from home is not reasonable.
- If no parking is available on Creighton and the next 2 streets, residents will have to drive at least ½ mile in a circle to get back to the neighborhood, due to the layout of dead-end streets.
- Many residents commute to job locations that are not accessible by public transportation and senior residents with mobility issues rely on cars.

The developers have researched onsite parking stackers and have determined that they can accommodate 11 cars onsite at 2072 Mass Ave; they are also willing to lease off-site parking spaces.

We believe that we can work together to make this project an outstanding success, that much needed housing can be provided without impacting the quality of life for those on Creighton Street and in the neighborhood.

Sincerely,

Ailish Keating

Doug Gault

John Grady

Lucie Prinz

Anna Jeffers

Douglas Jeffers

Kamran Rastegar

Mitzi Fennel

Bryan Cook

Elizabeth Kenney

Kristen Graves

Pauline Fennel

Cheryl Gault

Elizabeth Kon

Lein Tung

Peter Katz

Christin Cleary

Gatewood West

Lisa McManus

Tesair Lauve

Christine Bustany

Hugh McManus

Lou Soltys

Pacheco, Maria

From: Ausra Kubilius <ausmkub@gmail.com>
Sent: Monday, December 7, 2020 12:30 PM
To: Pacheco, Maria
Subject: dangerous project--action needed

Dear Maria Pacheco, Board of Zoning Appeals:

Please help avoid more deaths and accidents at the notoriously dangerous intersection of Mass Ave and Walden St in North Cambridge.

As you know, an affordable housing high-rise--to house many children--has been proposed for 2072 Mass Ave at this corner. **And no Traffic Impact Study has been done.** Without any evidence, the developers claim their widening of Walden St will solve the problem.

Please help ensure that the Zoning Board of Appeals, meeting on Dec. 10, honors Article 19 and requires a full Traffic Impact Review and Study.

Also, the proposed looming project abuts (only 4 feet away) the Russell affordable housing apartments for seniors/disabled and the North Cambridge Senior Center. The safety (and access to sunshine) of us seniors must be taken into account. Again, please enforce all provisions of Article 19.

Many, many, many Cambridge neighbors and commuters are concerned about this issue.

Thank you, A.M. Kubilius (close neighbor at 21 Cogswell Ave)

PS: Our North Cambridge Neighbor group's petition for action from the BZA/city government has already been signed by over 200--and counting.

Pacheco, Maria

From: Cyrus Dochow <cyrus.dochow@gmail.com>
Sent: Monday, December 7, 2020 4:31 PM
To: Pacheco, Maria; Daglian, Sisia
Subject: 2072 Mass Ave Comp Permit—CASE NO. BZA-017326-2020 Comments
Attachments: BZA-017326-2020-5WaldenComments.pdf

Dear Members of the Zoning Board,

Attached is a letter expressing concerns regarding the proposed project at 2072 Mass Ave (BZA-017326-2020). This letter is prepared on behalf of a majority of the Condo Association at 5 Walden Street (a direct abutter).

Thank you for your consideration.

Cyrus Dochow
Trustee, 5 Walden Condo Association

5 Walden Condominium Association
Cyrus Dochow, Trustee
5 Walden Street, Apt. 8
Cambridge, MA 02140

December 7, 2020

Zoning Board of Appeal
Inspectional Services-Zoning
831 Massachusetts Ave.
Cambridge, MA 02139

Re: 2072 Massachusetts Avenue Comprehensive Permit—CASE NO. BZA-017326-2020

Dear Members of the Zoning Board,

I write to you on behalf of the 5 Walden Street Condominium Association to raise concerns about the height of the proposed development at 2072 Massachusetts Avenue as it meets the residential buildings of Walden Street. We are direct abutters to the development site, located immediately across Walden Street. Although we support the many positive impacts of the project, we strongly urge the Zoning Board to require that the development team reduce the height of the building along the Walden portion from the proposed six-stories by at least one-story as a condition of approval.

The proposed six-story portion along Walden abruptly meets the surrounding low-lying residential buildings and narrow public way, negatively impacting the transition from Mass Ave to Walden, both at ground level and from a distance. We refer to the following:

- The existing adjacent buildings along Walden (5 and 14 Walden) are three and two stories (approximately 32 and 25 feet tall respectively). The six-story (~70 ft) portion is greater than double the height of the neighboring residential buildings (figs. 1 & 2)
- The width of the Walden public way is 40 feet, less than half the width of Mass Ave (98 feet). The narrow width will further contribute to the perception of excess height along Walden (figs. 1 & 2).
- From a distance (corner of Mead and Walden), the six-story portion exceeds the top of the tree canopy, contributing to the perception of excessive bulk and reinforcing the insufficient transition from Mass Ave to Walden.

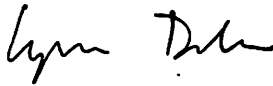
To date there has been little attention given to the urban design of the six-story portion of the building. The CDD memo dated November 25, 2020 places due emphasis on the positive urban design impact of the development on Mass Ave, but offers few details on how the building addresses the adjoining Walden context. Similarly focused, the discussion of the Planning Board at last week's public hearing failed to scrutinize the proposal's dimensional relationships to Walden and the scale of the surrounding two and three-story buildings.

The negative impacts of the six-story portion of the proposal can be remedied with modest changes to the height. A reduction of one floor (~11 ft) would result in two fewer units; a reduction of two floors (~22 ft) would result in four fewer units. Reducing the six-story portion by one or two floors would not require a wholesale redesign of the project, nor would it require the dramatic reductions to the overall unit count associated with a building designed in accordance with the Affordable Housing Overlay. This more substantial step-down from Mass Ave would have a positive and permanent impact for all who visit and use Walden on a daily basis (including the future residents of the proposed development).

We support the many positive aspects of the project: the major contribution to affordable housing stock, widening of Walden to accommodate three lanes of traffic, the two foot easement along Walden, sustainability goals, and the major contributions to the Mass Ave streetscape. We are grateful for the professionalism with which the development team has engaged our group and look forward to continue working with them toward a project that addresses the scale of the Walden neighborhood.

We urge the Zoning Board to employ their expertise to evaluate the project as it meets Walden and to exercise their authority to compel the development team to reduce the height of the six-story portion to address the low-lying residential scale of the immediate surroundings. This pragmatic revision would improve the urban design of the project and restore faith in the public review process.

Sincerely,

A handwritten signature in black ink, appearing to read "Cyrus Dochow".

Cyrus Dochow
Registered Architect
Trustee, owner Unit 8

Cosigned by owners of 5 Walden:

Inger Kwaku, Trustee, Unit 2
Marie Stroud, Unit 6
Pei-yu Lin, Unit 5
Tien-Yi Lee, Unit 4
Dennis Li, Unit 3

Attachment: figs. 1 & 2, cross-sections through Walden Street and Mass Ave

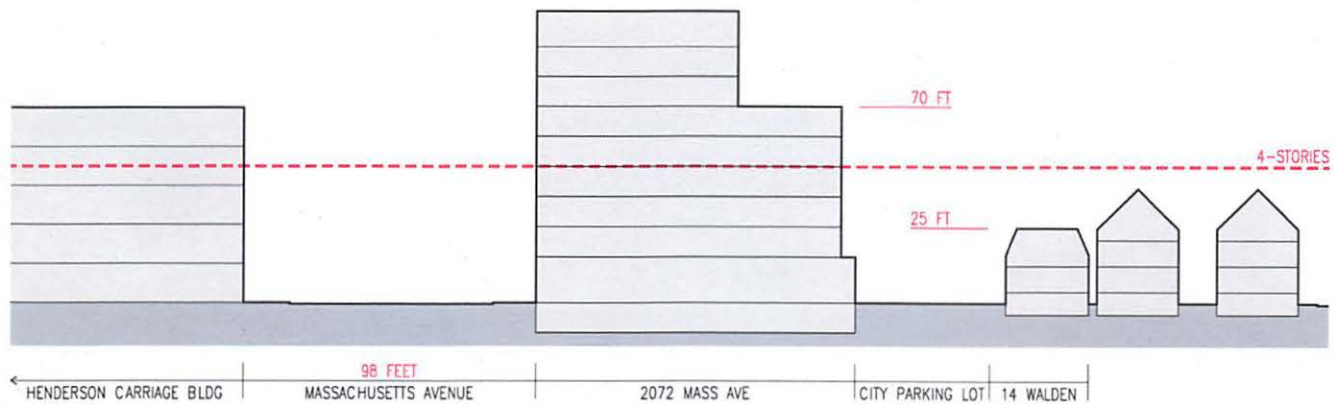


FIG. 2
MASSACHUSETTS AVENUE SECTION

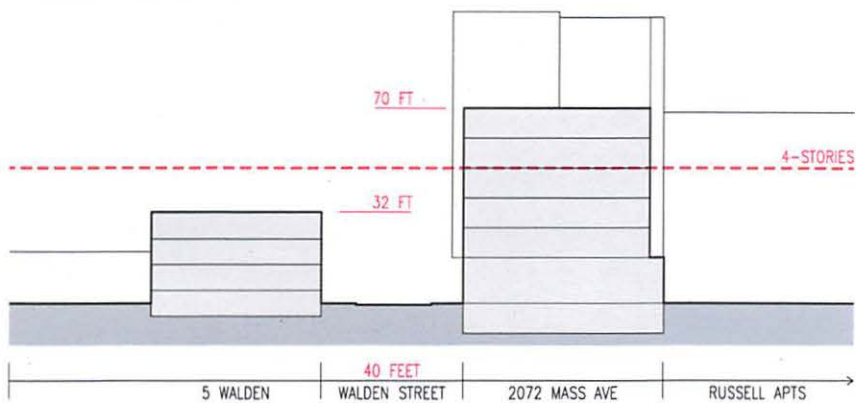


FIG. 1
WALDEN STREET SECTION

0 10 50 FT

2072 MASSACHUSETTS AVENUE
CASE NO. BZA-017326-2020

PREPARED BY CYRUS DOCHOW ARCHITECT
5 WALDEN STREET, APT. 8 CAMBRIDGE MA 02140

Pacheco, Maria

From: mariestroud@verizon.net
Sent: Monday, December 7, 2020 4:41 PM
To: Pacheco, Maria
Subject: BZA Case No. 017326-2020 - 2072 Mass Avenue Permit

December 7, 2020

Zoning Board of Appeal
Inspectional Services - Zoning
831 Massachusetts Avenue
Cambridge, MA 02139

To the Members of the Zoning Board:

I am writing to reiterate my support for the letter written by architect and trustee Cyrus Dochow on behalf of the 5 Walden Street Condominium Association in regards to the 2072 Massachusetts Avenue project. Although his suggested proposal of altering the Walden Street height would diminish the intended number of affordable units from 49 to 47 or 45 units in a time of dire need, I compel you to give it full your attention.

While this proposal does not satisfy the needs of all, it is intended, on balance, as a compromise in the interests of the common good. Importantly too, it would foster a more positive relationship between the CDD, the developers and the members of our neighborhood, and in turn, all citizens of Cambridge.

I encourage the Zoning Board to give this proposal serious scrutiny and hope you will see it for what it is - a practical and elegant solution to a complex problem in urban planning and design.

Sincerely,

Marie Stroud
5 Walden Street, Unit 6
Cambridge, MA 02140

Pacheco, Maria

From: Merry White <corkela2@gmail.com>
Sent: Monday, December 7, 2020 4:53 PM
To: Pacheco, Maria; Carlone, Dennis; Nolan, Patricia; north-walden-neighbors@googlegroups.com; Mitzi Fennel; Singanayagam, Ranjit; Barr, Joseph; Baxter, Patrick; Farooq, Iram; City Manager
Subject: To the City of Cambridge and its Agencies

Dear all,

I'm a very long term resident - we moved here when I was 12 in 1953. I have lived in many of Cambridge's neighborhoods but for the last 30-plus years have lived at 6 Cypress Street, a cul-de-sac off Walden Street between Mass. Ave and the railroad bridge. My son and daughter grew up here, and felt the neighborliness and safety of the block.

I love it here. I love my neighbors, I love the community, I love walking in our diverse streets and taking in minute changes as the seasons go by - even in Covid-19 time, it is a place of peace and comfort.

That comfort is about to be seriously damaged, and for the rest of my life here. The project to build a 9-story building on the corner of Mass. Ave and Walden is a dramatic slap in the face to the neighborhood. We all want affordable housing - and are very hurt when promoters of the project choose to call us NIMBYs or elitists or even, a few times, racists. We want it, and we want it here but we want it in context.

My concerns are many and you will have heard them from other neighbors too. I am worried about the density and height of the building, concerned that the building is planned to go right to the edge of the plot without set-backs. I just took a walk around the neighborhood as I do every day, and at sunset came to the top of the railroad bridge and saw the sun hit the Henderson Carriage Building on Mass. Ave, illuminating brightly the red brick against the darkening sky. Wow. And I realized, if that building goes up, it will block that view.

A view is not the most important thing in the world but it stands for an imposition, a dangerous one too in this case as the traffic on Walden, now a speeding thoroughfare from West Cambridge to Mass Ave - except at rush hour when it stalls back to Huron - is dangerous and has been investigated and found to be so by the City. The corner at Mass Ave will, I hear, be given some "relief" in the form of a few feet more to make the lanes larger - but only for the length of the proposed building - not going farther back which of course is impossible.

I am also seriously concerned for the residents of Russell Apartments, adjoining the proposed building with only three-four feet between them. The residents recently returned after about two years of absence while their building was remodeled and now, many will be forced to leave again or suffer noise and dust and worse. And when it is done, lose the little sunlight they have in their small patio and garden in the rear, the only place of outdoor recreation for many in the building.

I understand that the project is requesting up to 18 waivers of building code and other "exemptions" to regulations usually required. One is a Project Review which I would strongly recommend, before the whole project advances. In a recent Planning Board meeting, I and my attending neighbors felt unheard as we requested this and other measures to make the building and the neighborhood fit - and safe. There is much more to say, but above all, please consider closer coordination with those who will experience a great impact from the building's construction, its neighbors.

I look forward to hearing from, and to working with, you. Thank you for your consideration.

Sincerely,

Merry White
6 Cypress St
Cambridge MA 02140

7

Pacheco, Maria

From: Rosemary Mosco <rmosco@gmail.com>
Sent: Monday, December 7, 2020 4:54 PM
To: Pacheco, Maria
Subject: Support for 2072 Mass Ave

Dear Ms. Pacheco,

I'm a Cambridge resident (I live at 120 Rindge Ave) and I'm writing to voice my support for the nearby 49-unit affordable housing project at 2072 Mass Ave. I have lived in Cambridge and Somerville for many years, and though I have off-street parking options now, I have never found myself unable to find parking. Instead, I find myself dismayed that my neighborhood is so economically unfriendly. Projects like this will make my community much stronger.

Thanks so much,
-Rosemary Mosco

Pacheco, Maria

From: James in Cambridge <tompaine@hotmail.com>
Sent: Monday, December 7, 2020 4:58 PM
To: Pacheco, Maria
Cc: Singanayagam, Ranjit
Subject: Comments on the 2072 Mass. Ave. Project: CASE NO. BZA-017326-2020

December 7, 2020

Re: 2072 Mass. Ave. - CASE NO. BZA-017326-2020

Dear BZA Members:

If you haven't already, please be sure to listen to the comments of Dennis Carlone, which come near the end of the public comment portion of the video of the Planning Board review of this BZA Case. (Approx. -50 mins.)

[See: <https://cambridgema.ig2.com/Citizens/SplitView.aspx?Mode=Video&MeetingID=2768&Format=Agenda>]

These comments were very good, I thought, and summarize well a body of good argument for a far more reasonable approach to permitting an appropriately scaled affordable housing project at 2072 Mass. Ave.

The zoning and planning documents for this part of Cambridge call for a building of significantly more modest height and scale. The desire to support affordable housing is real, and shared by neighbors, but should not "trump" all other important considerations at this location, which also, incidentally, will seriously affect the people who currently live in the affordable housing right next door at the Russell Apartments.

A building of 4 or 5 storeys in the rear, and an ample 6 or 7 storeys in the front along Mass. Ave. would be a significant and handsome building which would be future-looking in scale, fit better in this context and location, and be welcomed happily by neighbors and the neighborhood and community.

The applicant will still receive many advantages, e.g., Russell Apts. actually have 9 parking spaces for a similar number of units. (What will happen to their small, pleasant, rear patio?)

We can do significant affordable housing here we can feel good about without having to play, "Beggars neighbor."

Thanks for being reasonable and just, to all.

Sincerely, James Williamson
1000 Jackson Place
(Jefferson Park)
Cambridge, MA. 02140

Pacheco, Maria

From: Young Kim <ycknorris@gmail.com>
Sent: Monday, December 7, 2020 4:58 PM
To: Pacheco, Maria
Cc: Singanayagam, Ranjit; Siddiqui, Sumbul; Mallon, Alanna; Carlone, Dennis; McGovern, Marc; Nolan, Patricia; Simmons, Denise; Sobrinho-Wheeler, Jivan; Zondervan, Quinton; Toomey, Tim; Barr, Joseph; City Clerk; DePasquale, Louie
Subject: 2072 Mass. Ave. – CASE NO. BZA-017326-2020
Attachments: email to Commissioner Rajit et al.pdf; 2072 Mass Ave AHD Submit.pdf

Dear Chairman Alexander and Members of Board of Zoning Appeal:

I am a long time resident of North Cambridge and I am writing in opposition to Case No. BZA-017326-2020 2072 Mass Ave. Comprehensive Permit application by CC HRE 2072 MASS AVE TENANT LLC (CC HRE). I submit to you the attached presentation package to present our case on behalf of North Walden Neighbors, a neighborhood group with more than 220 people, many in close proximity to the proposed site, who have signed on with concerns (petition was submitted to you via separate email).

I am also attaching the email I sent to Assistant City Manager Farooq and Commissioner Singanayagam asking them to reschedule the Dec. 10 BZA hearing of this case and direct the applicant, CC HRE, to submit revised documents to the Planning Board (PB) in accordance with established data submittal guidelines. I requested that PB should rehear this case after all the City had the time to review the revised documents and resubmit their updated memos and the public and PB members had adequate time to review the matter. Finally, I urged that a project that exceeds the threshold of **Article 19 Project Review Special Permit** by the PB by more than factor of 2.5 (57391 sq. ft, living space waiver requested for the 20,000 sq. ft. threshold) **should NOT be granted a waiver** even for a "friendly 40B". If this waiver is granted for this project, I shudder to think where this will lead to. I discussed this email with Commissioner Singanayagam today, 12/7/2020, and he assured me that he will make sure the Board will receive a copy of it.

I would like to request that you **do not accept the PB's favorable recommendation** because the CC HRE plans that were presented at the 12/1/2020 PB hearing was not what they had submitted for the hearing; rather, it was a modified version for which they had not provided any supplemental data before the hearing. So, the plan on record for that hearing was the original 8 story building whereas the case before you is the amended version of 9 story facing Mass Ave with 6 story facing Walden neighborhood. What guarantee does anyone have, including yourselves, that there aren't any new changes since the PB hearing? If you let the PB recommendation stand, you will be setting a dangerous precedence of allowing developers to present conflicting, undocumented changes to their application material to PB and BZA hearings and other agencies per requirements of 40b Comprehensive Permit regulations. Clearly, the developer is rushing this process and creating confusion for everyone involved and may lead to lasting unforeseen/unintended consequences to the community.

I would also like to ask you to consider a fundamental question of **the motive behind this application**. If it is purely to **provide critically needed affordable housing**, which I do not deny, then why wouldn't CC HRE work with the community and City to design a building that will meet that demand at the same time satisfy the needs of the community around 2072 Mass Ave? Why is 49 the magic number of units? Certainly, this project alone will not solve the need, so why is CC HRE so adamant about the number of units? Too, the city of Cambridge has already met a 10% threshold for affordable housing, so the 40b Comprehensive Permitting policy is not to be automatically granted without careful consideration. When CC HRE agreed to lower the western section of the building to 6 stories at the request from some of the neighboring property owners, they compensated that by raising the eastern section to 9 stories in order to keep the number of the units the same.

There must be a reason to keep the number of units to 49. Is that driven by **their financial viability** and if so is that worth sacrificing the needs of the community that includes the senior and disabled residents of Cambridge Housing Authority's Russell Apartments? Why must the residents of this neighborhood have to bear the brunt of this proposed 100% affordable housing development for the sake of developer's financial viability when Frost Terrace, the same CC HRE is building only ½ mile south of this site, is so much less dense with plenty of open space, and only 5 stories high that blends far better into the surrounding neighborhood rather than sticking out like a sore thumb? And when there are already 133 affordable housing units with 128,091 square feet of living space with average Floor Area Ratio (FAR) of 2.14 within ½ mile of Porter Square along or just off of Mass Ave. The proposed 2072 project will add 49 more units, a 36.8% increase, at a FAR of 6.74, more than triple the FAR of existing AHUs.

I implore you to **continue this case** until CC HRE works out a compromise plan with the community that will satisfy our valid concerns, and send this application back to the PB for a full Project Review in accordance with Article 19 in conjunction with any and all related parking and traffic issues as described in the Zoning Ordinance.

Thank you for your kind attention,

Respectfully
Young Kim
17 Norris Street

12/7/2020

Gmail - What did the Planning Board vote to recommend favorably to BZA on 12/1?



Young Kim <ycknorris@gmail.com>

What did the Planning Board vote to recommend favorably to BZA on 12/1?

Young Kim <ycknorris@gmail.com>

Thu, Dec 3, 2020 at 12:01 PM

To: "Farooq, Iram" <ifarooq@cambridgema.gov>, "Singanayagam, Ranjit" <ranjits@cambridgema.gov>
Cc: "Siddiqui, Sumbul" <ssiddiqui@cambridgema.gov>, "Mallon, Alanna" <amallon@cambridgema.gov>, "Carlone, Dennis" <dcarlone@cambridgema.gov>, "McGovern, Marc" <mmcgovern@cambridgema.gov>, "Nolan, Patricia" <pnolan@cambridgema.gov>, "Simmons, Denise" <dsimmons@cambridgema.gov>, "Sobrinho-Wheeler, Jivan" <jsobrinhowheeler@cambridgema.gov>, "Zondervan, Quinton" <qzondervan@cambridgema.gov>, "ttoomey@cambridgema.gov" <ttoomey@cambridgema.gov>, "DePasquale, Louie" <ldepasquale@cambridgema.gov>, "Glowa, Nancy" <nglowa@cambridgema.gov>, Joseph <jbarr@cambridgema.gov>, "cityclerk@cambridgema.gov" <cityclerk@cambridgema.gov>, "Paden, Liza" <lpaden@cambridgema.gov>, Swaathi <sjooseph@cambridgema.gov>

Dear Assistant City Manager Farooq and Commissioner Singanayagam,

I would like to call your attention to the Dec. 1 Planning Board (PB) hearing of the Board of Zoning Appeal (BZA) Comprehensive Case 2072 Mass Ave Project (2072 case). I would also like to beg your pardon for a rather lengthy email and ask your patience in reading it in its entirety. Then you will understand the subject line.

I should have known but I was surprised to find out that the PB hearings are now scheduled weekly which seems to be very aggressive given that the members are volunteers with full time jobs and families. I don't know how they could possibly read through all the material associated with complex agenda items such as the two cases on Dec. 1.

The 2072 CP case will have a profound impact not only on the Porter Square neighborhood but throughout the City as it sets very dangerous precedence going well beyond even the limits of the recently ordained 100% Affordable Housing Amendment. It was heard as a BZA Case and I would like to point out two major flaws in this hearing. To the PB's credit, I must thank them for allowing public comments albeit at a reduced time limit of 2 minutes. The public comment lasted about an hour but the debate and decision took less than half that.

In the past, it was my understanding that the public comments and developer's material have to be in by close of business the Thursday before the hearing. However, because the City offices were closed for Thanksgiving holiday, the comments were due by close of business on Wed (which PB couldn't have seen anyways since the offices were closed) with additional comments accepted on Monday through noon on the day of the hearing. Getting this submittal schedule was very difficult and I am sure the Board members didn't have adequate time to review all the comments we submitted. I want to thank the staff for their tireless work in assembling all the comments we sent in and getting them to the PB members.

In particular, I submitted a slide presentation package on behalf of the North Walden Neighbors to voice the concerns for/by the senior and disabled residents of Russell Apt and to make a case why the 2072 Mass Ave project does not fit in that neighborhood. We were very disappointed that we could not even make a request to pool our time to present the case more cohesively because we could not raise our hands.

But the most alarming flaw was that the Comprehensive Permit - 2072 Massachusetts Avenue - [Materials](https://www.cambridgema.gov/CDD/zoninganddevelopment/planningboard/planningboardmeetings) in the Planning Board's Meeting table (<https://www.cambridgema.gov/CDD/zoninganddevelopment/planningboard/planningboardmeetings>) was a link to Comprehensive Permit Application received by the City Clerk on Nov 12 which stated: DIMENSIONAL INFORMATION Project Address; 2072 Massachusetts Avenue Building Height(s) (Ft) - REQUESTED CONDITIONS 8 Stories/ appx 89'8". There was no updated material available and the new design of 9 stories/102 ft. facing Mass Ave and 6 stories facing Walden neighborhood was presented to the PB and the staff only at the hearing. This would mean that all the [CDD](#), [TP+T](#), [DPW Staff Memos](#) were based on the old design. The hearing should have been called off since the developer team did not submit the most up to date material in time. Once again, we could not raise point of order as we could not raise our hands.

<https://mail.google.com/mail/u/0/?ik=26c2602198&view=pt&search=all&permmsgid=msg-a%3Ar-7590175528614077799&simpl=msg-a%3Ar-7590175528614077799>

1/2

12/7/2020

Gmail - What did the Planning Board vote to recommend favorably to BZA on 12/1?

One of the PB members said that the height of the building is not much taller than other buildings near the proposed site including the Henderson Carriage House. Perhaps he was referring to the 8 story original design. The only other building taller than 100 ft. that I am aware of near 2072 Mass Ave is 2373 Mass Ave which is 0.5 mile north of 2072 Mass Ave. Please see the attached height map as an excel spreadsheet with graphical representation of heights of various buildings we have used for comparison

In view of the failure of CC HRE to submit the revised application to the City Clerk to be distributed to the PB and BZA, there is a serious question as well as legal implications as to what the Planning Board had voted unanimously (with one absentee) to recommend favorably to the BZA. Therefore, I beg you to reschedule the Dec. 10 BZA hearing of this case and direct the applicant, CC HRE, to submit revised documents in accordance with established data submittal guidelines, not presenting them at the hearing. Furthermore, PB should rehear this case after all the City had the time to review the revised documents and resubmit their updated memos and the public and PB members had adequate time to review the material.

Going one step further, a project that exceeds the threshold of Article 19 Project Review Special Permit by the PB by more than factor of 2.5 (57391 sq. ft. living space waiver requested for 20,000 sq. ft. threshold) should NOT be granted a waiver even for a "friendly 40B". If this waiver is granted for this project, I shudder to think where this will lead to.

Thank you very much for your attention to this matter and looking forward to your response

Respectfully yours,

Young Kim

17 Norris Street

 Height Map 2072 Mass Ave.xlsx
24K

**Comments to the City of Cambridge
Board of Zoning Appeal
2072 Mass Ave Affordable Housing Project**

North Walden Neighbors
December 7, 2020

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Comparison of Proposed 2072 Mass Ave AHD with Nearby Residential Property	20
COVID-19 Traffic	21
Conclusion	23
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Data Sources

- Satellite Views from Google Maps
- GIS Maps from the Assessor's Property Database of City of Cambridge
- Property information from the Assessor's Property Database of City of Cambridge
- Additional property information and rendering from CC HRE's Community Presentation packages (<https://www.2072massaveapts.com/plans-reports>) and email exchanges with Mr. Jason Korb of Capstone Communities
- Property Condition Report for Cambridge Housing Authority Leonard J. Russell Apartments, Cambridge, MA dated January 18, 2017
- Input and comments from the neighbors of 2072 Mass Ave, including seniors from Russell Apartments

Prepared by : Young Kim

Submitted on behalf of North Walden Neighbors, a neighborhood group with more than 220 people in close proximity to the proposed site, who have signed on with concerns

North Walden Neighbors Contact : Northwaldenneighbors@gmail.com

Proposed 2072 Mass Ave Development

By many metrics including height, density, and footprint, this building is not suited to be on a small lot next to dangerous, congested intersection

- 102 Feet high – jarring drop to one story building next block
- ~150-200 occupants
- No set back from dangerous intersection
- No Green Area Open Space
- Infringes on quality of life for seniors and disabled residents at Russell Apartments



ELEVATIONS North | View from Mass Ave

Bracewell & Smith HRE

PAGE 42

2072 MASS AVE
DECEMBER 1, 2020

Purpose

- Illustrate that proposed project is too high and dense in this neighborhood and that the parking study performed by Vanasse & Associates on behalf of Capstone Communities HRE is insufficient in that it did not measure the proposed development's traffic and safety-related impact on the surrounding neighborhood in non-COVID-19 conditions
- Request Board of Zoning Appeal to require:
 - **Full Project Review** by the Planning Board (including Traffic Impact Review and Urban Design Review)
 - Requested Comprehensive permit is for "friendly 40b" project
 - Proposed plan of 57,391 sq. ft. of living space far exceeds Project Review threshold of 20,000
 - Requires more stringent review for its impact for now and for future.
 - Applicant to provide necessary documents required for the Project Review in accordance with Article 19
 - Context Study by the Applicant
 - Reduction in the size / height of the proposed development to **6 stories facing Mass Ave and 3 stories facing Walden Street** to be more in line with other 100% affordable housing developments (AHDs) in Porter Square neighborhood
 - 10 foot setback along Walden Street

Require CC HRE to Guarantee in Writing

guarantee during construction and/or in perpetuity or

- Construction mitigation plan for noise, traffic and parking associated with the construction; comfort and safety of the residents in Russell Apt during construction
- Privacy/Noise mitigation plans for privacy and noise issues posed by the windows looking into the open space and western windows of Russell Apartment
- Master Arborist/Landscape plan for the active landscaped terrace on the 6th floor roof with deep enough soil to support mature trees
- Easement guarantee that it shall not be less than what was proposed and approved by the BZA

Proposed 2072 Mass Ave Development (as Presented by Capstone at Planning Board Hearing)

Building towers over Russell Apartments and 1-story building across Walden

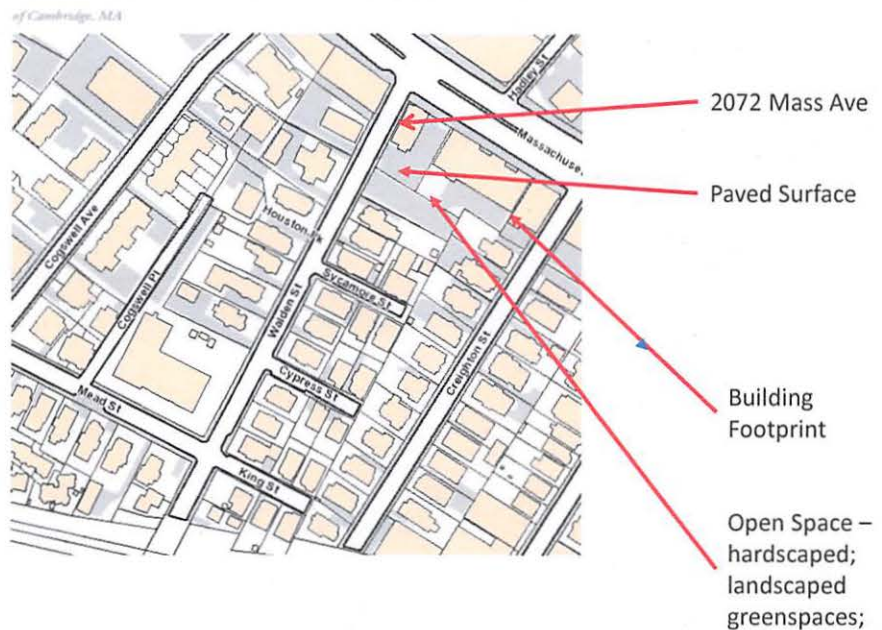
- 44 feet higher than Russell Apartments
- 89-90 feet higher than 1-story retail building on Mass Ave between Walden St and Cogswell Ave



2072 Mass Ave Neighborhood GIS Map Legend

Context: the surrounding neighborhood is comprised of residential buildings with setbacks and open space

GIS Map of the
Walden Street
neighborhood shows
residential properties
well set back from the
property lines with
abundant open spaces

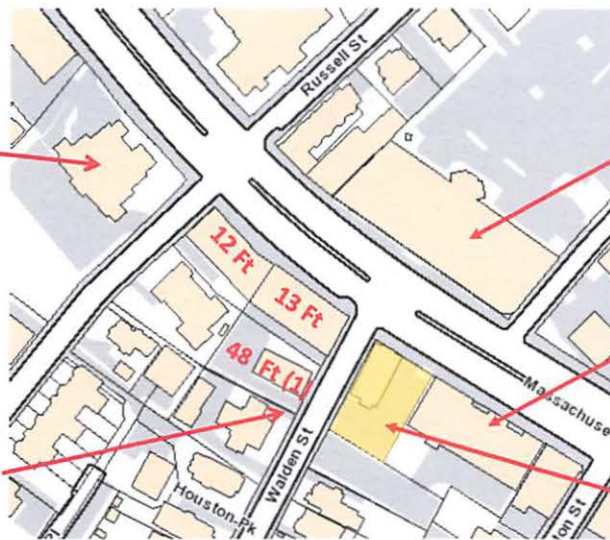


Height Map Near 2072 Mass Ave

Office/Condo
2130 Mass Ave
8 story; **96 feet**
(Note 1)

Apartment
4 Cogswell Ave
4 story; **40 feet**

Note 1 – Wall Height
not available in
Property Database;
assuming **12 ft.**

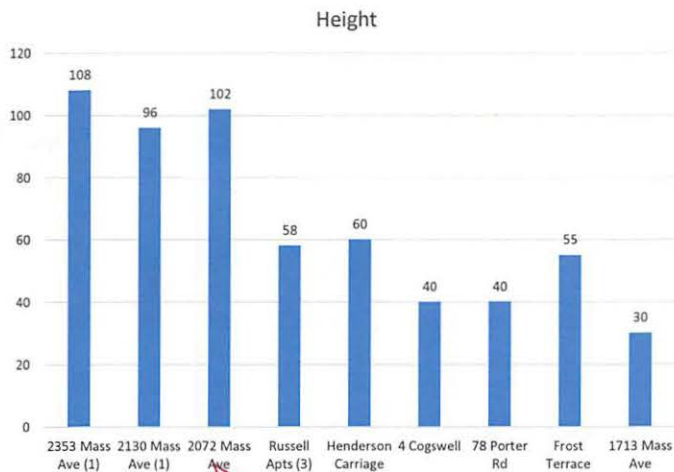


Henderson Carriage
2067 Mass Ave
5 story; **60 feet**

Russell Apartments
2050 Mass Ave
6 story; **58 feet**

Proposed 100% AHD
2072 Mass Ave
9 story Mass Ave facing;
102 ft without roof
mechanical room

Height of Relevant Buildings Near 2072 Mass Ave



Proposed Project

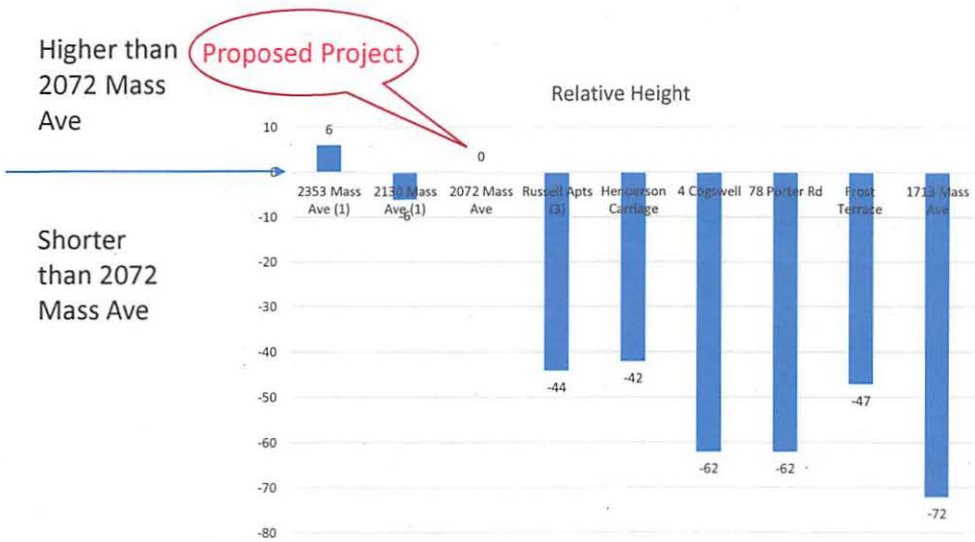
Address	Distance from 2072 Mass Ave (2)
2353 Mass Ave (1)	-2640
2130 Mass Ave (1)	-482
2072 Mass Ave	0
Russell Apts (3)	85
Henderson Carriage	100
4 Cogswell	400
78 Porter Rd	1056
Frost Terrace	2112
1713 Mass Ave	3168

Note 1 – Wall Height not available in Property Database; assuming 12 ft.

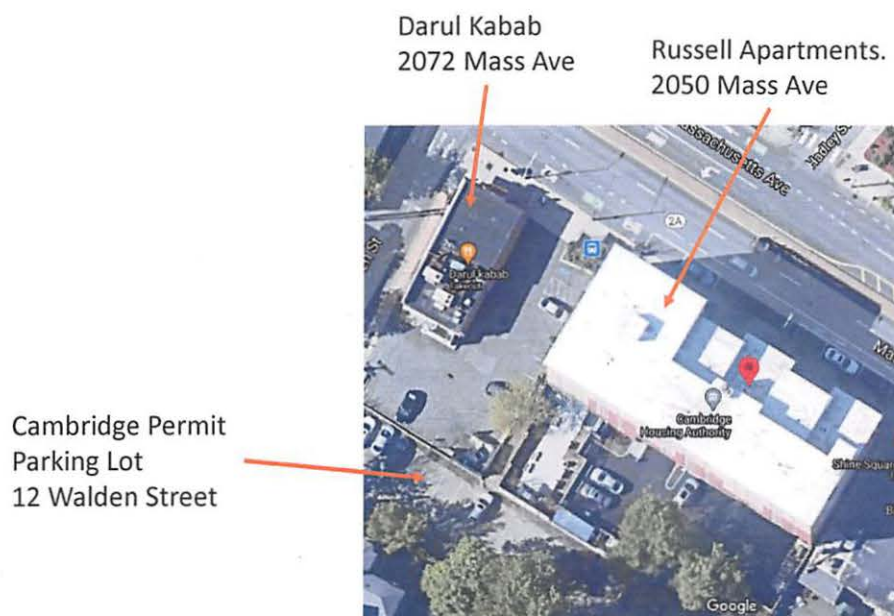
Note 2 - As measured using Google maps (negative is north of 2072 Mass Ave)

Note 3 - Property Condition Report for Cambridge Housing Authority

Relative Height of Relevant Buildings Near 2072 Mass Ave

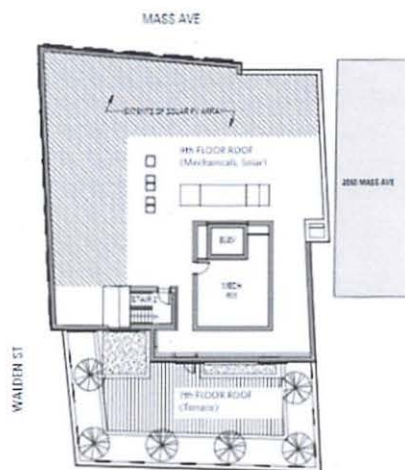


Proposed 2072 Mass Ave & Russell Apartments Satellite View



Proposed 2072 Mass Ave Footprint

The proposed building occupies the entire site with no setbacks and only 4' distance from the adjoining Russell Apartments



Proposed Roof Plan showing
Building Footprint

Existing Site GIS Map

Darul Kabab
2072 Mass Ave

Russell Apartments.
2050 Mass Ave



Cambridge
Resident Parking
12 Walden St.

Comparison to Nearby Affordable Housing Developments

The proposed building has significantly greater height and density than the other affordable housing in the Porter Square area

	2072 Mass Ave	Frost Terrace	Russell Apartments (Note 2)	78 Porter RD (Note 2)	1713-15 Mass Ave (Note 2)
Stories	9 (Mass Ave)/6 (Walden neighborhood)	5 at highest	6	4	3
Height (feet; Note 1)	102 - Tallest towering 44 ft over Russell Apt. (Note 3)	55 (Note 3)	58 (Note 4)	40	30
Units	49	40	51	26	16
Parking	3 accessible 2 Drop off	3 accessible	10 including 1 accessible	None	8 including 1 accessible
Lot (Square Feet)	8,515	22,000	15,258	8,130	14,499
Living Area(Sq. Ft.)	57,391	49,772	43,864	26,970	7,485
FAR	6.74 - highest; more than 3X avg of other 4	2.26	2.87	3.32	0.52

Note 1 – Based on Wall Height in Property Database and information from Mr. Korb

Note 2 – Owned by Cambridge Housing Authority or its subsidiary

Note 3 – To rooftop; 2072 Mass Ave will also have mechanical room on the roof which will be visible from nearby public way

Note 4 - Property Condition Report for Cambridge Housing Authority

Concerns from Russell Apartments' Seniors and Disabled Residents

Many residents express they feel vulnerable to voice their concerns

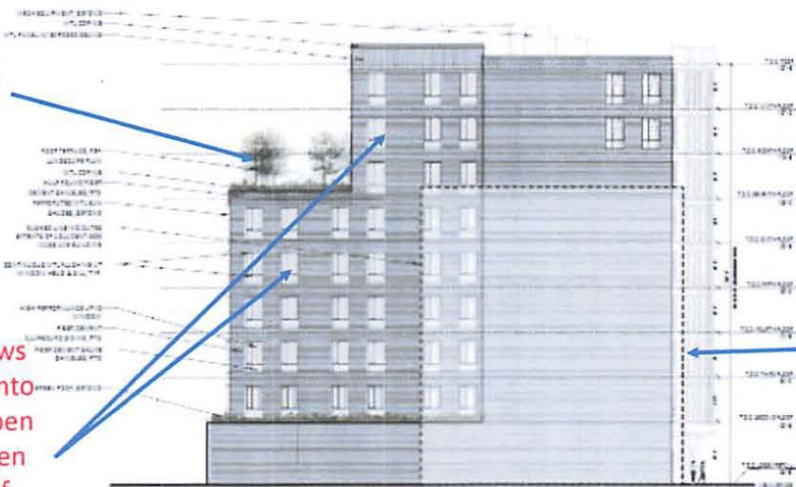
- Residents have just moved back to their building after 2-year rehab. Construction will mean some people may have to move again. **Density of proposed 2072 Mass Ave building next door will impose additional hardship to elderly residents including one who is 101 years old.**
- Drilling so close for the deep foundation required for a 9-story building will cause **vibration to be felt by the residents** and may even damage their interior.
- Russell Apartments is not air conditioned. **Residents are concerned they will not be able to open windows during construction due to noise and dust.**
- **Daily noise** from the densely-populated building next door is a pervasive concern for residents, who currently enjoy peace and quiet.
- Russell Apartments and the Senior Center has an entry to their parking on Mass Ave and turning into it is often treacherous, with fast moving traffic, a bike lane and cyclists, a sidewalk and pedestrians, a bus stop, and drop off / pick up for 2050 Mass Ave. An additional 150+ residents at 2072 site exacerbates these issues.
 - Congestion (pedestrians, cyclists) and traffic (drop off, pick up, deliveries) near the entrances and driveway will increase greatly due to the close proximity to 2072 Mass Ave residential entrance.
 - There is concern for safety for those who use mobility scooters and for senior pedestrians with reduced situational awareness.

Noise and Privacy Concerns from Russell Apartments' Seniors and Disabled Residents

Opposing windows and Roof Terrace looking directly into open space of Russell Apartment

Noise and Privacy :
Rooftop gatherings
and opposing
windows

These windows
look directly into
Russell Apt open
space and even
into some of
Russell tenants'
windows



DASHED LINE INDICATES
EXTENTS OF ADJACENT 2050
MASS AVE BUILDING

BUILDING ELEVATION – EAST
Board of Zoning Appeal Supplemental
Materials (12.03.20)

Russell Apartment Resident Outdoor Amenity

Russell Apartments' outdoor space would be impacted by the proposed design



Gardening Containers



Mature Littleleaf linden although to be protected during construction as outlined in CC HRE's Tree Protection Plan; tree will receive far less sunlight due to building footprint

Concern for being dwarfed by new building;

Now threatened

- Fenced in patio area will have shadow causing loss of sunlight due to shadow cast by proposed 2072 Mass
 - Enjoyment of fresh air in the sun will be diminished
 - Residents' ability to grow vegetables and flowers will be diminished
- Loss of privacy with people looking down from the southern windows and the 7th floor roof terrace of proposed 2072 Mass Ave

78 Porter Road Affordable Housing

A nearby affordable housing complex with the same lot size as 2072 Mass Ave, but half the density



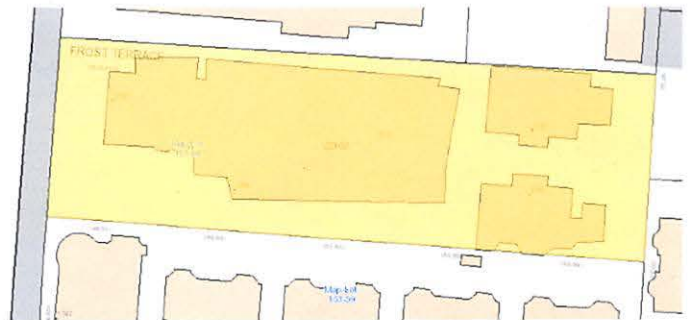
- 2nd building in from Mass Ave on Porter Road
- Currently undergoing renovation
- **Same size lot as 2072 Mass Ave but half the density**
- No green space but set back from property line

Frost Terrace Affordable Housing

Other nearby affordable housing complexes offer designs more appropriate to the neighborhood context



- Same development team
- Less dense with 2.5 times the lot size (22000 sq. ft.) as 2072 Mass Ave, only about half the living space (49,772 sq. ft) and 1/3 the FAR (2.26)
- Considerable green area open space with trees



2130 Mass Ave

This building, often used as a comparable, is significantly mitigated by building setback, large parking area, and greenspace; on a 40,000+ square foot lot



8-story building at 2130 Mass Ave
Extensive garden and play areas
beyond the tenant parking area

- At Corner of Mass Ave & Cogswell Ave
- 8 Stories
- 33 Office-Condo/Condo units
- Off-street parking with underground garage
- Lot Size 11,999 Sq. feet
- Living Sq. Feet 40,958
- FAR = 3.4



East Bound Traffic on Walden Street (Even during Pandemic)

The Walden Street / Mass Ave intersection had chronic, significant traffic congestion pre-Covid-19



View from the corner of Mass Ave and Walden St.



View from Sycamore Street

Photos taken at 3 PM, Friday Nov 20, 2020

- Traffic Assessment conducted by Vanasse & Associates is flawed in not considering the normal pre-COVID conditions at Mass Ave and Walden St intersection
- Traffic will return to pre-COVID levels, where daily traffic backups went past Raymond Park (1/2 mile away)
- Neighborhood residents met with City of Cambridge Traffic department (more than 50 people in attendance) in Feb 2020 to discuss safety concerns due to the untenable traffic issues at this intersection
- Public records search revealed no available pre-COVID-19 traffic studies done by the City of Cambridge

2072 Mass Ave Proposed Development Not Suited For the Location

The current proposed building is not appropriately scaled for the North Walden neighborhood and will pose safety issues due to the dangerous traffic conditions at the Mass Ave / Walden intersection



- Vehicles turning onto Walden Street from Mass Ave will have difficulty crossing eastbound traffic on Walden Street to enter 2072 Mass Ave development causing gridlock
- Drop off / pick up parking entrance and exit location on Walden Street will likely exacerbate traffic issues
- Pick up / drop off / deliveries will also happen on Mass Ave, causing dangerous traffic concerns
- Senior Center driveway entrance, Russell parking driveway, bus stop, pick up / drop off and 2072 Mass Ave residential entrance all within a few feet of one another

Conclusion

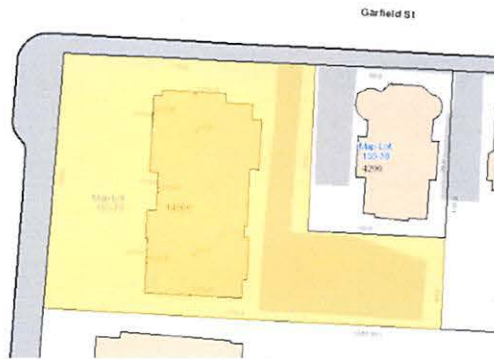
- The proposed project is too high and too dense for this neighborhood and the traffic analysis performed by Vanasse & Associates on behalf of Capstone Communities HRE needs to be augmented with a full traffic impact study.
- We are requesting the Board of Zoning Appeals require the following:
 - No waiver granted for Article 19. Planning Board to Conduct **Project Review per Article 19**– including Traffic Impact Review and Urban Design Review)
 - Context Study by the Development Team
 - Reduction in the scale and height of the proposed development to **6 stories facing Mass Ave and 3 stories facing Walden Street** to be more in line with other 100% affordable housing developments (AHD) in Porter Square neighborhood

Appendix: Additional Comparisons

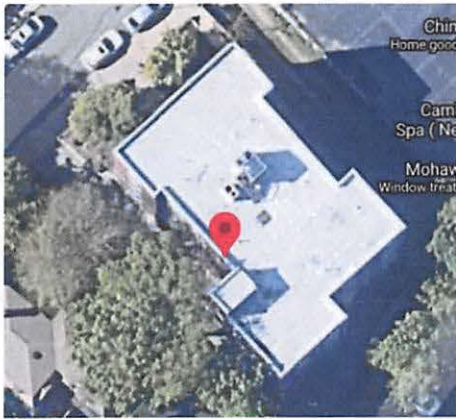
1713-1715 Mass Ave



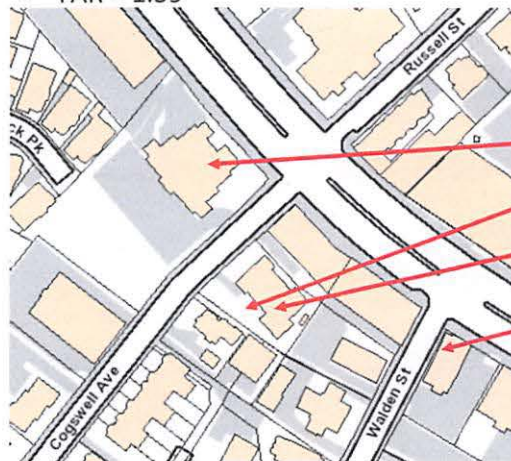
- At Corner of Mass Ave & Garfield Street
- Less dense with 1.7 the lot size but 13% living space
- Abundance of open space



4 Cogswell Ave



- South side of Cogswell Ave; 2nd building in from Mass Ave;
- across from 2130 Mass Ave
- 4 Stories; 28 units
- No off-street parking
- Lot Size 9,279 Sq. feet
- Living Sq. Feet 14,800
- FAR = 1.59



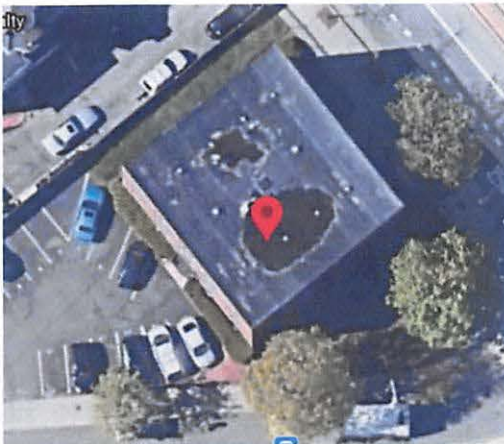
2130 Mass Ave

Open Space

4 Cogswell Ave

Existing 2072 Mass Ave

2192 Mass Ave



- At Corner of Mass Ave & Rindge Ave
- 4 Stories
- 16 Condo units
- Lot Size 11,999 Sq. feet
- Living Sq. Feet 10,827
- FAR = 1.1



Pacheco, Maria

From: Rachel Grashow <rachel.grashow@gmail.com>
Sent: Monday, December 7, 2020 5:24 PM
To: Pacheco, Maria
Cc: Derek Etkin
Subject: Creating affordable housing on Mass Ave- in support

To the Board of Zoning Appeal,

As a homeowner and parent in Cambridge, I would like to add my voice to those who support the affordable housing project at 2072 Mass Ave (CASE NO. BZA-017326-2020).

These are exactly the housing options we need to have if we are to hold onto one of the aspects that make Cambridge great- the chance for all types of families to enjoy the high quality education, transit and community benefits I get to enjoy.

These are at least 49 more potential people to befriend, to have as parents in our classrooms, and for my children to have playdates with. Guaranteeing options like these is what keeps me in Cambridge.

Please make sure this voice is heard on the 10th.

Best,
Rachel Grashow
107 Rindge Avenue
Cambridge, MA 02140

**Comprehensive Permit Application
2072 Massachusetts Avenue
Cambridge, MA 02140**

Submitted by:

CC HRE 2072 Mass Ave Tenant LLC

Submitted to:

City of Cambridge Board of Zoning Appeal

November 10, 2020



COMPREHENSIVE PERMIT APPLICATION

2072 MASS AVE APARTMENTS

2072 MASSACHUSETTS AVENUE, CAMBRIDGE 02140

Submitted to: CITY OF CAMBRIDGE BOARD OF ZONING APPEAL

By: CC HRE 2072 Mass Ave Tenant LLC

Date: NOVEMBER 10, 2020

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- 2) Comprehensive Permit Application Form
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COMPREHENSIVE PERMIT APPLICATION

2072 MASS AVE APARTMENTS
2072 MASSACHUSETTS AVENUE, CAMBRIDGE, 02140

SECTION 1
COVER LETTER AND NARRATIVE

CC HRE 2072 Mass Ave Tenant LLC
c/o Capstone Communities LLC
1087 Beacon Street, Suite 302
Newton, MA 02459

November 10, 2020

Board of Zoning Appeal
City of Cambridge
831 Massachusetts Avenue
Cambridge, MA 02139

Re: Comprehensive Permit Application for 2072 Mass Ave Apartments
2072 Massachusetts Avenue, Cambridge, MA

Dear Board of Zoning Appeal Members:

CC HRE 2072 Mass Ave Tenant LLC (“Applicant”), an affiliate of Capstone Communities LLC (www.capstonecommunities.com) (“Capstone”) and Hope Real Estate Enterprises LLC (“Hope”), propose to construct an affordable housing community located at 2072 Massachusetts Avenue in Cambridge’s Porter Square and North Cambridge (the “Site”). The resulting residential community will consist of 49 affordable rental apartment homes with a variety of unit sizes – 14 one-bedroom, 21 two-bedroom, and 14 three-bedroom apartments – that will be affordable to individuals and families earning a range of incomes – from 30% to 60% of the area median income (the “Project”). Capstone is a Newton-based developer of mixed income, affordable, and historic apartment communities, and Hope is a Cambridge-based real estate development company with significant zoning consultation and permitting expertise. Capstone and Hope are currently developing Frost Terrace, a 40-apartment 100% affordable housing community located a few blocks south at 1785-1791 Massachusetts Avenue which is anticipated to open in spring 2021. Previously, Hope and Capstone developed Port Landing, a 20-apartment, 100% affordable housing community in The Port/Kendall Square neighborhood in 2016.

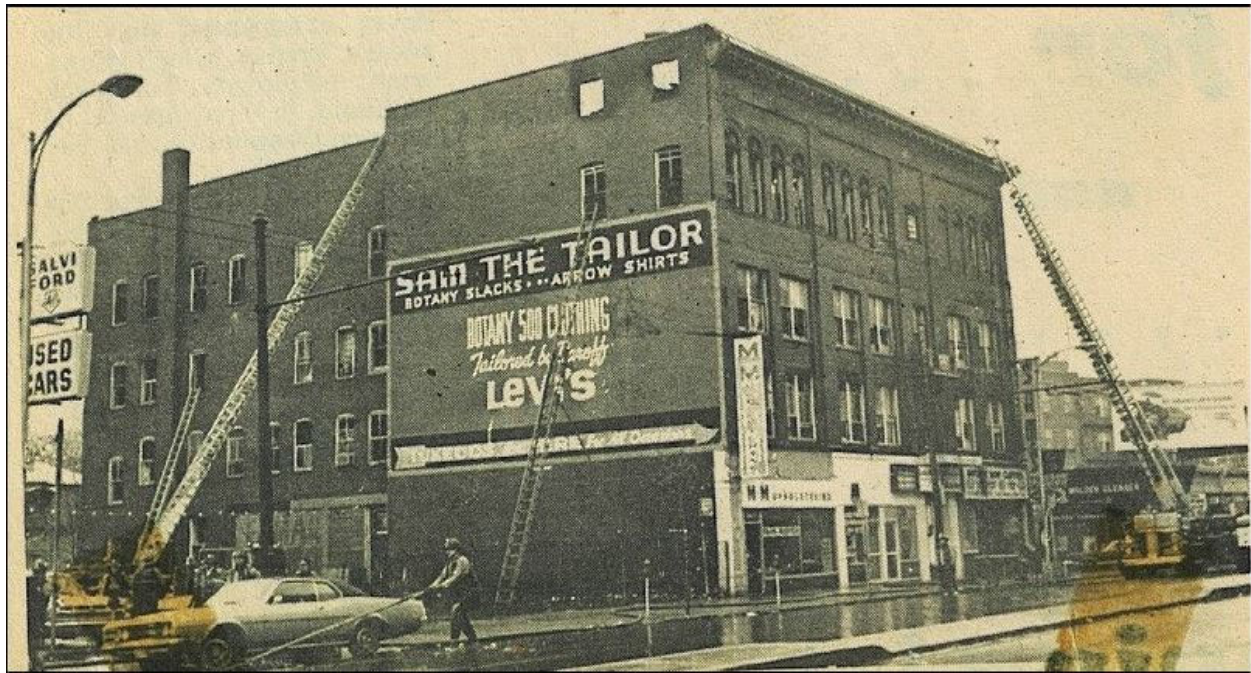
Development Proposal

Existing Site Conditions

Located with frontage on Massachusetts Avenue to the northeast and Walden Street to the northwest, the Site is comprised of one lot with approximately 8,515 square feet. Currently, the Site is occupied by an 1,860 sf, one-story building leased to Darul Kabab restaurant. The building is widely known to have housed Kentucky Fried Chicken for many years.

The immediate context along Massachusetts Avenue includes a mix of commercial, residential, and institutional buildings directly fronting the Avenue. The current one-story building and surface parking lot on the Site were constructed in 1971, having replaced a ca. 1890 four-story Odd Fellows Building which occupied most of the parcel. Though the Massachusetts Avenue corridor maintained a small-scale residential character throughout much of the 19th century, by the early 20th century the blocks north of Porter Square were dominated by multi-story commercial, civic, and religious buildings directly fronting the

Avenue. Massachusetts Avenue remains a densely developed artery lined with multi-story commercial, residential, and institutional buildings.



I.O.O.F. Building formerly located at 2072 Massachusetts Avenue, depicted during the 1968 fire which led to its demolition (*Cambridge Chronicle*).

Proposal Summary

The Project will include forty-nine (49) 100% affordable rental housing apartments. The affordability will be in perpetuity. Approximately 71% (35 apartments) will be two and three bedrooms for families. Specifically, the proposal includes 14 one-bedroom apartments, 21 two-bedroom apartments, and 14 three-bedroom apartments. Units average in size from 625 sf (one-bedroom apartments) to 824 sf (two-bedroom apartments) to 1,087 sf (three-bedroom apartments). The high proportion of two- and three-bedroom apartments will provide safe and accommodating housing to families.

Additionally, the Project will include three handicapped accessible parking spaces and two short-term drop-off/pick-up spaces in a covered garage area at the rear of the Site, along with 51 long term bike parking spaces located on the lower level of the building (48 regular and 3 tandem). Electric vehicle charging stations will be provided for the three accessible spaces and power outlets will be provided in the bike room for electric bicycles and repairs. The Project will also include retail space on Massachusetts Avenue and a resident amenity space within the first floor. The retail space is slated for a community use. The Project's operating budget also includes a part-time resident services coordinator who will plan events for families and others in the building and will assist with creating community building and educational activities.



Current Condition



Proposed Condition

The new eight-story building will be constructed to Passive House US (PHIUS) standards and will target PHIUS Certification, and will include a green roof, rooftop solar along with a stormwater management system. The resilient, environmentally friendly materials used to construct the contemporary building façade will ensure long-term sustainability for the Project while adding dimension and rhythm to the Avenue. See **Section 9 Green Building Report** for more information.

Neighborhood

Situated to the northwest (less than 0.3 miles from the center) of Porter Square, the Project will complement an already vibrant mixed-use area of institutional, residential, office, restaurant and retail uses. Located in the North Cambridge neighborhood and adjacent to Neighborhood Nine, the Project's density and scale aligns with other buildings on Massachusetts Avenue. Directly to the Site's southeast and fronting on Massachusetts Avenue is the six-story affordable age restricted Russell House Apartments that is owned and operated by the Cambridge Housing Authority. Russell House Apartments is approximately 60' and contains 52 apartments. The five-story, 68' historic Henderson Carriage Building is located across Massachusetts Avenue from the Site. 2130 Massachusetts Avenue, an eight-story, 71' building is located one block to the northwest of the Site. Directly across Walden Street from the Site is a single-story retail building and the three and a half story 5 Walden Street condominiums. Spanning the entire rear of the Site is a municipal parking lot that provides an almost 50' wide buffer from the mostly three-story multifamily residential neighborhood to the west. The owners of the Henderson Carriage Building and Russell House Apartments support the Project.

The below Neighborhood Buildings – Height Map details the surrounding building heights.



Area Amenities

This Site is well-served by public transit. The Porter Square MBTA Redline and Commuter Rail stations are less than 0.3 miles from the Site and the Davis Square MBTA Redline is located 0.5 miles from the Site. In addition, the #77 and #83 MBTA bus routes are at the Site, the nearest BLUEbikes station is located directly across the street, and the nearest Zipcar space is less than 200 feet away. These various rapid transit options provide access to jobs, services, and amenities throughout Cambridge, Boston, and the suburbs.

Additionally, the Site is located within walking distance to the Porter Square Shopping Center, which provides several key family-friendly amenities including a grocery store, pharmacy, hardware store and various other community-serving businesses. In the context of Cambridge's extreme shortage of affordable family housing, the Project will provide forty-nine (49) households with access to a desirable, transit oriented, and thriving neighborhood. According to Walkscore.com, the Site's Walk Score is 97, which is considered a Walker's Paradise.

The Site is also within walking distance to a number of public elementary schools and daycare centers including the Rindge Avenue Upper School (0.2 miles or 6-minute walk), Wild Rose Montessori School (0.3 miles or 7-minute walk) and Benjamin Banneker Public Charter School (0.4 miles or an 8-minute walk). Below is a partial list of amenities within 0.5 miles of the Site. The play yard at St. James Church will be open to the Project's residents as well as the general public from 8am to dusk every day of the week with few exceptions. St. James Church, at 1991 Massachusetts Avenue, is one block from the Project.

Restaurants		Groceries		Shopping	
Wasabi at Porter	0.03 Miles	Star Market	0.2 Miles	China Fair	0.01 Miles
McCabe's on Mass	0.08 Miles	Pemberton Farm	0.2 Miles	Barefoot Books	0.04 Miles
Andy's Diner	0.1 Miles	Spindler Confections	0.2 Miles	Seth Berman Gardeners	0.05 Miles
Posto	0.2 Miles	Stop & Shop	0.3 Miles	Drinkwater's	0.05 Miles
Palm Sugar Thai Cuisine	0.2 Miles	Davis Square Farmers Market	0.3 Miles	Big Picture Framing	0.09 Miles
Dakzen	0.2 Miles			General Optical Co.	0.01 Miles
The Shawarma Place	0.2 Miles	Parks		Fun Antiques	0.2 Miles
Newtowne Grille	0.2 Miles	Bergin Playground	0.2 Miles	Stellabella toys	0.2 Miles
Domino's Pizza	0.2 Miles	Rindge Field	0.2 Miles	The Caning Shoppe	0.2 Miles
Sugar & Spice Thai	0.2 Miles	Kenney Park	0.3 Miles	Books by Design	0.2 Miles
Urban Hearth	0.3 Miles	Corcoran Playground	0.4 Miles	Nebia	0.2 Miles
Punjabi Grill	0.3 Miles	Statute Park	0.4 Miles	Buffalo Exchange	0.3 Miles
Rosebud American Cuisine	0.3 Miles	Sheridan Square	0.4 Miles	Watch Shop	0.3 Miles
Snappy Kitchen	0.3 Miles	Seven Hills Park	0.4 Miles	Ace Wheel Works	0.3 Miles
Anna's Taqueria	0.3 Miles	Saint Peters Field	0.5 Miles	Michaels	0.3 Miles
Christopher's	0.3 Miles			Family Dollar Store	0.3 Miles
Café Barada	0.3 Miles	Schools		Magpie	0.3 Miles
Redbones BBQ	0.3 Miles	Rindge Avenue Upper School	0.2 Miles	Sprint	0.3 Miles
Toad	0.3 Miles	Wild Rose Montessori School	0.3 Miles	Mind's Eye Yarns	0.3 Miles
Wok N Roll Restaurant	0.3 Miles	Benjamin Banneker Public Charter	0.4 Miles	Porter Square Books	0.3 Miles
Panera Bread	0.3 Miles	Cambridge Friends School	0.4 Miles	Bike Boom	0.4 Miles

Building Program and Site Design

The Project's design balances a complex set of contextual priorities for the Site which have arisen from analysis, community meetings, and various meetings with the City of Cambridge Urban Planning, Community Development and Traffic and Parking teams. The design goals are:

Create a high-quality affordable housing apartment community for families in Cambridge

The design reflects its context, a strong corner on Cambridge's main street. The building massing is slender, allowing for light and air for all apartment homes. The site is linked to amenities and services in this walkable neighborhood and the unit interiors are simple, but elegant.

Create significant transparency: Activate the streetscape

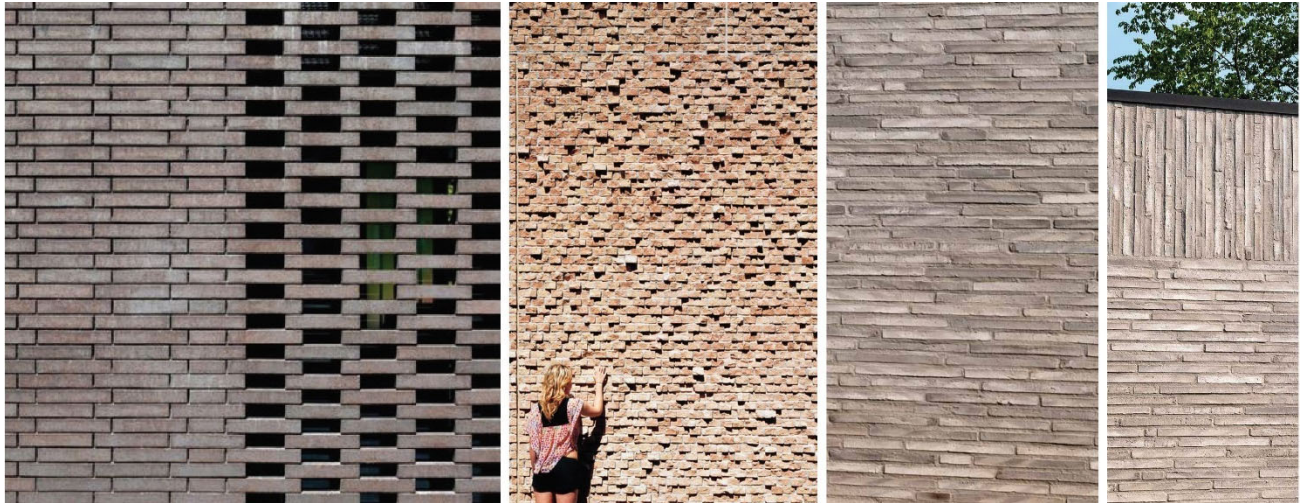
A primary design goal is to maintain transparency at street level to facilitate visual connections and to activate the streetscape. The first floor along Mass Ave will be wrapped with transparent storefront glazing and will include both the retail and residential entryway. The residential entryway will be pushed back to create a more pedestrian-scaled experience for residents. The storefront turns the corner onto Walden St and continues along with transparency into the retail and resident amenity spaces. An entrance to limited parking and services area is hidden on the rear part of the site.

Shaping: Building Form

The building consists of two interconnected 8-story volumes. Along Massachusetts Avenue and West along Walden Street, a seven-story suspended aluminum clad cube sits above one story of recessed with storefront glazing. The suspended cube of the floors is clad in light colored anodized aluminum / zinc vertical panels with a staggered pattern, creating a light and airy volume that floats above the street, minimizing the Project's perceived mass, and also has a strong corner presence that is visible from the street intersection. A grid of openings accentuates the suspended cube, incorporating large windows which provide natural daylight into the interior units, as well as vertical infill composite panels that resemble wood, bringing warmth and texture. Each window and infill panel pair are framed with a metal projection that provides a rhythm to the façade.

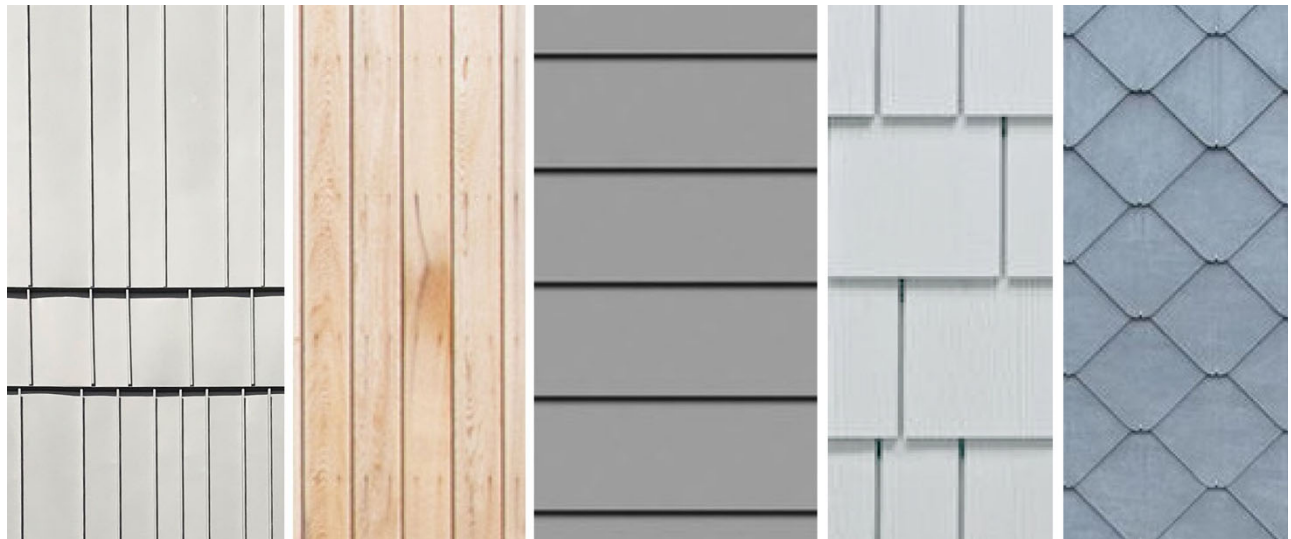
Human Scale: Material Palette

The residential anchor consists of a materials palette that relates to the surrounding residential buildings. The brick plinth / base is located on the ground floor, most prominently at the residential entry which is recessed along Mass Ave, adjacent to the retail space and bus shelter. The crafted long brick at street levels enlivens the pedestrian experience and allows for interactions as the wall angles back and forth. This culminates in a gradient of increasingly frequent openings that also serve as visibility into the parking area, as well as creating ventilation. Above the parking entry, the brick is oriented vertically as a contemporary response to the traditional lintel approach, while highlighting the playfulness of the material.



Brick pattern material precedents

Above the brick plinth, painted clapboard comprises the main field of the body. Proportionately sized and spaced windows bordered by a trim are located along all orientations of the residential anchor. On the south elevations, each window has an infill panel below, clad in a lighter colored shingle panel to provide an accent from the main field, as well as to give the perception of the same proportions compared to windows on the north and west facades. In addition, solar shades are proposed on the South façade that not only livens the rhythm of the façade, but also serves a functional purpose of providing shade to reduce the building's overall energy consumption. The top of the residential anchor is defined by a cornice band of half-round shingles.



Suspended cube and residential anchor material precedents

Streetscape and Greenery

Except as indicated below, while there is no open space or landscaping on the Site, several street trees are located on the opposite side of Walden Street. Due to the existing electrical duct banks below the sidewalks adjacent to the Site on Massachusetts Avenue and Walden Street, it is not possible to plant street trees in these locations. Planters and fences supporting the growth of vertical green are located on the Project's

southwest facade, and at the openings to the parking and service areas. An intensive green roof is also visible along Walden Street above the parking entrance, with a variety of non-invasive plant species.

There is a 12" DBH *Tilia cordata*, Littleleaf linden located on the Site's south property line. This tree is mostly located on the 2050 Massachusetts Avenue property. Daniel E. Cathcart, an ISA Board Certified Master Arborist, developed a Tree Protection Plan (included herein) that will be followed by the development team prior to, during, and after construction. See **Section 10** for the **Tree Protection Plan**.

Accessibility

The Project is designed to comply with the requirements of 521 CMR, Massachusetts Architectural Access Board (MAAB), the Fair Housing Act, the Americans with Disabilities Act (ADA), as well as the Uniform Federal Accessibility Standards (UFAS). 3 apartments (1 of each bedroom type) in the Project will be Group 2 units for individuals with mobility difficulties (521 CMR 9.4), with the rest being Group 1 units (521 CMR 9.3). In addition, 1 apartment will be a Group 1 unit that also provides sleeping accommodations for persons who are deaf or hard of hearing (521 CMR 9.7).

Sustainability

The Project will incorporate sustainable and resilient design strategies that reflect a commitment to environmental stewardship that aligns with affordable housing with a strong focus on lowering utility costs, carbon and greenhouse gases reduction and creating healthy environments. The Project is targeting Passive House certification to the standards set by the Passive House Institute US (PHIUS) for their PHIUS+ Core rating system, as well as certifying through the EPA Indoor air PLUS program. The PHIUS+ CORE rating system includes stringent and verified building performance metrics as well as professional testing of the building envelope and air sealing at two stages during building construction. EPA Indoor air PLUS certification includes verification of indoor air quality (IAQ) quality control measures including but not limited to: moisture control, HVAC venting and sealing, and use of low VOC materials in construction. In addition, the design team is also planning to conduct a systems commissioning process in addition to the envelope. The above will result in a highly efficient building that lowers utility costs, protects occupant health through excellent indoor air quality, as well as contributes to the overall reduction in carbon and greenhouse gases emissions. Specifications for a simple, durable materials palette will emphasize the choice of healthier building materials and reinforce the Passive House approach, these measures also act as quality-of-life improvements for the residents, and will be integrated with the management of the property.

In terms of addressing resiliency concerns such as extreme weather events and future climate change, the project team evaluated the flood risk based on current maps and future projections for the site and surrounding area. In addition, various protection, adaptation, and backup strategies have been incorporated. Additional details can be found in **Section 9 Green Building Report**.

Affordable Housing

Due to its unit mix and income set asides, the Project's 49 apartment homes will attract a variety of households. 41 units will be set aside for households that earn at or below 60% of AMI, currently a household income ranging from \$53,760 – \$82,920, depending on household size. Eight (8) units will be Section 8 PBV units set aside for households that earn at or below 30% of AMI, currently there is no minimum household income, and a maximum income of \$30,720 – \$41,460, depending on household size. Including all utilities and for the 60% AMI apartments, monthly one-bedroom rents are estimated to be \$1,440, monthly two-bedroom rents are estimated to be \$1,728, and monthly three-bedroom rents are

estimated to be \$1,995. When accounting for utilities, these rents are approximately 40% of the neighborhood's market rate rents, which are deeply unaffordable to households in this income range.

The proposed income mix provides housing that is affordable to families and individuals earning a range of incomes. The immediate market area shows strong demand for this unit mix as effective vacancy rates are 0%. The demand for affordable units is even more significant than that for market rate apartments. At Port Landing, which was opened in 2016, 1,386 applicants applied for its 20 apartments prior to the lottery. Data obtained from The Finch, an affordable apartment community owned by Homeowner's Rehab which completed its lease-up in 2020, showed that 2,261 individuals or families applied for 98 apartment homes. There are currently 20,703 unique applicants on the Cambridge Housing Authority waitlists.

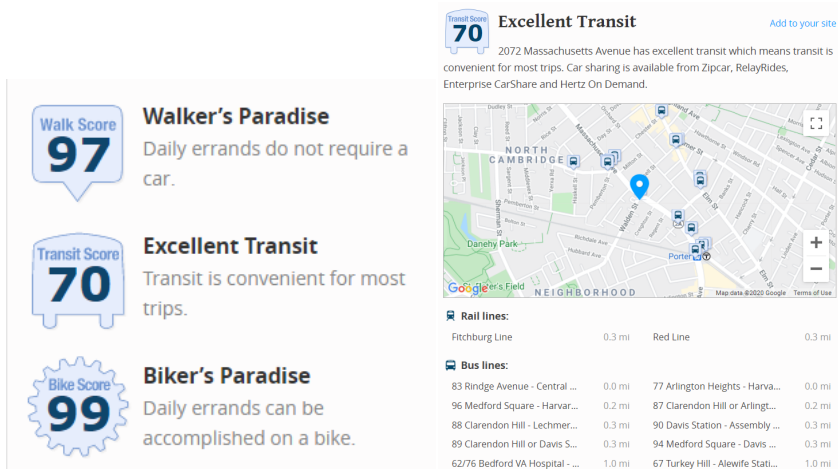
To the extent permitted by Department of Housing and Community Development, 70% of the apartments will give preference to current Cambridge residents, municipal and school department employees, and employees of local businesses.

Transportation and Parking

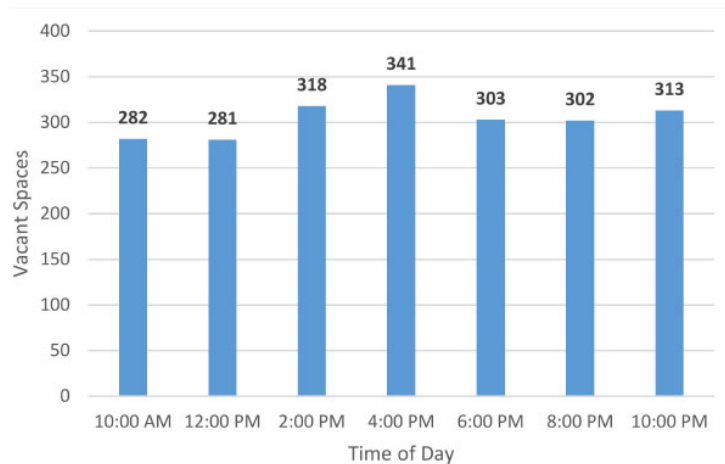
The site plan proposes three (3) covered, on-site accessible parking spaces that will be dedicated to residents and guests with disability plates or placards. In addition, there will be two (2) short-term COVERED drop-off and pick-up spaces to aid in reducing congestion along Walden Street.

As previously indicated, the Site is a short walk from numerous forms of transit options including the Porter Square Station less than 0.3 miles from the Project that includes the MBTA subway Red Line, Fitchburg/South Acton Commuter Rail Line, four Bus Lines (Bus Route #77, 96, 83 and 87) and several car sharing locations. The Parking and Traffic Assessment by Vanasse & Associates, Inc. (VAI) included in **Section 6** suggests approximately half of the building's households, or ~25 households, will have cars. Those residents with or without cars will benefit from Transportation Demand Management (TDM) that will include covering the cost of MBTA passes for property management staff, subsidizing MBTA passes for residents, BLUEbikes memberships, or ride sharing memberships, as further detailed in the VAI report. In addition, the installation of public transportation and ride share timing screens at a centralized location will provide residents with information for easy access to transit. Ownership and building management will provide information at move-in to all residents on all public transportation options within a short distance of the Site.

Below is a map from Walkscore.com highlighting the Site's adjacent transit options:



VAI determined the Project's projected parking demand and quantified the availability of on-street parking within a quarter (0.25) mile from the Site entirely within the City of Cambridge boundaries during peak parking demand hours. In summary, at the time of its study on Tuesday, October 20, 2020, VAI determined that the peak hour demand was at 12:00pm at which time a total of 281 on-street parking spaces were available within a quarter (0.25) mile of the Site. VAI concludes its report by stating: "In summary, a detailed parking survey was completed in the area of the Project and based upon this data it can be concluded that there is more than sufficient availability of on-street parking to accommodate the Project. The Project proponent is committed to implementing a Travel Demand Management plan which promotes alternative modes of transportation and will minimize the Project's impact on available on-street parking and traffic in the area."



VAI Parking Study, October 20, 2020, Figure 4

It is expected that many of the Project's residents will utilize alternative modes of transportation other than automobiles. Based upon the U.S. Census and 2018 American Community Survey data for Census Tract 3547, the tract in which the Project is located, the mode split characteristics of the Project are estimated as follows: 32 percent automobile trips; 43 percent transit; 10 percent walk; six (6) percent bicycle, and nine (9) percent other trips. Pursuant to VAI's report, the Project is expected to generate approximately 98 vehicle trips on an average weekday (49 entering/49 exiting), with approximately six (6) vehicle trips (2 entering/4 exiting) expected during the weekday morning peak-hour. During the weekday evening peak hour, the Project is expected to generate approximately 9 new vehicle trips (5 entering/4 exiting).

The Project's proximity to several alternative transit options and community serving amenities (schools, pharmacy, grocery store) within walking distance will produce a thriving walkable residential community. The Project's design is consistent with smart growth principles and the Cambridge City Council's goal to reduce reliance on vehicle usage while promoting alternative forms of transportation.

Walden Street Widening

Through discussions with Cambridge Community Development, Traffic and Parking, Department of Public Works, and community outreach, the development team identified that the portion of Walden Street adjacent to the Site is unusually narrow for a three lane street. Currently Walden Street is ~26'-8", with a ~10'-0" travel lane outbound from Massachusetts Avenue and two ~8'-4" travel lanes inbound. This constriction slows traffic moving through the intersection, makes turns difficult, and does not align with best practices and guidelines.

The proposed design includes the widening of Walden Street from ~26'-8" to 30'-0". This is achieved by locating the exterior walls of the ground floor ~4'-0" inside the property line and then granting an easement to the City for a sidewalk in that area, resulting in three (3) 10'-0" travel lanes and making a more spacious street condition. The proposed design also improves the sidewalk by widening it from its current ~6'-5" width to be ~7'-2" wide, both measurements are inclusive of the curb. The proposed upper floors of the new building overhang the sidewalk by ~3'-6" with an overhead clearance of ~13'-0".

Regarding the constructability of widening Walden Street, the development team approximately located electrical duct banks in the Walden Street sidewalk. Based on visual inspections of the manholes and a utility Ground Penetrating Radar (GPR) report by GPRS, it seems unlikely that the location of these duct banks will conflict with the proposed construction associated with the widening of Walden Street. Conversations with Eversource on the exact elevation of the duct banks are ongoing. The GPR report is included in **Section 8**.

Evidence of Need for Affordable Housing:

The City of Cambridge historically was the home of immigrants and low and middle-income earners that were vital to Cambridge's glassworks and furniture factories from the 1920's through the 1970's. However, since rent control ended in 1994, Cambridge has experienced an exponential increase in land value, resulting in a disproportionate impact on the availability of affordable housing options for low and middle-income individuals and families. HUD defines "cost burdened" households as those who pay more than 30% of their income for housing.¹ According to the U.S. Census Bureau, 43% of renters in Cambridge are paying 30% or more of their household income on rent and are therefore considered cost burdened.²

Lower and middle income families in Cambridge find it exceptionally difficult to secure family friendly housing throughout the City. In the last six (6) months, accordingly to MLS, the median sale price for a single family home in Cambridge was \$1,733,500 and the average condominium sale price was \$812,500. Based on an informal October 2020 survey of larger apartment communities in the immediate market area, two-bedroom apartment rents are \$3,600 and three-bedroom apartment rents are \$4,500.³ In order to afford these rents and not be cost burdened, a household would need to earn at least \$152,000 – \$190,000 annually. Over 60% of Cambridge households make less than \$150,000, making these units unaffordable to a majority of current Cambridge residents.⁴

The City's housing stock is also older and many units contain lead based paint hazards, which further constrain families from locating safe, quality housing for their children. In fact, 71.4% of Cambridge's housing inventory was constructed prior to 1980.⁵ Upon completion, all of 2072 Massachusetts Avenue's apartments will be new.

Once completed, the Project will provide urgently needed affordable housing in an area where a significant number of families and individuals are unable to afford quality housing. Additionally, the high number of three-bedroom apartments (29%) will specifically be occupied by families.

¹ U.S. Department of Housing and Urban Development, "Housing Choice Voucher Program" <https://www.hud.gov/hudprograms/hcvp> (accessed October 9, 2020)

² U.S. Census Bureau American Community Survey 2019: ACS 1-Year Estimates TableID DP04

³ The Wyeth – 120 Rindge Avenue, Cambridge

⁴ U.S. Census Bureau American Community Survey 2019: ACS 1-Year Estimates TableID S2503

⁵ U.S. Census Bureau American Community Survey 2019: ACS 1-Year Estimates TableID DP04

Financing

If 2072 Massachusetts Avenue's Site Comprehensive Permit application is approved, the development team anticipates applying for an array of local, state and federal subsidies as well as private investments. The development team will apply to the Massachusetts Department of Housing and Community Development (DHCD) for an allocation of state and federal low-income housing tax credits as well as additional subsidies.

In 2018 the Cambridge Affordable Housing Trust provided a \$3.8 million loan to finance the Site acquisition and certain predevelopment expenses. If the Project receives its permitting, the development team anticipates applying for additional City funding and for eight (8) Cambridge Housing Authority Section 8 Project Based Vouchers.

Site Control, Permitting and Community Process

Site Control

Capstone and Hope have created separate entities that own/will own the Site/Project and that will develop the Project. CC HRE 2072 Mass Ave LLC, an affiliate of Capstone and Hope, purchased the 2072 Mass Ave land and building using loan proceeds from the Cambridge Affordable Housing Trust in April 2018.

On November 9, 2020, CC HRE 2072 Mass Ave LLC entered into a 99-year ground lease with CC HRE 2072 Mass Ave Tenant LLC for the 2072 Massachusetts Avenue land. See the attached organizational chart that outlines the entity structure once the Project receives all its financing. This ground lease structure is typical of affordable housing developments in Cambridge due to the complicated financing structure required by local and state financing agencies.

Permitting

CC HRE 2072 Mass Ave Tenant LLC is a Limited Dividend Organization under M.G.L. c.40B, §§ 20 through 23. Pursuant to 760 CMR 56, CC HRE 2072 Mass Ave Tenant LLC is applying to the Department of Housing and Community Development, as the Subsidizing Agency, for Project Eligibility under the following subsidy programs: Affordable Housing Trust Fund, DHCD Housing Stabilization Fund (HSF), HUD HOME Program (Rental Production), State Low Income Housing Tax Credit Program, and the Federal Low Income Housing Tax Credit Program (LIHTC). An affordable housing restriction will be recorded against the land and buildings with a term in perpetuity and the Project will comply with the Affirmative Fair Housing Marketing and Resident Selection Plan as required by 760 CMR 56.

According to the Zoning Map, the majority of the Site has a base zoning of Business A-2 and a small portion (13'+/-) at the rear of the parcel is in the Residence B base zoning district. Both districts allow for residential uses as of right although the Business A-2 district also allows for a range of commercial/retail and multi-family residential uses whereas the Residence B district is a one- and two-family district only. The Site also is in the Massachusetts Avenue Overlay District and North Massachusetts Overlay Sub- district (the "Massachusetts Avenue Overlay").

The development team is proposing to obtain all of 2072 Massachusetts Avenue's local approvals through an MGL Chapter 40B Comprehensive Permit from the Cambridge Board of Zoning Appeal (BZA). Since the City of Cambridge has met its obligations under MGL Chapter 40B, the development team is requesting that the BZA accept this Comprehensive Permit application. Pursuant to 760 CMR 56.05(7), please refer to

Section 5 for a detailed list of requested Waivers from Local Requirements and Regulations. **Section 4** is the Dimensional Form.

Community Process

The development team has worked extensively with the City of Cambridge, including the Community Development Department (Zoning, Housing, Sustainability, Urban Design, Pedestrian and Bicycle), Fire Department, Traffic and Parking, Department of Public Works, and Historical Commission.

In addition, the development team has engaged the community through individual abutter Zoom meetings and a large Zoom community meeting on September 29, 2020 that was attended by 82 people. The Project was also presented to the Porter Square Neighbors Association (PSNA) on October 15, 2020 and is scheduled to reappear at the PSNA on November 19, 2020. Understanding that many residents of the adjacent Russell House Apartments do not have access to computers and/or the internet, and in conjunction with the building's management, the development team displayed large renderings of the Project and comment cards in the building's lobby so that Russell House residents would have an opportunity to provide input.

Community outreach and various City department input resulted in the following changes to the proposal: (1) setting back further the first floor facing Walden Street, (2) relocating the main pedestrian entrance from Walden Street to Massachusetts Avenue, (3) widening Walden Street and the sidewalk adjacent to the Project, (4) programming the retail space to accommodate varied and community uses, and (4) developing partnerships with community stakeholders to access additional amenities for the Project's residents. An additional community meeting via Zoom is scheduled for November 16, 2020 to share design updates and elaborate on changes that were made to incorporate community feedback. The development team has maintained a website, www.2072massaveapts.com, that provides updated and detailed information on the proposal. The website includes copies of plans, FAQs (forthcoming), news and events, information on the development team, and contact information for the community to provide feedback and ask questions.

Development Team

The following development team has been formed to include industry experts ensuring a seamless and successful completion:

- **Developers:**
 - *Capstone Communities LLC* (www.capstonecommunities.com), is a Newton, Massachusetts based real estate development firm experienced in structuring complex financing involving multiple federal and state subsidies. Jason Korb is the principal of Capstone Communities LLC where he has developed market rate, mixed income, and 100% affordable housing. Since founding Capstone in October 2010, Jason has successfully completed a total of \$60,000,000 of development transactions in Cambridge, Somerville, Arlington, Newton and Brockton Massachusetts. These include converting Brockton's first brick shoe factory into 25 mixed-income apartments, co-developing 20 100% affordable family apartments on a vacant lot in Cambridge's Port neighborhood. Additionally, Capstone and Hope are currently developing Frost Terrace, a 40 apartment, 100% affordable community in Porter Square which is estimated to be complete in Spring of 2021.

Prior to forming Capstone, Jason was the Vice President of Acquisitions at Beacon Communities LLC, a developer, owner, and manager of over 9,000 apartment homes in the Northeast. At Beacon, Jason was responsible for sourcing new acquisitions and overseeing

mixed income, affordable and market rate development and financing opportunities. In his seven years at Beacon, Jason was responsible for developing over 600 apartment homes totaling over \$100M. Prior to joining Beacon in 2004, Jason was a Housing Project Manager at the Fenway Community Development Corporation in Boston. Jason is a former Director of Caritas Communities and a former Vice-Chair of Preservation Massachusetts. Jason received an MS from the Massachusetts Institute of Technology's Center for Real Estate and a BA from the University of Michigan, Ann Arbor. Jason's MIT thesis, *The Low Income Housing Tax Credit: HERA, ARRA, and Beyond* has been cited by Harvard University's Joint Center for Housing Studies and the US Senate Budget Committee.

- *Hope Real Estate Enterprises LLC*, is led by Cambridge based attorney and Real Estate Developer Sean D. Hope who specializes in Zoning and municipal permitting/consulting. Sean was co-developer with Jason Korb of Port Landing - a 100% affordable housing development located in The Port/Kendall Square that opened at the end of 2016 providing 20 units of family friendly housing to the Port neighborhood where Sean's family first moved to from the island of Barbados. Additionally, Hope has partnered with Capstone again to develop Frost Terrace, a 40 apartment, 100% affordable community in Porter Square which is estimated to be complete in Spring of 2021. Sean has also represented developers and property owner on numerous construction projects in Cambridge including new construction, historic preservation and adaptive reuse projects. Prior to entering into private practice in 2008 Sean was an associate member of the Cambridge Board of Zoning Appeal voting on several keys applications including Print Shop Condominiums, an affordable home ownership project developed by CASCAP in 2007. Sean also serves as in-house counsel for Pentecostal Tabernacle in Cambridge and is former member of the Cambridge Historic Society.
- **Preconstruction Cost Estimator:** *Callahan Construction Managers* (www.callahan-inc.com): Established in 1954, Callahan Construction Managers is a full-service construction company experienced with a variety of building types and construction methods in Massachusetts and the Northeast. Callahan specializes in a wide range of preconstruction and construction management services, and including projects in multi-family residential, senior housing, hospitality, retail, corporate office, life sciences, educational, and other markets. Most recently, Callahan began the historic renovation, addition, and new construction project at Squirrelwood Apartments, an affordable housing community owned by Just-A-Start Corporation in Cambridge, MA. The Squirrelwood Apartments contain 88 units and the new construction building will be built to Passive House Standards.
- **Architect:** *Bruner/Cott Architects (Bruner/Cott)* (www.brunercott.com): Bruner/Cott is a mid-sized, full service architecture and planning firm, located in Boston, Massachusetts. Founded 45 years ago, Bruner/Cott is dedicated to enhancing the quality of life, economic vigor, and sense of community through thoughtful, sustainable design. As pioneers in transformative reuse, Bruner/Cott strives to recognize the character and value of an existing structure and understand that sustainable design extends beyond the boundaries of a single building. Whether new construction, transformative reuse, historic preservation, or a large-scale planning project, Bruner/Cott makes buildings that communicate with their surroundings, transforming place by creating architecture of enduring value.

Bruner/Cott is committed to its mission of achieving design excellence through collaboration, creativity, and critical thinking, crafting thoughtful design solutions that fulfill their clients'

aspirations, and enhancing the human experience while respecting the natural environment. The firm's work has been consistently recognized for design, winning awards for renovation, adaptive reuse, and new construction. In the Cambridge/Boston area, Bruner/Cott is working on Frost Terrace (currently under construction), and completed the Lunder Arts Center at Lesley University in Porter Square and The Viridian at 1282 Boylston Street.

- **Legal (General and Zoning):** *Nixon Peabody LLP* (www.nixonpeabody.com): Nixon Peabody is a full-service law firm with more than 600 lawyers nationwide and internationally. Our clients range from developers (for-profit and nonprofit), financing institutions and governmental entities to *Fortune* 100 companies. Nationally recognized in real estate, the firm handles highly complex development and financing transactions involving every class of assets, and has been at the forefront of financing, developing and preserving affordable housing for more than 45 years. In fact, with approximately 25 attorneys and paralegals possessing significant experience working with federal, state and local governmental agencies, NP has one of the largest affordable housing legal teams in the country.

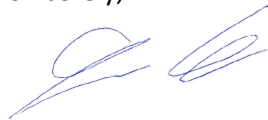
The NP team also handles land use, zoning and permitting for a range of development projects, and is regularly brought into transactions to review site plans and perform zoning analyses. Through this work, the team has developed a comprehensive understanding of the local zoning and permitting processes and the multiple administrative steps that developers face during the course of development, which can include navigating the zoning approval process, obtaining Comprehensive Permit approvals, or establishing zoning overlay district areas and zoning map amendments.

Conclusion

The Project will bring high-quality housing to the Porter Square/North Cambridge neighborhood that will be affordable to a diverse array of low- and moderate-income households. With immediate proximity to rapid transit and essential community services, the Project can provide critically needed housing in an attractive, sustainable development. Designed to provide contemporary amenities with a focus on transit-oriented development and sustainability, the Project will be high-quality family housing in the heart of one of Cambridge's most vibrant neighborhoods.

We look forward to presenting this exciting Project to the Board at your earliest convenience.

Sincerely,

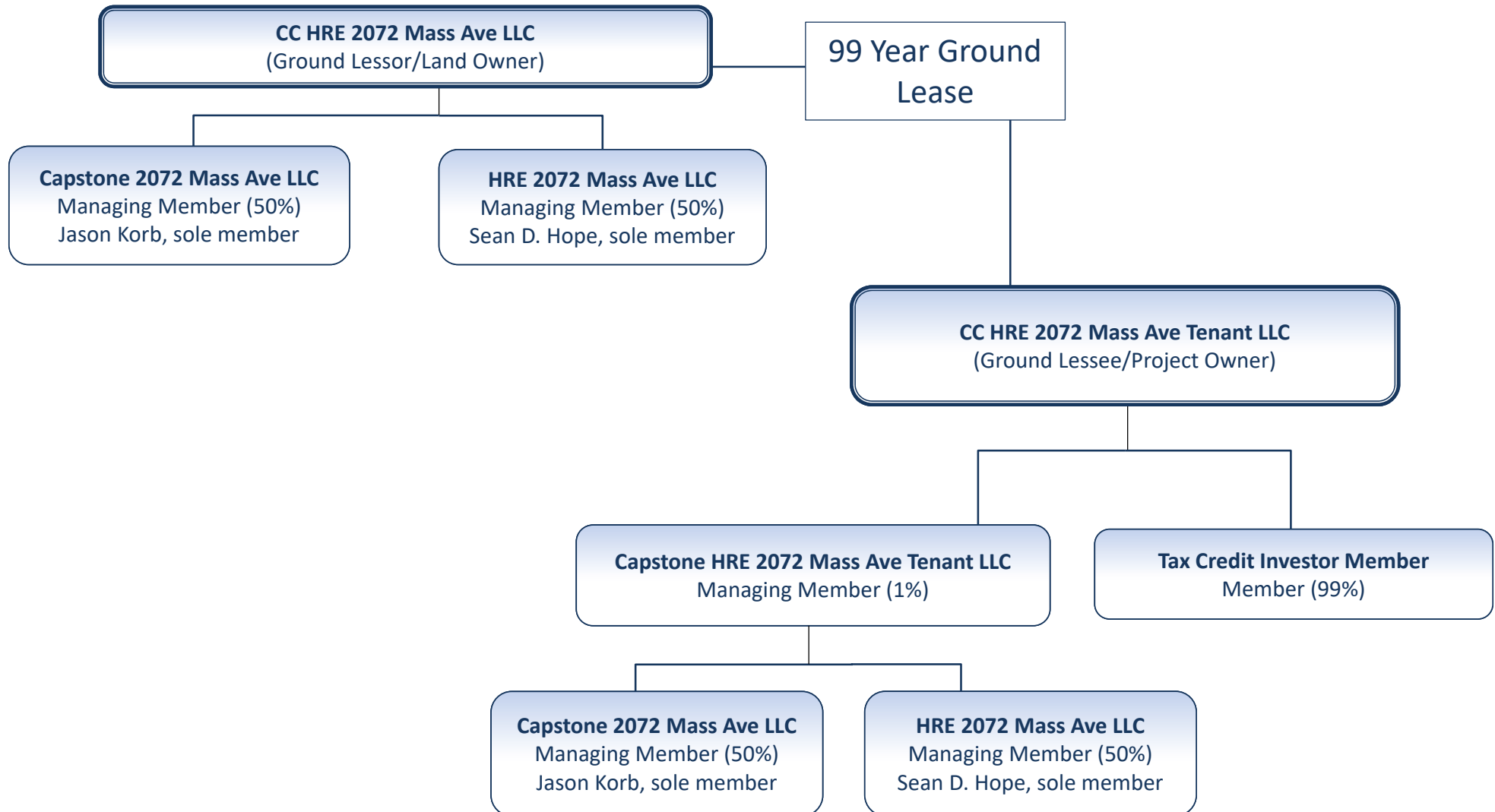


Jason Korb
Managing member of managing member



Sean D. Hope
Managing member of managing member

2072 Mass Ave Apartments Organizational Chart



COMPREHENSIVE PERMIT APPLICATION

2072 MASS AVE APARTMENTS

2072 MASSACHUSETTS AVENUE, CAMBRIDGE 02140

SECTION 2

COMPREHENSIVE PERMIT APPLICATION FORM

COMPREHENSIVE PERMIT APPLICATION

PETITIONER: CC HRE 2072 MASS AVE TENANT LLC

PETITIONER'S ADDRESS: C/O CAPSTONE COMMUNITIES LLC
1087 BEACON STREET, SUITE 302
NEWTON, MA 02459

NAME, ADDRESS, AND
PHONE NUMBER OF
CONTACT PERSON: CAPSTONE 2072 MASS AVE LLC
C/O CAPSTONE COMMUNITIES LLC
1087 BEACON STREET, SUITE 302
NEWTON, MA 02459
ATTN: JASON KORB
617.513.6320

HRE 2072 MASS AVE LLC
C/O HOPE REAL ESTATE ENTERPRISES LLC
907 MASSACHUSETTS AVENUE, SUITE 300
CAMBRIDGE, MA 02139
ATTN: SEAN D. HOPE
617.492.0220

LOCATION OF SITE: 2072 MASSACHUSETTS AVENUE, CAMBRIDGE, MA 02140

DESCRIPTION OF PROJECT: SEE **SECTION 1** (COVER LETTER AND NARRATIVE)

SPECIFY LOCAL
REGULATIONS OR
REQUIREMENTS FROM
WHICH RELIEF IS
REQUESTED: SEE **SECTION 5** REQUESTED WAIVERS FROM LOCAL REQUIREMENTS AND
REGULATIONS

THE PETITIONER IS: A LIMITED DIVIDEND ORGANIZATION

IS THE PROPOSED PROJECT
NEW CONSTRUCTION: THE PROPOSED PROJECT IS NEW CONSTRUCTION

SITE CONTROL: CC HRE 2072 MASS AVE TENANT LLC EXECUTED A 99-YEAR GROUND LEASE
WITH CC HRE 2072 MASS AVE LLC (AN AFFILIATE OF CC HRE 2072 MASS AVE
TENANT LLC) FOR THE LAND AND BUILDING AT 2072 MASSACHUSETTS AVENUE.
CC HRE 2072 MASS AVE LLC CURRENTLY OWNS 2072 MASSACHUSETTS AVENUE.

SITE ELIGIBILITY: THE PETITIONER IS SIMULTANEOUSLY APPLYING TO DHCD FOR SITE ELIGIBILITY
APPROVAL UNDER THE FOLLOWING SUBSIDIES: AFFORDABLE HOUSING TRUST
FUND, DHCD HOUSING STABILIZATION FUND (HSF), HUD HOME PROGRAM
(RENTAL PRODUCTION), STATE LOW INCOME HOUSING TAX CREDIT PROGRAM
AND THE FEDERAL LOW INCOME HOUSING TAX CREDIT PROGRAM (LIHTC).

TOTAL NUMBER DWELLING UNITS PROPOSED: 49
TOTAL NUMBER OF AFFORDABLE RENTAL UNITS: 49
TOTAL NUMBER OF AFFORDABLE HOME OWNERSHIP UNITS: 0

RESIDENT ELIGIBILITY STANDARDS: 41 UNITS AT OR BELOW 60% AMI, 8 UNITS AT OR BELOW 30% AMI, PURSUANT TO THE LOW INCOME HOUSING TAX CREDIT PROGRAM

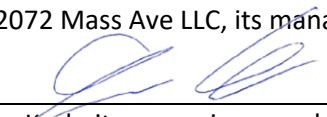
PLEASE SEE **SECTIONS 3 – 16** FOR A COMPLETE DESCRIPTION OF THE PROPOSED PROJECT, AND EACH OF THE FOLLOWING ITEMS:

- a) Site Development Plans - Site development plans showing locations and outlines of proposed building, existing street elevations, traffic patterns and character of open areas, if any, in the neighborhood;
- b) Report on Existing Site Conditions - a summary of conditions in the surrounding areas, showing the location and nature of the existing building, existing street elevations, traffic patterns and character of open areas, if any, in the neighborhood;
- c) Drawings – scaled, architectural drawings, including typical floor plans, typical elevations and sections, and identifying construction type and exterior finish. All projects of five or more units must have Site development plans signed by a registered architect;
- d) Building Tabulations - a tabulation of the proposed building by type, size (number of bedrooms, floor area) and ground coverage, and a summary showing the percentage of the tract to be occupied by the building by parking and other paved vehicular areas and by open areas; (2 copies)
- e) Subdivision Plan - where a subdivision of land is involved, a preliminary subdivision plan; (2 copies)
- f) Utilities Plan - a preliminary utilities plan showing the proposed location and types of sewage, drainage, and water facilities, including hydrants.
- g) Dimensional Form- provided with application; (2 copies)
- h) Photographs – photographs of Site and existing building;
- i) Assessor’s Plat – available at City of Cambridge, Engineering Department, 147 Hampshire Street, Cambridge, MA;
- j) Ownership Certificates – 2 Notarized copies, provided application.

I certify that the information contained herein is true and accurate to the best of my knowledge and belief.

CC HRE 2072 MASS AVE TENANT LLC

By: Capstone 2072 Mass Ave LLC, its managing member


By: Jason Korb, its managing member

By: HRE 2072 Mass Ave LLC, its managing member


By: Sean D. Hope, its managing member

Date: November 10, 2020

COMPREHENSIVE PERMIT APPLICATION

2072 MASS AVE APARTMENTS
2072 MASSACHUSETTS AVENUE, CAMBRIDGE, 02140

SECTION 3
BZA APPLICATION FORM – OWNERSHIP INFORMATION

BZA APPLICATION FORM - OWNERSHIP INFORMATION

***To be completed by OWNER, signed before a notary and returned to
The Secretary of the Board of Zoning Appeals.***

I/We CC HRE 2072 Mass Ave LLC
(OWNER)

Address: c/o Capstone Communities LLC, 1087 Beacon Street Suite 302, Newton MA 02459

State that I/We own the property located at 2072 Massachusetts Avenue,
which is the subject of this zoning application.

The record title of this property is in the name of CC HRE 2072 Mass Ave LLC

*Pursuant to a deed of duly recorded in the date 04/10/2018, Middlesex South
County Registry of Deeds at Book 70850, Page 295; or

Middlesex Registry District of Land Court, Certificate No. _____

Book _____ Page _____



SIGNATURE BY LAND OWNER OR
AUTHORIZED TRUSTEE, OFFICER OR AGENT*

****Written evidence of Agent's standing to represent petitioner may be requested.***

Commonwealth of Massachusetts, County of Middlesex

The above-name SEAN HOPE personally appeared before me,
this 10th of 11, 2020, and made oath that the above statement is true.



Notary

My commission expires JAN. 23, 2026 (Notary Seal)



CHRISTIAN JOSUE MALAGON
Notary Public
Commonwealth of Massachusetts
My Commission Expires Jan. 23, 2026

- If ownership is not shown in recorded deed, e.g. if by court order, recent deed, or inheritance, please include documentation.

BZA APPLICATION FORM - OWNERSHIP INFORMATION

To be completed by OWNER, signed before a notary and returned to The Secretary of the Board of Zoning Appeals.

I/We CC HRE 2072 Mass Ave LLC
(OWNER)

Address: c/o Capstone Communities LLC, 1087 Beacon Street Suite 302, Newton MA 02459


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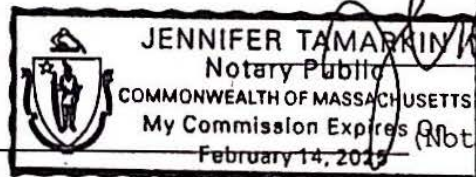

**SIGNATURE BY LAND OWNER OR
AUTHORIZED TRUSTEE, OFFICER OR AGENT***

***Written evidence of Agent's standing to represent petitioner may be requested.**

Commonwealth of Massachusetts, County of Middlesex

The above-name Jason Korb personally appeared before me,
this 9th of November, 2020, and made oath that the above statement is true.

My commission expires



JENNIFER TAMARKIN
Notary Public
COMMONWEALTH OF MASSACHUSETTS
My Commission Expires
February 14, 2025

(Notary Seal).

- If ownership is not shown in recorded deed, e.g. if by court order, recent deed, or inheritance, please include documentation.

COMPREHENSIVE PERMIT APPLICATION

2072 MASS AVE APARTMENTS
2072 MASSACHUSETTS AVENUE, CAMBRIDGE, 02140

SECTION 4
DIMENSIONAL FORM

DIMENSIONAL INFORMATION

Project Address: 2072 Massachusetts Avenue

	EXISTING CONDITIONS	ORDINANCE REQUIREMENTS		REQUESTED CONDITIONS
		BA-2 / BUSINESS A-2	RESIDENCE B	RESIDENTIAL USE
Lot Area (SF)	8,515 SF	No minimum	5,000 SF (min.)	8,515 SF
Lot Width (Ft)	~75.46' @ Massachusetts Avenue	No minimum	50' (min.)	~75.46' @ Massachusetts Avenue
Total Gross Floor Area (GFA)(SF)	1,860 SF	15,755 SF (max.)(g)	608 SF (max.)(g)	57,395 SF
Residential Base	0	13,129 SF (max.)(g)	507 SF (max.)(g)	54,425 SF
Non-Residential Base	1,860 SF	0	0	2,970 SF (d)
Inclusionary Housing Bonus w/20% affordable	N/A	2,626 SF (max.)(g)	101 SF (max.)(g)	N/A
Ratio of Floor Area to Lot Area	Baseline: MAOD:	1.0 / 1.75 (max.) 1.75 for mixed-use / 1.0 for all other uses (max.)	0.5/0.35 for portions exceeding 5,000 SF (max.) 1.75 for mixed-use / 1.0 for all other uses (max.)	6.74 6.74
Residential Base	Baseline: MAOD:	1.75 1.75	0.5/0.35 for portions exceeding 5,000 SF 1.75	6.27 6.27
Non-Residential Base	Baseline: MAOD:	1.0 N/A	N/A N/A	0.47 0.47
Inclusionary Housing Bonus - %	Baseline/MAOD:	20% bonus = 2,626 SF (GFA)(g)	20% bonus = 101 SF (GFA)(g)	N/A
Total Dwelling Units	Baseline/MAOD:	16 (max.)	0	49
Base Units		600 SF / D.U. = 12	2,500 SF / D.U. = 0	49
Inclusionary Bonus units - 20%	N/A	2	0	N/A
Base Lot Area / Unit (SF)		625 SF / D.U. @ 12 UNITS	0 UNITS	174 SF / D.U. @ 49 UNITS
Total Lot Area / Unit (SF)		536 SF / D.U. @ 14 UNITS	0 UNITS	174 SF / D.U. @ 49 UNITS
Building Height(s) (Ft)	MAOD : Requirements:	45' (max.)(Baseline Zoning)	35' (max.)(Baseline Zoning)	
		50' max. (Massachusetts Avenue Overlay District) - Active non-residential ground floor use - Minimum ground-floor use depth of 40' - Ground floor located at mean grade of abutting sidewalk - Minimum 75% Mass Ave frontage occupancy - Minimum 15' ground floor height - Maximum 5,000 sf per ground floor tenant - No bank frontage > 25'		8 Stories / ~89'-8"
Front Yard Setback - Massachusetts Avenue (Ft)(a) [Baseline Zoning - Article 5.33, Table 5-3, footnote (m)]	3.8'	Principal wall plane of an adjacent building facing the same street OR the BA-2 baseline requirement; whichever is less		Building is sited to align with building next door which is right on the sidewalk
Front Yard Setback - Walden Street (Ft)(a)	3.5'	5' (min.)(Baseline/MAOD)	15' (min.)(Baseline Zoning)	0' on Walden
Side Yard Setback - Abut City of Cambridge parking lot (Ft)(a)	42.2'	10' (min.)(Baseline/MAOD)	7'-6" (min.)(sum of 20)(Baseline Zoning)	0' (Abut City of Cambridge parking lot)
Side Yard Setback - Abut Cambridge Housing Authority (Ft)(a)	42.4'	10' min.	7'-6" (min.)(sum of 20)(Baseline Zoning)	0' (Abut Cambridge Housing Authority)
Open Space (% of Lot Area)	78.2%	No minimum		0
Private Open Space	78.2%	No minimum	40% Minimum Private Open Space to Lot Area =	0
Permeable Open Space	0.0%	No minimum	405 SF (min.)(g)	0
Other Open Space (Specify)	N/A	No minimum		0
Off-Street Parking Spaces	Baseline and MAOD:	1 per D.U. = 49 (min.)	N/A (Multifamily dwellings not allowed)	3 accessible (b)(c)
Long-Term Bicycle Parking	0	1:1 first 20 D.U., then D.U. x 1.05 (min.)	1:1 first 20 D.U., then D.U. x 1.05 (min.)	51 (Residential) + 0.4 (Commercial) = 51 (c)
Short-Term Bicycle Parking	0	0.10 per D.U. (min.)	0.10 per D.U. (min.)	0 (e)
Loading Bays	0	N/A	N/A	N/A
Allowable Uses	N/A	Multi Family Residential, Retail, Restaurant, Office, Institutional and Lab	Residential	Residential / Ground floor commercial / restaurant (f) and other uses as described on the Waiver List

(a) Lot is located on a corner. Project team assumed two front and side yards with no rear yard.

(b) Accessible parking requirement rounded up under UFAS (required for Section 504) to three (3) spaces

(c) Commercial Parking is waived under Article 6.36 based on actual quantity required being below four (4) required spots

(d) Garage and bicycle parking exempt from calculation

(e) Project team pursuing public contribution approach for short-term bicycle parking per Article 6.104.2 (b)

(f) Along with other future possible uses as described on the Waiver List

(g) ~1,013 SF of the total lot area is in Residence B, with the remainder in BA-2

COMPREHENSIVE PERMIT APPLICATION

2072 MASS AVE APARTMENTS
2072 MASSACHUSETTS AVENUE, CAMBRIDGE, 02140

SECTION 5
REQUESTED WAIVERS FROM LOCAL REQUIREMENTS
AND REGULATIONS

**LIST OF REQUESTED EXEMPTIONS/
EXCEPTIONS/WAIVERS FROM THE APPLICABLE
CITY OF CAMBRIDGE ORDINANCES AND REGULATIONS
for CC HRE 2072 MASS AVE TENANT LLC
(Updated as of November 10, 2020)**

CAMBRIDGE ZONING ORDINANCE – CHAPTER 17 OF MUNICIPAL CODE

	<u>Section</u>	<u>Provision</u>	<u>Requested Exemption/Exception/Waivers</u>
1.	§ 4.30 Table of Use Regulations and §4.31(g)	Multifamily dwelling is Prohibited in Residence B Zoning District.	The Applicant seeks zoning relief to allow the proposed Multifamily Development.
2.	§ 4.21 Special Classification Rules (Accessory Uses)	Allowed accessory uses include, inter alia, off-street parking, customary home occupations and certain service establishments and eating establishments for residents of multi-family dwellings.	To the extent that the proposed building amenity uses are not enumerated in Section 4.21, the Applicant seeks zoning relief to allow the proposed amenity uses for the building occupants.
3.	§ 4.37(B) (2) Light Industry, Wholesale Business and Storage	Table of Uses in article 4.30 prohibits Catering Commercial kitchen as known as wholesale food products, including bakery, confectionery and dairy products	The Applicant seeks zoning relief to allow a Catering or Commercial Kitchen in the area shown on the plans as Retail.
4.	§ 5.11 Development Standards – General Regulations	No building or structure shall be built nor shall any existing building or structure be enlarged which does not conform to the regulations as to maximum ratio of floor area and lot areas, minimum lot sizes, minimum lot area for each dwelling unit or equivalent, minimum lot width, minimum dimensions of front, side and rear yards and maximum height of structures.	The Applicant seeks zoning relief to allow the proposed Multifamily Development as shown on the Plans. Specific requests are set forth below. A Comprehensive Permit may provide all local permits and approvals per M.G.L. c. 40B.
5.	§ 5.22.1 and § 5.22.3; §5.31 and Table 5-1 - Table of Dimensional Requirements – Residential Districts	Private open space shall be provided and shall be a percentage of the lot area as set forth in Section 5.31. An area designated as private open space must have both a width and a length of at least 15', except for balconies, and may not have a slope greater than 10%. With the exception of balcony areas, private open space shall be accessible to all occupants of a building; not less than ½ of the required private open space shall be provided at ground level or within 10' of the level of the lowest floor used for residential purposes. In the Residence B Zoning District, at least 50% of the required Private open space shall meet all of the requirements of Section 5.22.1. At least 50% of the required Private open space shall meet the definition of Permeable Open Space and shall not be subject to the dimensional limitations of Section 5.22.1 as applied to Private open space.	The Applicant seeks zoning relief to allow the proposed Multifamily Development as shown on the Plans. The proposed private open space has a width and length of less than 15', as shown on the Plans. All private open space is located at ground level. At least 50% of the provided private open space will be Permeable Open Space (as shown on the Plans); however, as described above, the proposed Multifamily Development does not meet the required private open space requirement. A Comprehensive Permit may provide all local permits and approvals per M.G.L. c. 40B.

	<u>Section</u>	<u>Provision</u>	<u>Requested Exemption/Exception/Waivers</u>
6.	§ 5.31 and Table 5-1 - Table of Dimensional Requirements – Residence B and Business A-2 District	<p>Business A-2 Maximum Ratio of Floor Area to Lot Area = 1.0 for Non-residential Uses and 1.75 for Residential Uses.</p> <p>Residence B Maximum Ratio of Floor Area to Lot Area is .50.</p> <p>For those portions of any lot exceeding 5,000 sf, the applicable Maximum Ratio of Floor Area to Lot Area shall be 0.35 for all permitted residential uses.</p>	To waive the requirement for dimensional variances and to allow the proposed Multifamily Development as shown on the Plans. The existing Ratio of Floor Area to Lot Area is approximately 0.22 and the proposed Ratio of Floor Area to Lot Area is approximately 6.74.
7.	§ 5.31 and Table 5-1 - Table of Dimensional Requirements – Residence B and Business A-2 District	<p>Business A-2 Minimum Lot Area for Each Dwelling Unit = 600 sf. Per dwelling unit.</p> <p>Residence B Minimum Lot Area for Each Dwelling Unit = 2,500 per dwelling unit.</p> <p>For those portions of any lot exceeding 5,000 sf, the applicable Minimum Lot Area for Each Dwelling Unit shall be 4,000 sf.</p>	To waive the requirement for dimensional variances and to allow the proposed Multifamily Development as shown on the Plans. The proposed 49 dwelling units cannot comply with the Minimum Lot Area for Each Dwelling Unit requirement.
8.	§ 5.31 and Table 5-1 - Table of Dimensional Requirements – Residence B and Business A-2 District	<p>Business A-2 Minimum Front Yard = 5'</p> <p>Residence B Minimum Front Yard = 15'</p>	To waive the requirement for dimensional variances and to allow the proposed Multifamily Development as shown on the Plans. The smallest existing front yard setback is approximately 0'; the smallest proposed front yard setback will be approximately 0'.
9.	§ 5.31 and Table 5-1 - Table of Dimensional Requirements – Residence B and Business A-2 District	<p>Business A-2 Minimum Side Yard = 10' on both side yards.</p> <p>Residence B Minimum Side Yard = 7'6" (sum of 20).</p>	To waive the requirement for dimensional variances and to allow the proposed Multifamily Development as shown on the Plans. The smallest existing side yard setback is approximately 42.4'; the smallest proposed side yard setback will be approximately 0'.
10.	§ 5.31 and Table 5-1 - Table of Dimensional Requirements – Residence B and Business A-2 District	<p>Business A-2 Maximum Height = 45'</p> <p>Residence B Maximum Height = 35'</p>	Due to the need for a building height of approximately 89' and considering the adjustments that may occur during development of the plans and drawings from design development to full construction drawings, the Applicant seeks a waiver of the height requirement not less than 89' and not greater than 95'.
11.	§ 6.36.1 – Schedule of Parking and Loading Requirements	<p>In Business A-2 District, there is a one parking space per dwelling requirement.</p> <p>In Residence B district, multifamily dwellings are not allowed; therefore, Section 6.36.1(g) states that there is no applicable requirement for off-street parking for multifamily dwellings in the Residence B district.</p>	<p>Although there is no technical requirement for off-street parking in the Residence B district for a multifamily use, the Applicant seeks a waiver to allow the proposed Multifamily Dwelling with three (3) proposed accessible off-street parking spaces and two (2) drop off spaces as shown on the Plans.</p> <p>A Comprehensive Permit may provide all local permits and approvals per M.G.L. c. 40B.</p>

	<u>Section</u>	<u>Provision</u>	<u>Requested Exemption/Exception/Waivers</u>
12.	§6.42 – Design and Maintenance of Off-Street Parking Facilities – Dimensions for Off-Street Parking Spaces	Dimensions for off street parking spaces. Aisle Width of 22’ required.	To waive the requirement for a dimensional variance and to allow the proposed Multifamily Development as shown on the Plans. The proposed dimension of parking spaces shall be less than minimum required aisle width will be less than the required 22’.
13.	§6.43.4(c) – Design and Maintenance of Off-Street Parking Facilities – Driveways	Grade and design of driveway shall provide a clear view to the driver of any car exiting from the facility, or traffic on the street and of pedestrians.	Due to constraints of the driveway location and building, the clear view from the proposed driveway to Massachusetts Avenue when looking south may be compromised. The Applicant requests a waiver from the requirement for a variance and to allow the proposed Multifamily Development as shown on the Plans.
14.	Article 19 – Project Review	Establishes traffic and urban design standards for development projects exceeding 20,000 gross square feet that are likely to have a significant impact on abutting properties and the surrounding urban environment. Requirements include a Special Permit from the Planning Board including Traffic Impact Review (including a Traffic Impact Study), Urban Design Review, Tree Study, Sewer Service Infrastructure Review, Water Service Infrastructure Review, Noise Mitigation Review, Citywide Advisory Development Consultation and specific building and site plan elements.	To waive all of the applicable Article 19 requirements for a Planning Board Special Permit and other requirements and to allow the proposed Multifamily Development that exceeds 20,000 gross square feet. A Comprehensive Permit may provide all local permits and approvals per M.G.L. c. 40B.
15.	§20.100 Massachusetts Avenue Overlay District	Contains specific requirements for projects located within the Massachusetts Avenue Overlay District, including use regulations, dimensional requirements, and design standards. Projects are also required to comply with the Large Project Review process.	To waive all of the applicable requirements of Section 20.100 without the need for a Planning Board Special Permit and to allow the proposed Multifamily Dwelling without a Special Permit from the Planning Board. A Comprehensive Permit may provide all local permits and approvals per M.G.L. c. 40B.

MUNICIPAL CODE

	<u>Section</u>	<u>Provision</u>	<u>Requested Exemption/Exception/Waivers and Notes</u>
1.	Chapter 12.04.020 - Street Numbers	The City Council shall assign numbers to houses.	To waive the requirements of this provision of the Municipal Code so that no separate approval is required from the City Council. To allow the Board of Zoning Appeals to delegate the task of assigning house number(s) for the proposed Multifamily Development to the Building Department, to be completed prior to issuance of Certificates of Occupancy. A Comprehensive Permit may provide all local permits and approvals per M.G.L. c. 40B.
2.	Chapter 12.12.010 – Street Excavations	No excavation in a public way or disturbance of any sidewalk without a license from the Superintendent of Streets or approval from the City Council.	To waive the requirements of this provision of the Municipal Code so that no separate approval is required from the Superintendent of Streets and/or the City Council. To allow the Board of Zoning Appeals to allow any necessary excavation or disturbance of any public way or sidewalk needed for the construction and operation of the Multifamily Development, as shown on the Plans. A Comprehensive Permit may provide all local permits and approvals per M.G.L. c. 40B.

COMPREHENSIVE PERMIT APPLICATION

2072 MASS AVE APARTMENTS
2072 MASSACHUSETTS AVENUE, CAMBRIDGE, 02140

SECTION 6 PARKING AND TRAFFIC INFORMATION

- o Parking and Traffic Assessment by Vanasse & Associates, Inc.
- o Plans
 - Bike Room Layout Plan
 - Bus Stop Layout Plan
 - Parking Sight Lines

MEMORANDUM

TO: Mr. Jason Korb
Sean Hope, Esq.
CC HRE 2072 Mass Ave LLC
c/o Capstone Communities LLC
1087 Beacon Street, Suite 302
Newton, MA 02459

FROM: F. Giles Ham, P.E. *and*
Derek Roach, E.I.T
Vanasse & Associates, Inc.
35 New England Business Center Drive
Suite 140
Andover, MA 01810
(978) 474-8800

DATE: November 9, 2020

RE: 8716

SUBJECT: Parking and Traffic Assessment - Proposed Residential Development
2072 Massachusetts Avenue, Cambridge, Massachusetts

Vanasse & Associates, Inc. (VAI) has completed a Parking and Traffic Assessment of a proposed 49-unit 100% affordable residential development with 1,040 square feet (sf) of ground floor retail to be located at 2072 Massachusetts Avenue in Cambridge, Massachusetts (Project). This is a revised study of the October 13, 2020 memorandum. The entire study was redone due to some Somerville streets being included in the October study area. Two short-term drop-off/pick-up spaces and three handicap spaces are proposed on-site. Contained within this memorandum is a parking supply and demand analysis within a quarter mile radius of site, estimated trip generation by mode split and a recommended a Travel Demand Management plan (TDM) for the proposed project.

PARKING SUPPLY AND DEMAND ANALYSIS

A comprehensive field inventory of the existing parking supply within approximately a quarter-mile radius of the Project was conducted in September 2020. While residents want to park as close as possible to their residence, the study area radius established by the city is a relatively short walk (5 minutes). Figure 1 depicts the study area. The field inventory consisted of on-street parking by quantity and type (handicapped, permit only and regulations). The study area was subdivided into twenty-seven (27) parking zones in order to identify parking trends occurring within the study area. Figure 2 identifies the parking regulations and number of parking spaces (928) in the area. Figure 3 depicts the residential permit parking spaces, handicap spaces, and spaces with no regulations which total 806 of the 928 spaces.

In order to determine the availability of parking spaces, a parking demand survey was conducted during a typical weekday (Tuesday October 20, 2020). The parking observations were conducted every 2 hours from 10:00 AM to 10:00 PM. Table 1 and Figure 4 summarize the parking demand observations for the available resident parking.

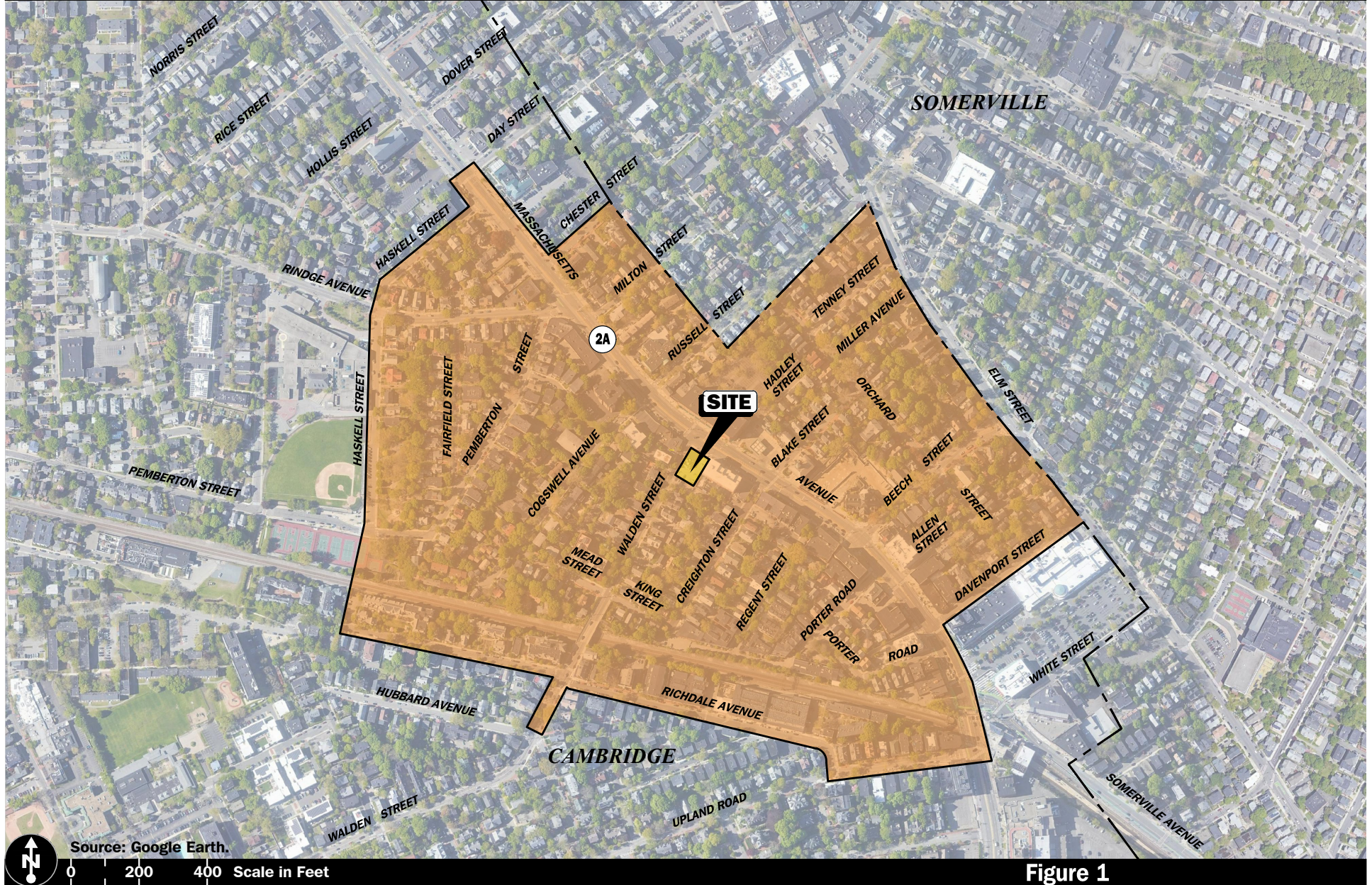


Figure 1
Parking Supply Area

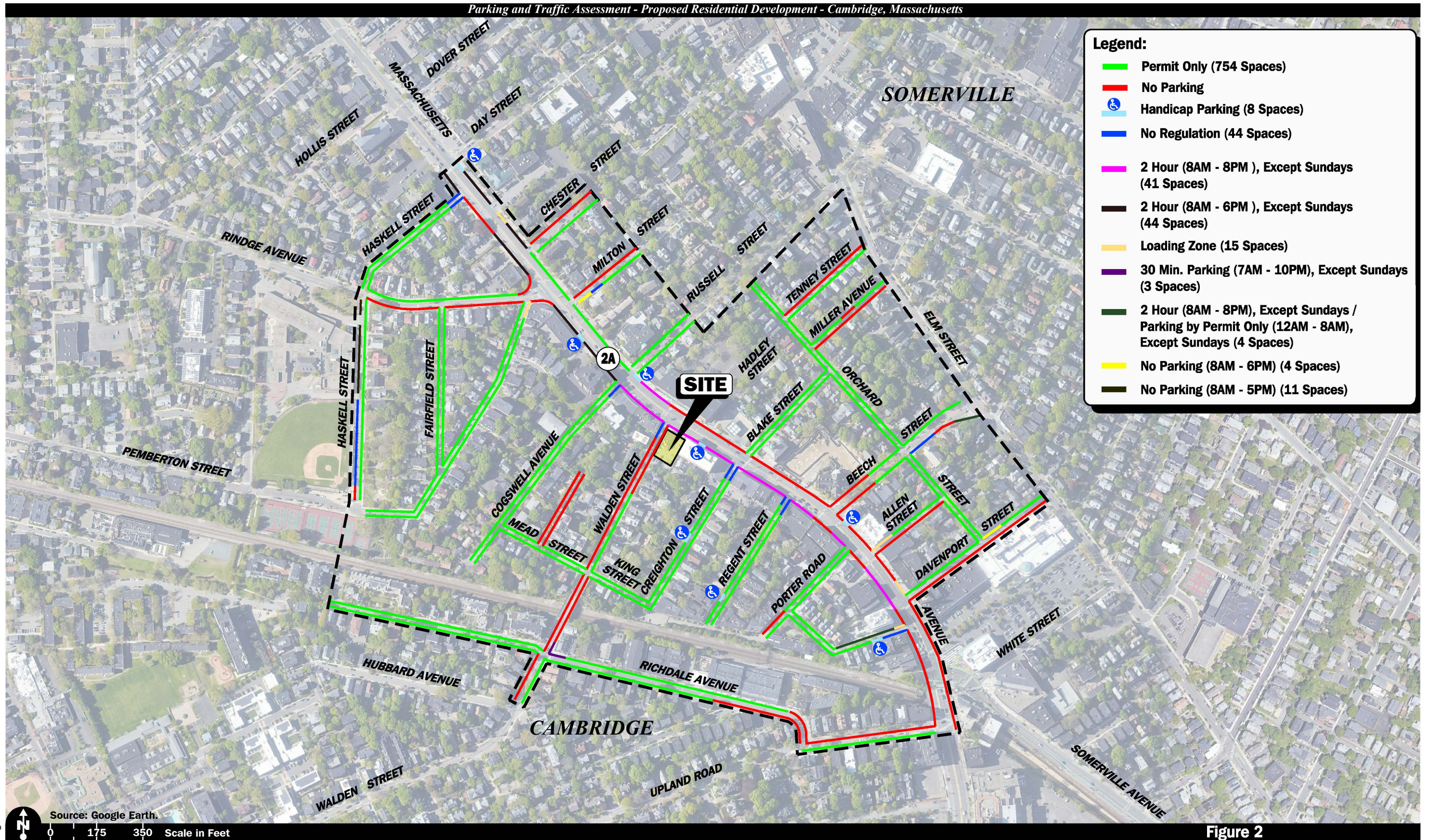


Figure 2

Existing Parking Regulations

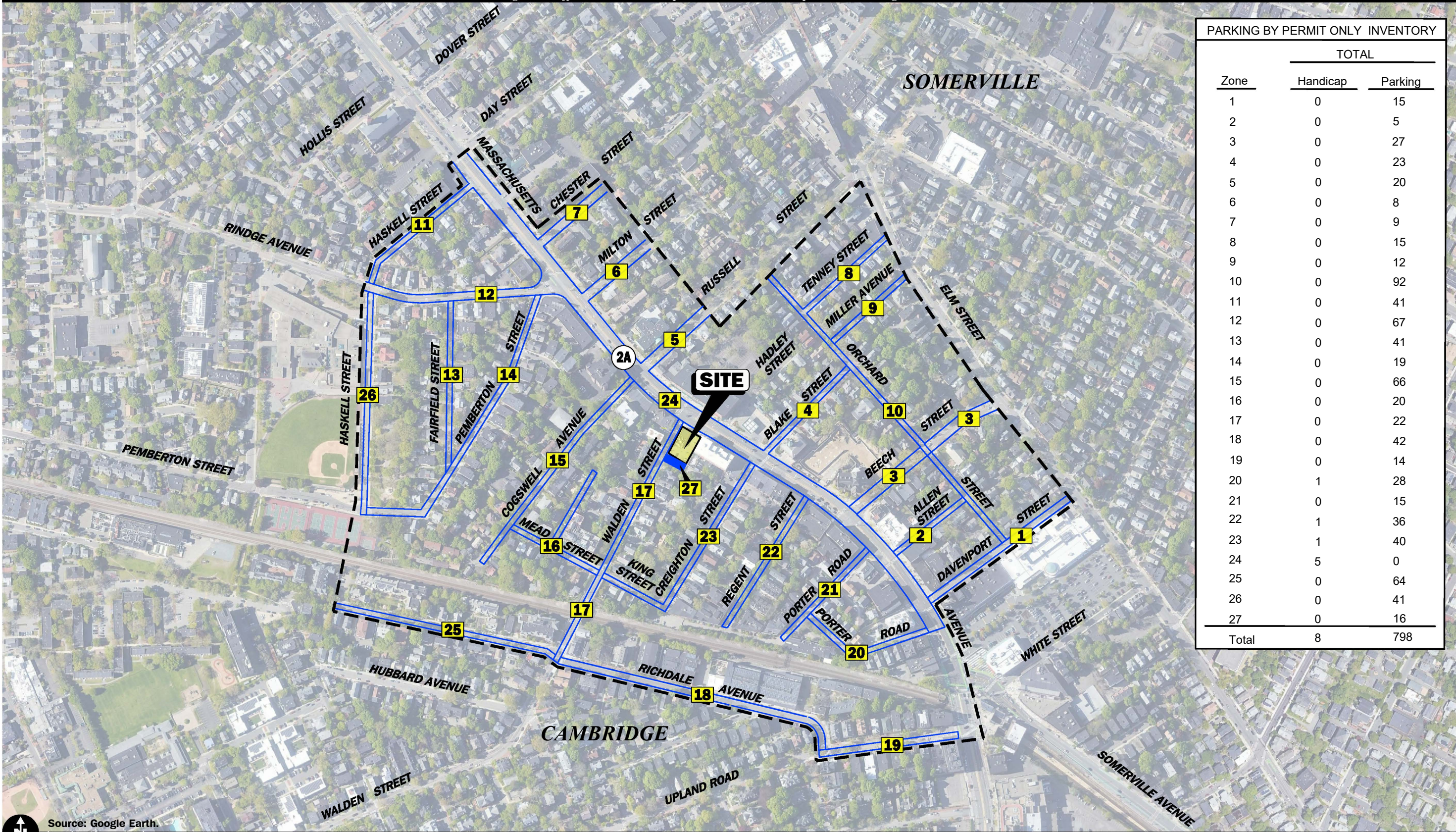


Figure 3
Existing Permit Only Parking /
No Regulation Spaces

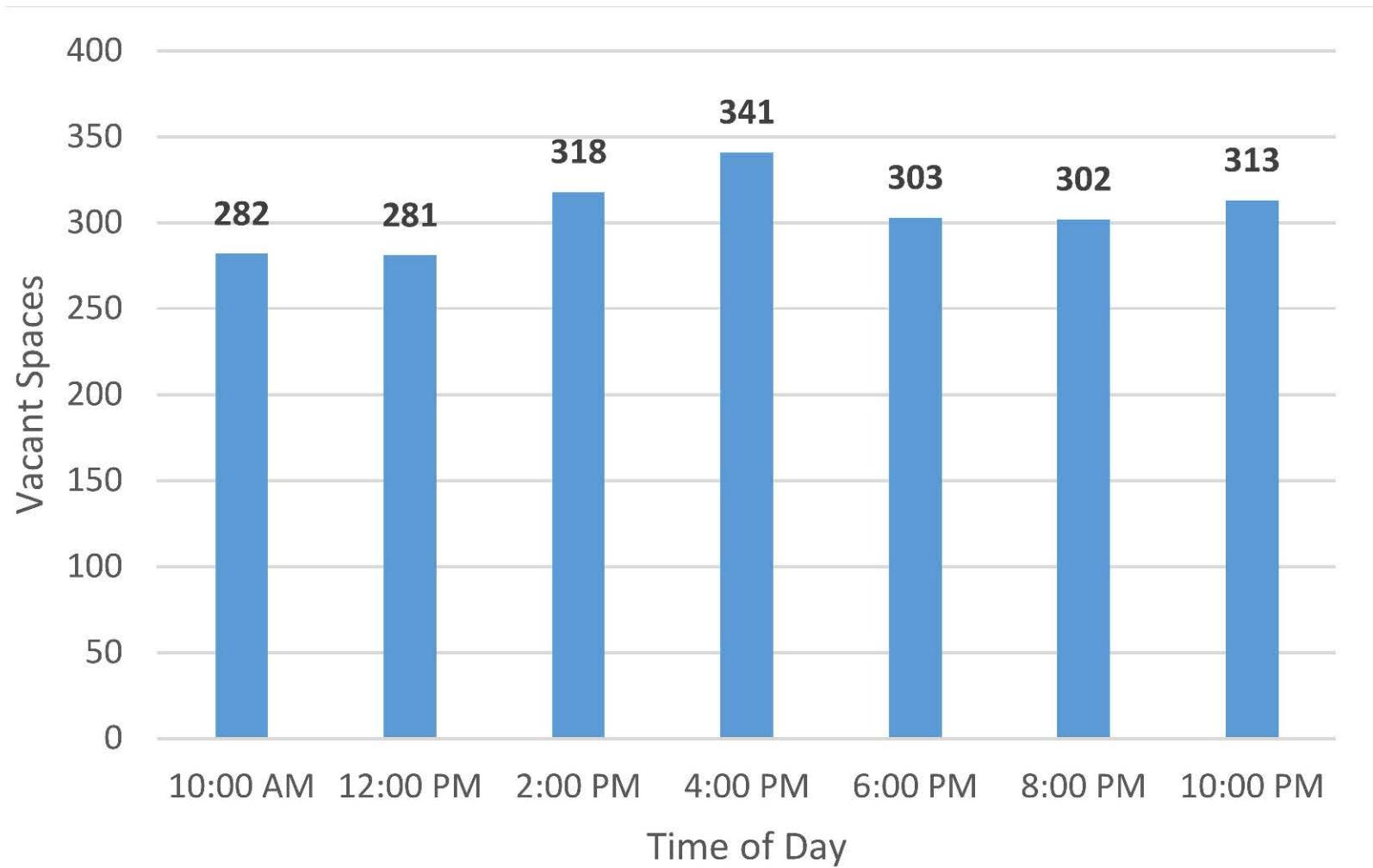


Figure 4

Parking Chart

Table 1
PARKING OBSERVATIONS
October 20, 2020

Zone	Parking Supply	Vacant Spaces						
		10:00 AM	12:00 PM	2:00 PM	4:00 PM	6:00 PM	8:00 PM	10:00 PM
1	20	5	3	8	7	11	10	16
2	6	0	1	1	1	2	1	2
3	30	3	4	6	6	4	6	3
4	23	13	10	11	7	7	7	6
5	22	4	4	4	5	1	6	4
6	8	2	3	3	3	2	2	2
7	9	1	1	2	2	4	0	0
8	15	6	5	6	6	3	3	3
9	12	6	7	7	8	4	6	7
10	92	21	19	18	20	12	14	15
11	41	13	10	9	17	7	9	11
12	70	18	28	30	25	27	30	26
13	41	13	11	13	15	12	9	6
14	19	4	6	9	8	10	10	11
15	66	16	17	14	23	13	13	14
16 ^a	20	7	5	6	4	11	10	8
17 ^a	23	7	6	5	4	5	7	6
18 ^a	44	21	20	24	23	19	14	12
19	14	1	0	0	0	0	0	0
20	35	6	4	6	13	9	9	8
21	15	1	1	1	3	3	1	1
22 ^a	37	11	9	14	10	9	8	9
23 ^a	41	2	4	2	5	8	6	7
24	93	47	43	55	60	61	63	82
25	64	25	27	27	26	26	28	23
26	52	20	24	28	29	24	24	21
27 ^a	16	9	9	9	11	9	6	10
TOTAL	928	282	281	318	341	303	302	313

^aHighlighted rows include zones within two block of the site.

As shown in Table 1 and Figure 4 the peak demand occurs at 12:00 PM when 281 spaces were vacant. The Project has proposed only 2 short-term drop-off/pick-up spaces and 3 handicap spaces on-site and all other parking will be on-street. By not providing parking, the Project impact will be minimized as auto ownership will be discouraged. An auto ownership of 0.50 vehicles per unit equates to 25 vehicles. Zoning requires one space per unit. U.S. Census and 2018 American Community Survey data for Census Tract 3547, the tract in which the Project is located, indicates that 32 percent of trips are automobile trips. This suggests that the affordable residential unit ownership may be lower than the 50 percent. The 50 percent auto ownership is conservative. Residents of the site will want to park as close to the site as possible. Focusing on Zones 15, 16, 17, 22, 23, and 27 which are all 2 blocks from the site, there is a minimum of 50 spaces available during the peak parking demand at 12:00 PM. At 10:00 PM there are 54 spaces available in close proximity to the site. Based upon the parking analysis, there is more than adequate on-street parking to accommodate the Project.

PROPOSED SITE TRIP GENERATION

Traffic volumes expected to be generated by the Project were determined by using the ITE *Trip Generation*¹ manual and utilized Land Use Code (LUC 221), Multifamily Housing (Mid-Rise) and LUC 820, Shopping Center. It should be noted that the project is proposing affordable housing units which have lower vehicle trip rates than market rate units therefore the actual trip increases due to the development will be less than what is estimated by LUC 221. In addition, it is expected that a significant portion of the residents of the Project will utilize alternative modes of transportation other than automobiles. Based upon the U.S. Census and 2018 American Community Survey data for Census Tract 3547, the tract in which the Project is located, the mode split characteristics of the Project are estimated as follows: 32 percent automobile trips; 43 percent transit; 10 percent walk; 6 percent bicycle, and 9 percent other trips.

The Project trip generation by mode is summarized in Table 2.

As can be seen in Table 2, the Project is expected to generate approximately 98 vehicle trips on an average weekday (49 entering/49 exiting), with approximately 6 vehicle trips (2 entering/4 exiting) expected during the weekday morning peak-hour. During the weekday evening peak hour, the Project is expected to generate approximately 9 new vehicle trips (5 entering/4 exiting).

¹*Trip Generation*, 10th Edition; Institute of Transportation Engineers; Washington, DC; 2017.

Table 2
PROJECT TRIP GENERATION SUMMARY

Time Period/Direction	ITE LUC 221 (A) ^a	ITE LUC 820 (B) ^b	ITE Total Vehicle Trips (C=A+B)	Vehicle Occupancy Rate (D) ^c	Person Trips						Total Vehicle Trips (K=F/D)
					Total Trips (E=C*D)	Auto Trips ^d (F=E*0.32)	Transit Trips ^e (G=E*0.43)	Walk Trips ^f (H=E*0.10)	Bicycle Trips ^g (I=E*0.06)	Other Trips ^h (J=E*0.09)	
<i>Weekday Daily:</i>	266	40	306	1.07	328	104	142	32	20	30	98
<i>Weekday Morning Peak Hour:</i>											
Entering	4	1	5	1.07	5	2	2	1	0	0	2
Exiting	<u>13</u>	<u>0</u>	<u>13</u>	1.07	<u>14</u>	<u>4</u>	<u>7</u>	<u>1</u>	<u>1</u>	<u>1</u>	<u>4</u>
Total	17	1	18	1.07	19	6	9	2	1	1	6
<i>Weekday Evening Peak Hour:</i>											
Entering	13	2	15	1.07	16	5	7	2	1	1	5
Exiting	<u>9</u>	<u>2</u>	<u>11</u>	1.07	<u>12</u>	<u>4</u>	<u>5</u>	<u>1</u>	<u>1</u>	<u>1</u>	<u>4</u>
Total	22	4	26	1.07	28	9	12	3	2	2	9

^aBased on ITE LUC 221 Multifamily (Mid-Rise), 49 units.

^bBased on ITE LUC 820 Shopping Center, 1,040 sf.

^cSource: United States Census and American Community Survey 2018 5-year estimates; Census Tract 3547.

^dAutomobile trips are 32 percent of total person trips, Census Tract 3547.

^eTransit trips are 43 percent of total person trips, Census Tract 3547.

^fWalking trips are 10 percent of total person trips, Census Tract 3547.

^gBicycle trips are 6 percent of total person trips, Census Tract 3547.

^hOther trips are 6 percent of total person trips, Census Tract 3547. Includes work from home.

TRANSPORTATION DEMAND MANAGEMENT (TDM)

Reducing the amount of traffic generated by the Project is an important component of the development plan. The goal of the TDM plan is to reduce the use of Single Occupant Vehicles by encouraging car/vanpooling, bicycle commuting, the use of public transportation and pedestrian travel. The following measures will be implemented as a part of the proposed project management team in an effort to reduce the number of vehicle trips generated:

- Designate an on-site employee as the site's Transportation Coordinator to oversee marketing and promoting of transportation options at the site.
- Provide new residents transportation information packets with information on getting around Cambridge sustainably.
- Install a real-time transit display screen in the lobby to make it simpler for residents, visitors, and employees to access real-time transit and Bluebikes availability information in the area. The screens will also post other useful information on single occupancy modes of travel, such as carpool/vanpool to supermarkets, etc.
- Subsidize 100 percent of the cost of a MBTA T pass for employees (building property managers/maintenance staff) or \$240 annual reimbursement for bike maintenance for employees who choose to commute by bike.
- Organize orientation sessions with residents to teach biking rules, safe biking measures, basic maintenance and repairs and help identify bike routes to various locations.
- Bicycle racks and a bicycle "Fix-it" station will be provided on-site.
- Annually, upon initial move-in and lease renewal, residents will be offered the choice of: (1) annual Bluebikes membership (including one-time discounted helmet through bluebikes), (2) \$90 credit for ride share service; (3) 1-month adult MBTA Monthly LinkPass, and/or (4) 3-month Student or Senior Monthly LinkPass. This will be provided PER RESIDENT (not per household) on an annual basis.

The above strategies will encourage non-auto travel by the residents.

SUMMARY

In summary, a detailed parking survey was completed in the area of the Project and based upon this data it can be concluded that there is more than sufficient availability of on-street parking to accommodate the Project. The Project proponent is committed to implementing a Travel Demand Management plan which promotes alternatives modes of transportation and will minimize the Project's impact on available on-street parking and traffic in the area.

APPENDIX

TRIP GENERATION CALCULATIONS

TRIP GENERATION CALCULATIONS

Institute of Transportation Engineers (ITE)
Trip Generation, 10th Edition
Land Use Code (LUC) 221 - Multifamily Housing (Mid-Rise)

Average Vehicle Trips Ends vs: Dwelling Units
Independent Variable (X): 49

AVERAGE WEEKDAY DAILY

$$T = 5.45 * (X) - 1.75$$

$$T = 5.45 * 49 - (1.75)$$

$$T = 265.30$$

$$T = 266 \text{ vehicle trips}$$

with 50% (133 vpd) entering and 50% (133 vpd) exiting.

WEEKDAY MORNING PEAK HOUR OF ADJACENT STREET TRAFFIC

$$\ln T = 0.98 * \ln(X) - 0.98$$

$$\ln T = 0.98 * \ln 49 - (0.98)$$

$$\ln T = 2.83$$

$$T = 17.01$$

$$T = 17 \text{ vehicle trips}$$

with 26% (4 vph) entering and 74% (13 vph) exiting.

WEEKDAY EVENING PEAK HOUR OF ADJACENT STREET TRAFFIC

$$\ln T = 0.96 * \ln(X) - 0.63$$

$$\ln T = 0.96 * \ln 49 - (0.63)$$

$$\ln T = 3.11$$

$$T = 22.33$$

$$T = 22 \text{ vehicle trips}$$

with 61% (13 vph) entering and 39% (9 vph) exiting.

Institute of Transportation Engineers (ITE)
Trip Generation, 10th Edition
Land Use Code (LUC) 820 - Shopping Center

Average Vehicle Trips Ends vs: 1,000 Square Feet Gross Leasable Area
Independent Variable (X): 1.040

AVERAGE WEEKDAY DAILY

$$T = 37.75 * X$$

$$T = 37.75 * 1.040$$

$$T = 39.26$$

$$T = 40 \text{ vehicle trips}$$

with 50% (20 vpd) entering and 50% (20 vpd) exiting.

WEEKDAY MORNING PEAK HOUR OF ADJACENT STREET TRAFFIC

$$T = 0.94 * (X)$$

$$T = 0.94 * 1.040$$

$$T = 0.98$$

$$T = 1 \text{ vehicle trips}$$

with 62% (1 vph) entering and 38% (0 vph) exiting.

WEEKDAY EVENING PEAK HOUR OF ADJACENT STREET TRAFFIC

$$T = 3.81 * X$$

$$T = 3.81 * 1.040$$

$$T = 3.96$$

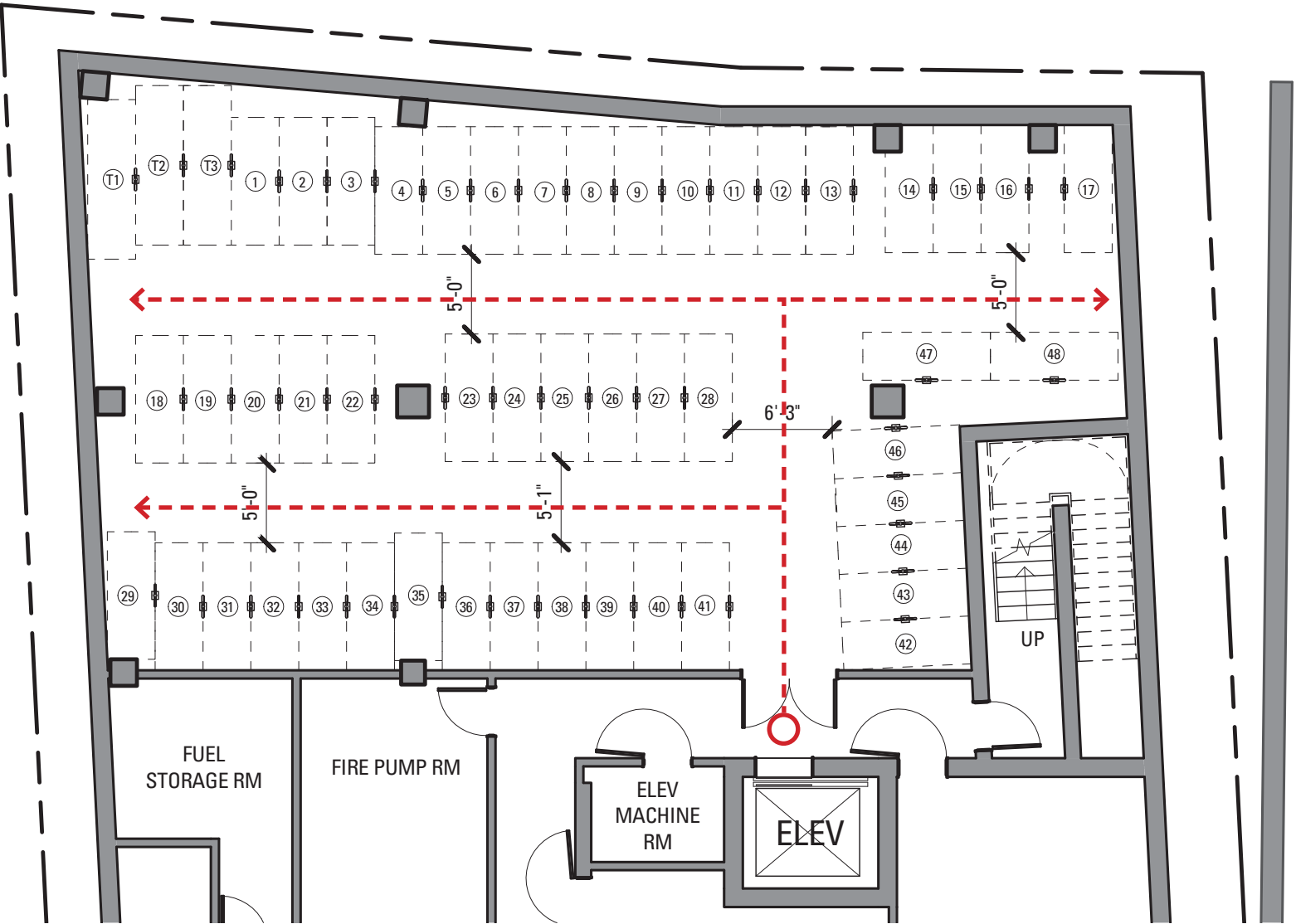
$$T = 4 \text{ vehicle trips}$$

with 48% (2 vph) entering and 52% (2 vph) exiting.

COMMUTING CHARACTERISTICS BY SEX

Note: This is a modified view of the original table produced by the U.S. Census Bureau. This download or printed version may have missing information from the original table.

Census Tract 3547, Middlesex County, Massachusetts				
Label	Total		Male	
	Estimate	Margin of Error	Estimate	Margin of Error
▼ Workers 16 years and over	1,752	±232	865	
▼ MEANS OF TRANSPORTATION TO WORK				
▼ Car, truck, or van	31.6%	±6.9	32.8%	
Drove alone	27.9%	±6.6	28.9%	
▼ Carpooled	3.8%	±2.2	3.9%	
In 2-person carpool	3.3%	±2	3.0%	
In 3-person carpool	0.0%	±2	0.0%	
In 4-or-more person carpool	0.5%	±0.7	0.9%	
Workers per car, truck, or van	1.07	±0.05	1.07	
Public transportation (excluding taxicab)	42.9%	±6.8	41.3%	
Walked	9.5%	±3.9	5.3%	
Bicycle	5.8%	±3.7	6.4%	
Taxicab, motorcycle, or other means	1.7%	±1.3	3.4%	
Worked at home	8.6%	±3.9	10.9%	
▼ PLACE OF WORK				
▼ Worked in state of residence	99.0%	±1.2	98.8%	
Worked in county of residence	64.4%	±6.2	63.2%	
Worked outside county of residence	34.5%	±6	35.6%	
Worked outside state of residence	1.0%	±1.2	1.2%	
▼ Living in a place	100.0%	±2	100.0%	
Worked in place of residence	42.2%	±7.3	41.3%	
Worked outside place of residence	57.8%	±7.3	58.7%	
Not living in a place	0.0%	±2	0.0%	
▼ Living in 12 selected states	100.0%	±2	100.0%	
Worked in minor civil division of residence	42.2%	±7.3	41.3%	
Worked outside minor civil division of residence	57.8%	±7.3	58.7%	
Not living in 12 selected states	0.0%	±2	0.0%	
▼ Workers 16 years and over who did not work at home	1,602	±237	771	
▼ TIME LEAVING HOME TO GO TO WORK				
12:00 a.m. to 4:59 a.m.	2.8%	±2.8	2.5%	
5:00 a.m. to 5:29 a.m.	0.0%	±2.2	0.0%	
5:30 a.m. to 5:59 a.m.	0.0%	±2.2	0.0%	
6:00 a.m. to 6:29 a.m.	1.1%	±1.2	1.2%	
6:30 a.m. to 6:59 a.m.	6.2%	±3.4	6.0%	
7:00 a.m. to 7:29 a.m.	16.6%	±4.4	19.8%	
7:30 a.m. to 7:59 a.m.	14.9%	±5.7	8.8%	
8:00 a.m. to 8:29 a.m.	21.4%	±5.1	24.3%	
8:30 a.m. to 8:59 a.m.	10.4%	±4.0	8.8%	



2050 MASS AVE
6 STORIES

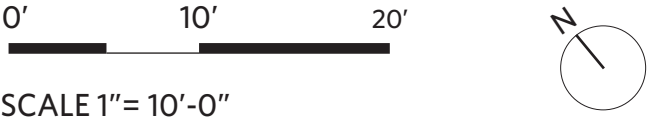
BIKE PARKING

<u>REQUIRED BY ZONING :</u>	<u>PROVIDED BY DEVELOPMENT :</u>
LONG-TERM: 48	STANDARD: 48
TANDEM: 3	TANDEM: 3
SHORT TERM: 5	SHORT TERM: 0 (e)
TOTAL: 56	TOTAL: 51

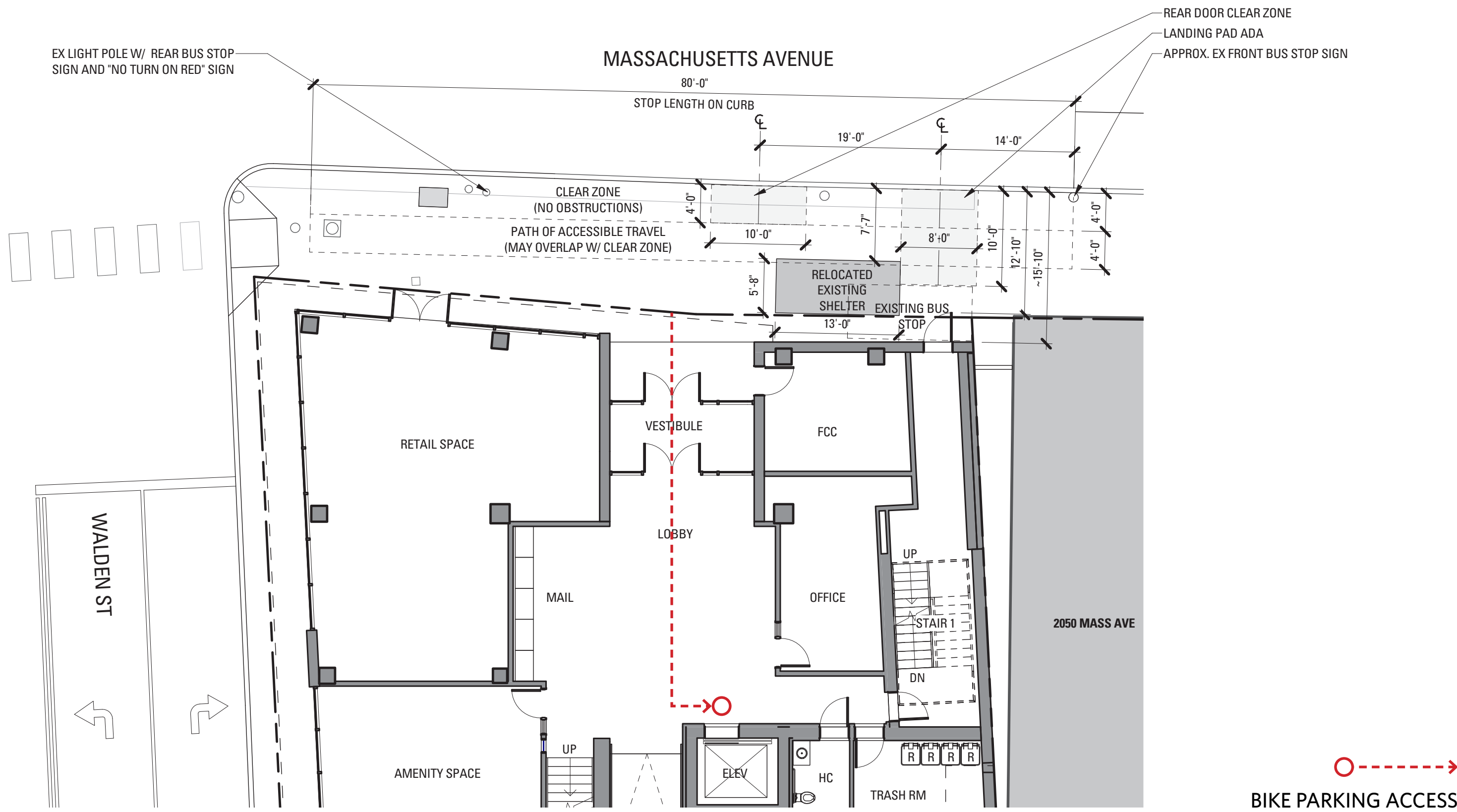
(c) COMMERIAL PARKING IS WAIVED UNDER 6.36 BASED ON ACTUAL QUANTITY REQUIRED BEING BELOW 4 REQUIRED SPOTS

(e) PROJECT TEAM SEEKING ALTERNATIVE PUBLIC CONTRIBUTION OPTION OF SATISFYING SHORT-TERM BIKE PARKING REQUIREMENTS PER ARTICLE 6.104.2 (b)

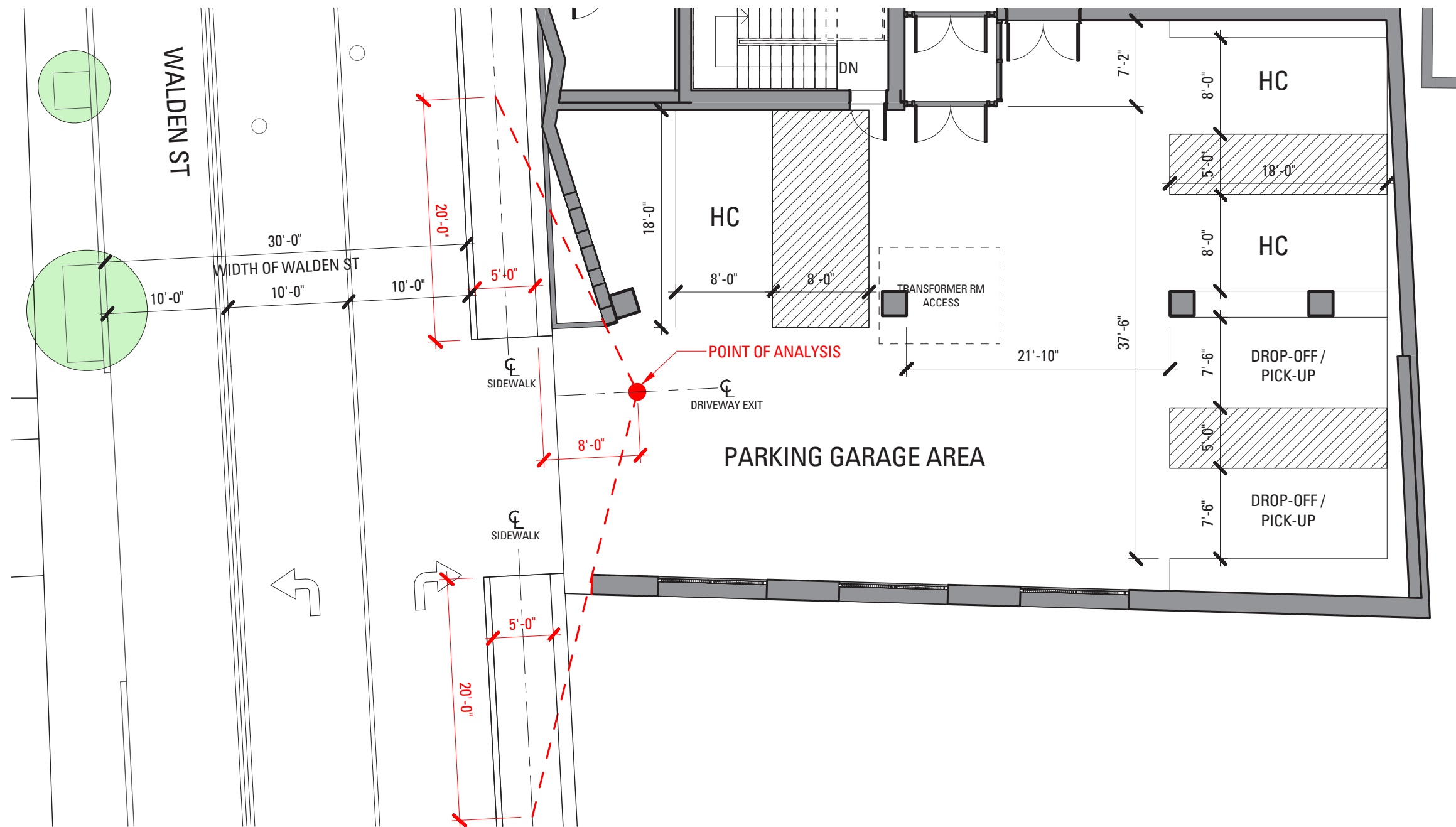
 BIKE PARKING ACCESS



BIKE ROOM LAYOUT PLAN



BUS STOP LAYOUT PLAN



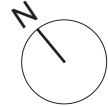
PARKING SIGHT LINES

Bruner/Cott
ARCHITECTS

APPLICANT: CC HRE 2072 MASS AVE TENANT LLC

0' 10' 20'

SCALE 1"= 10'-0"



2072 MASSACHUSETTS AVENUE
NOVEMBER 10, 2020

COMPREHENSIVE PERMIT APPLICATION

2072 MASS AVE APARTMENTS
2072 MASSACHUSETTS AVENUE, CAMBRIDGE, 02140

SECTION 7
CIVIL ENGINEER'S NARRATIVE

2072 Massachusetts Avenue – Nitsch Project #14047
Infrastructure Narrative

Sanitary Sewer

The existing site is currently comprised of a restaurant with associated access, parking, and utilities.

A breakdown of the site's existing sanitary sewer flow rates are as follows:

Existing Sanitary Sewer Flows (per 310 CMR 15.203)

Use	Unit Sewer Flow Rate (gpd)	Size	Existing Sewer Flow Rate (gpd)
Restaurant	35 (per seat)	37 seats	1,295
Total			1,295

The project proposes to redevelop the site into an eight-story building containing affordable housing units with ground floor retail. The sanitary sewage from the proposed building will be collected in new onsite sewer infrastructure and discharge into the existing 8-inch sewer main in Walden Street.

A breakdown of the site's proposed sanitary sewer design flow rates are as follows:

Proposed Sanitary Sewer Flows (per 310 CMR 15.203)

Use	Unit Sewer Flow Rate (gpd)	Size	Proposed Sewer Flow Rate (gpd)
Residential Occupancy	110 (per bedroom)	98 bedrooms	10,780
Retail	50 (per 1000 sf)	4,380 square feet	219
Total			10,999

Stormwater/Drainage

The proposed drainage system is designed in compliance with MassDEP's Stormwater Management Standards, as well as the City of Cambridge's design requirements. These requirements include the use of NOAA Atlas 14 precipitation frequency estimates and the 2030 City of Cambridge storms in stormwater modeling, as well as phosphorus removal.

The majority of the stormwater runoff from the site will be collected and directed to an onsite stormwater detention/infiltration system located underneath the proposed parking garage. The system has been designed to meet the City's attenuation and treatment requirements. The stormwater detention/infiltration system will overflow to the City's combined sewer main in Walden Street.

Peak Runoff Rates

The Project proposes to collect the entire roof area (a majority of the site), including 1,170 square feet of green roof and direct it to an infiltration system. The proposed stormwater system is able to achieve the reduction of the peak flow rate for the proposed 25-year storm event to the existing 2-year storm event using 2030 Cambridge storm events. The project will also reduce the peak flow rates of all analyzed storm events to meet the MassDEP Stormwater Standards.

Peak Rates of Runoff (cfs) for the Total Site

Storm Event	Existing (Total, DP1)	Proposed
2-Year	0.62*	0.31
10-Year	1.05	0.49
25-Year	1.36	0.61*
100-Year	1.91	1.88

*Indicates comparison for 25-to-2 Requirement for City of Cambridge

Water Quality

The proposed infiltration system will provide 80% TSS removal rate as required by the MassDEP Stormwater Standards, as well as 65% Total Phosphorus removal as required by the City of Cambridge.

PHOSPHORUS REMOVAL CALCULATIONS						
<u>Land Conversion</u>						
	Existing			Proposed		
	Area ac	Export Rate lbs/acre/yr	Phosphorus Load lbs P/yr	Area ac	Export Rate lbs/acre/yr	Phosphorus Load lbs P/yr
Impervious - Pavement	0.15	1.78	0.27	0.02	2.32	0.04
Impervious - Roof*	0.04	1.1	0.05	0.11	1.1	0.12
Pervious - Planted Roof	0.00	0.12	0.00	0.07	0.12	0.01
Pervious **	0.00	0.21	0.00	0.00	0.27	0.00
Total	0.20		0.32	0.20		0.17
Total Phosphorus Removal through Land Conversion						0.14
Phosphorus reduction required (65% of Existing Phosphorus Load)						0.20
Remaining reduction required after land-use change						0.06
*Export rate using average value from compiled research data						
<u>Structural Stormwater Strategies</u>						
	Surface Type	Treated Area (ac)	P Load Rate (lbs/ac/yr)	Starting P Load (lbs/yr)	P Removal (%)	P Removed (lbs/yr)
Infiltration	Roof	0.111	1.1	0.122	86%	0.105
	Pavement	0.000	1.78	0.000		0.00
	Landscape	0.000	0.27	0.000		0.000
				0.12		0.11
Total Phosphorus Removed						0.25

Groundwater Recharge

The annual recharge from the post-development site will approximate the annual recharge from pre-development conditions using the guidelines provided in the MassDEP Stormwater Management Handbook. The guidelines are based on soil type and the Project Site is an HSG C type soil.

Impervious Area in HSG C = 8,510 square feet
Rv (Recharge Volume) = 8,510 x 0.25 in. / (12 inches/ft)
= 177 cubic feet

Proposed Recharge Volumes

Infiltration BMP	Recharge Volume (cf)
Subsurface Infiltration System	666

A minimum two feet of separation has been maintained between the bottom of the infiltration system and seasonal high groundwater.

Conclusions

The project has been designed to meet, and in some cases, exceed, the MassDEP Stormwater Standards as well as the City of Cambridge's stormwater requirements.

Domestic Water and Fire Protection Service

The Project's water demand estimate for domestic services is based on the Project's estimated sewage generation, described above. A conservative factor of 1.1 (10%) is applied to the estimated average daily wastewater flows calculated with 310 CMR 15.203 values to account for consumption, system losses and other usages to estimate an average daily water demand. The Project's estimated domestic water demand is 12,099 gallons per day. The project proposes to install new domestic and fire protection services that connect to the City water main in Walden Street in accordance with the Cambridge Water Department regulations and requirements. All water service connections will be fully coordinated with the City Water Department.

A hydrant flow test was recently completed to determine pressure in the existing water main. Based on the results, it is anticipated that a water pressure booster pump will be required for the domestic water system, and a fire pump will be required to provide the required pressure for the building's sprinkler system. The fire protection system design will be coordinated with the City Fire Chief.

COMPREHENSIVE PERMIT APPLICATION

2072 MASS AVE APARTMENTS
2072 MASSACHUSETTS AVENUE, CAMBRIDGE, 02140

SECTION 8
WALDEN STREET UTILITIES REPORT



Summary of Underground Utility Locating

Prepared For: Capstone Communities

Prepared By:
Sean Parker
Sean.Parker@gprsinc.com
Project Manager -Boston
617-372-6695

Capstone Communities

Attn: Jenny Tamarkin

Site: 2072 Massachusetts Ave

Cambridge, MA

We appreciate the opportunity to provide this report for our work completed on November 2, 2020.

PURPOSE

The purpose of the project was to search for underground utilities within the project boundaries provided by the client. The scope of work consisted of 1 sidewalk and 2 electrical manholes/vaults. The client was concerned with the depth of the primary electrical lines running up and down the sidewalk adjacent to the building.

EQUIPMENT

- **Underground Scanning GPR Antenna.** The antenna with frequencies ranging from 250 MHz-450 MHz is mounted in a stroller frame which rolls over the surface. The surface needs to be reasonably smooth and unobstructed in order to obtain readable scans. Obstructions such as curbs, landscaping, and vegetation will limit the feasibility of GPR. The data is displayed on a screen and marked in the field in real time. The total depth achieved can be as much as 8' or more with this antenna but can vary widely depending on the types of materials being scanned through. Some soil types such as clay may limit maximum depths to 3' or less. As depth increases, targets must be larger in order to be detected and non-metallic targets can be especially difficult to locate. Depths provided should always be treated as estimates as their accuracy can be affected by multiple factors. For more information, please visit: [Link](#)
- **Electromagnetic Pipe Locator.** The EM locator can passively detect the electromagnetic fields from live AC power or from radio signals travelling along some conductive utilities. It can also be used in conjunction with a transmitter to connect directly to accessible, metallic pipes or tracer wires. A current is sent through the pipe or tracer wire at a specific frequency and the resulting EM field can then be detected by the receiver. A utility's ability to be located depends on a variety of factors including access to the utility, conductivity, grounding, interference from other fields, and many others. Depths provided should always be treated as estimates as their accuracy can be affected by multiple factors. For more information, please visit: [Link](#)
- **GPS.** This handheld GPS unit offers accuracy down to 4 inches; however, the accuracy will depend on the satellite environment and obstructions and should not be considered to be survey-grade. Features can be collected as points, lines, or areas and then exported into Google Earth or overlaid on a CAD drawing. For more information, please visit: [Link](#)

PROCESS

The process typically begins with using the EM pipe locator to locate pipes or utilities throughout the scan area. First, the transmitter is used to connect to and trace any visible risers, tracer wires, or accessible, conductive utilities provided that there is an exposed, metallic surface. The areas are then swept with the receiver to detect live power or radio frequency signals. Locations and depths are painted or flagged on the surface. Depths cannot always be provided depending on the location method and can be prone to error.

Initial GPR scans were then collected in order to evaluate the data and calibrate the equipment. Based on these findings, a scanning strategy is formed, typically consisting of scanning the entire area in a grid with 5x5' scan spacing in order to locate any potential utilities that were not found with the pipe locator. The GPR data is viewed in real time and anomalies in the data are located and marked on the surface along with their depths using spray paint, pin flags, etc.

LIMITATIONS

Please keep in mind that there are limitations to any subsurface investigation. The equipment may not achieve maximum effectiveness due to soil conditions, above ground obstructions, reinforced concrete, and a variety of other factors. No subsurface investigation or equipment can provide a complete image of what lies below. Our results should always be used in conjunction with as many methods as possible including consulting existing plans and drawings, exploratory excavation or potholing, visual inspection of above-ground features, and utilization of services such as One Call/811. Depths are dependent on the dielectric of the materials being scanned so depth accuracy can vary throughout a site. Relevant scan examples were saved and will be provided in this report.

FINDINGS

The subsurface conditions at the time of the scanning allowed for maximum GPR depth penetration of 4-5 feet in most areas. Multiple utilities were able to be located such as gas, water, unknown, signal controls and possible secondary lines feeding street lamps using either the GPR or EM pipe locator. Some utilities were not able to be located such as the sanitary line. GPR data did not allow for depth information and exploring manholes located on Walden St did not show any laterals, estimated depth entering the sanitary main would be 7-9 feet.

The primary electrical line was estimated at 3.5-4.5 feet for most of the investigation, one manhole it was measured at 2.5-3 feet from the surface, and after passing through the service utilities it measured 5 feet at the manhole on the corner of Walden and Mass Ave. GPR depths in the middle sections estimated at lines at 3.5-4.5 feet.

The following pages will provide further explanation of the findings.



Terms and Conditions

GPRS does not provide land survey or civil engineering data collection or documentation. This is provided as a reference map of the field markings and is not survey-grade.

LEGEND

—	ELECTRIC	—	SANITARY
—	WATER	—	STORM
—	COMM	—	UNKNOWN
—	GAS		

2072 Massachusetts Ave
Cambridge, MA

Prepared by:





Secondary electrical line to the building exits at 1.5-2 feet and enters the building under the concrete ramp area.



Primary electrical line exits this manhole at 2.5-3 feet from the surface and travels up the sidewalk at a depth of 3.5-4.5 feet.



Water service 4.5-5.5 feet from the surface, valve located on the sidewalk, gas line 3-3.5 deep, valve located on the sidewalk. Electrical line 4-4.5 in this section.



Manhole located on the corner of Walden and Mass Ave, enters the vault at 5-5.5 feet with multiple lines running to adjacent handholes and vaults 2-3 feet from the surface and 1-2 feet.



Possible signal control or site lighting located 1-2 feet from the surface, extending in both directions down Mass Ave.



Electrical handhole located on the bottom right, potential signal or site lighting controls – electrical was being feed from a black transformer located on the sidewalk.

GPR Data Screenshots and Photos

2072 Massachusetts Ave
Cambridge, MA



CLOSING

GPRS, Inc. has been in business since 2001, specializing in underground storage tank location, concrete scanning, utility locating, and shallow void detection for projects throughout the United States. I encourage you to visit our website (www.gprsinc.com) and contact any of the numerous references listed.

GPRS appreciates the opportunity to offer our services, and we look forward to continuing to work with you on future projects. Please feel free to contact us for additional information or with any questions you may have regarding this report.

Thank you,

Sean Parker
Project Manager — Boston



Direct: 617-372-6695

Sean Parker

www.gprsinc.com

COMPREHENSIVE PERMIT APPLICATION

2072 MASS AVE APARTMENTS
2072 MASSACHUSETTS AVENUE, CAMBRIDGE, 02140

SECTION 9
GREEN BUILDING REPORT

2072 Mass Ave. Passive House Net Zero Narrative

2072 Massachusetts Avenue Passive House
2072 Mass Ave., Cambridge MA, 02140
September 10, 2020



Submitted To:
Community Development Department, City of Cambridge
344 Broadway, Cambridge MA, 02138

PROJECT PROFILE

DEVELOPMENT CHARACTERISTICS

Lot Area (sq.ft.):	~8,515 SF
Existing Land Use(s) and Gross Floor Area (sq.ft.), by Use:	BA-2 / Business A02, ~1,860 GSF
Proposed Land Use(s) and Gross Floor Area (sq.ft.), by Use:	Residential Use, ~65,710 GSF (w/Basement), ~57,400 GSF (no basement)
Proposed Building Height(s) (ft. and stories):	~89'-0", 8 Stories
Proposed Dwelling Units:	49 affordable
Proposed Open Space (sq.ft.):	0
Proposed Parking Spaces:	3 handicapped
Proposed Bicycle Parking Spaces (Long-Term and Short-Term):	51 long-term (48 Long-Term & 3 Tandem), 5 short-term

GREEN BUILDING RATING SYSTEM

The Rating System Selected for this project is as follows:

Passive House Institute US (PHIUS)			
Rating System & Version:	PHIUS+ Core	Seeking Certification?	YES

PROPOSED PROJECT DESIGN CHARACTERISTICS

BUILDING ENVELOPE

Roof	Roof Trusses w/ ~R-32 c.i. (~6" XPS Insulation)
Foundation	Concrete Foundation w/ ~R-30 c.i. (6" Low GWP Closed-cell Spray Foam Insulation @ R-5/in)
Exterior Walls	6" metal stud wall w/ ~R-18 c.i. (3" Polyisocyanurate or XPS), exterior rainscreen system

Windows	PHIUS approved window assemblies, thermally broken storefront system
Window to Wall Ratio	~30%
Other Components	Project team is considering sun shades on the south facade

ENVELOPE PERFORMANCE

	Proposed		Baseline	
	Area (sf)	U-value	Area (sf)	U-Value
Window	~7,250 SF	U-0.17 (SHGC – 0.32)	~7,250 SF	U-0.38 (fixed), U-0.45 (operable), 0.38 (SHGC - South, East, West), 0.51 (SHGC - North)
Wall	~32,865 SF	~U-0.05	~32,865 SF	U-0.064
Roof	~7,500 SF	~U-0.031 c.i.	~7,500 SF	U-0.032

ENVELOPE COMMISSIONING PROCESS

The project team has planned to test and verify the envelope air barrier and air infiltration rates using bi-directional blower door testing both at construction midpoint and again after construction completion. Two (2) inspections will be performed after framing and air-sealing are complete but before insulation is installed, in order to identify any potential areas of thermal bridging and/or air infiltration. These inspections will be documented with site photos. Once installed, the air barrier will be tested with a bi-directional whole building blower door test conducted to PHIUS+ CORE standards. At the end of construction, the whole building blower door test will be repeated to confirm air-tightness, and 13 units will be blower door tested for air infiltration rates per RESNET sampling protocols. In addition, a two hour inspection using a thermal imaging camera will be conducted to show compliance with thermal bridging and air sealing protocols.

BUILDING MECHANICAL SYSTEMS

SYSTEM DESCRIPTIONS

System	System Description
Space Heating:	Central VRF (11.2 EER, 23.4 IEER, 3.30 COP at 47F, 24.7 SCHE)
Space Cooling:	Central VRF (11.2 EER, 23.4 IEER, 3.30 COP at 47F, 24.7 SCHE)
Heat Rejection:	See above systems
Pumps & Auxiliary:	See above systems
Ventilation:	Central rooftop energy recovery ventilator with 75% heat recovery efficiency wheel and DX coil for heating/dehumidification
Domestic Hot Water:	Central gas-fired boiler plant potentially located at a penthouse level mechanical room to allow for future conversion to an all-electric DHW system
Interior Lighting:	LED
Exterior Lighting:	LED
Other Equipment:	TBD

SYSTEMS COMMISSIONING PROCESS

The project will retain a licensed commissioning agent (CxA) who will develop a detailed commissioning plan based on the building specifications and systems. The CxA will develop a functional performance test sheet for each system to be commissioned, and will commission the following systems: Mechanical systems and equipment including Energy Recovery Ventilation (ERV) systems, common space exhaust fans, the central VRF heating and cooling system and all apartment fan coils, and all direct digital controls. For lighting systems, all common space lighting control systems including occupancy sensors will be commissioned and sampled at the appropriate rate. For plumbing systems, the domestic hot water heating system including hot water heaters, storage tanks, circulating pumps, thermostatic mixing valves, and controls will be sampled at the appropriate rate.

ANTICIPATED ENERGY LOADS AND GREENHOUSE GAS EMISSIONS

Assumptions

The project will pursue Passive House certification and utilize WUFI energy modeling to demonstrate energy loads and energy use. The anticipated baseline building (ASHRAE 90.1-2013) energy use is indicated in the table below. Building heating and cooling loads, hot water heating load, lighting in units and common spaces, appliance and plug loads as well as miscellaneous system loads were included in this preliminary energy model.

	Proposed	Baseline
Site EUI (kBtu/yr./sq.ft.)	20	43.9
Source EUI (kBtu/yr./sq.ft.)	48	124.8

Annual Projected Greenhouse Gas (GHG) Emissions:

The annual expected Co2 emissions for the building based on the preliminary WUFI energy model are as follows:

Utility	Co2 emissions in metric tons/yr.
Electricity	69.45
Natural Gas	14.98

Annual Projected Energy Consumption:

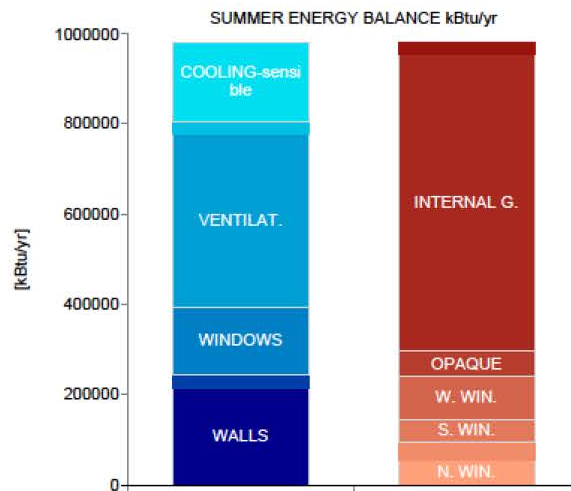
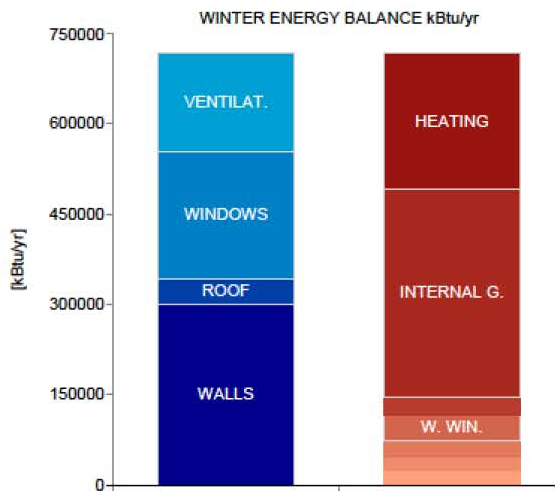
The annual expected energy consumption for the project is presented in the tables on the following pages. These tables were generated as part of the preliminary WUFI modeling exercise for the project.

ANNUAL HEAT DEMAND

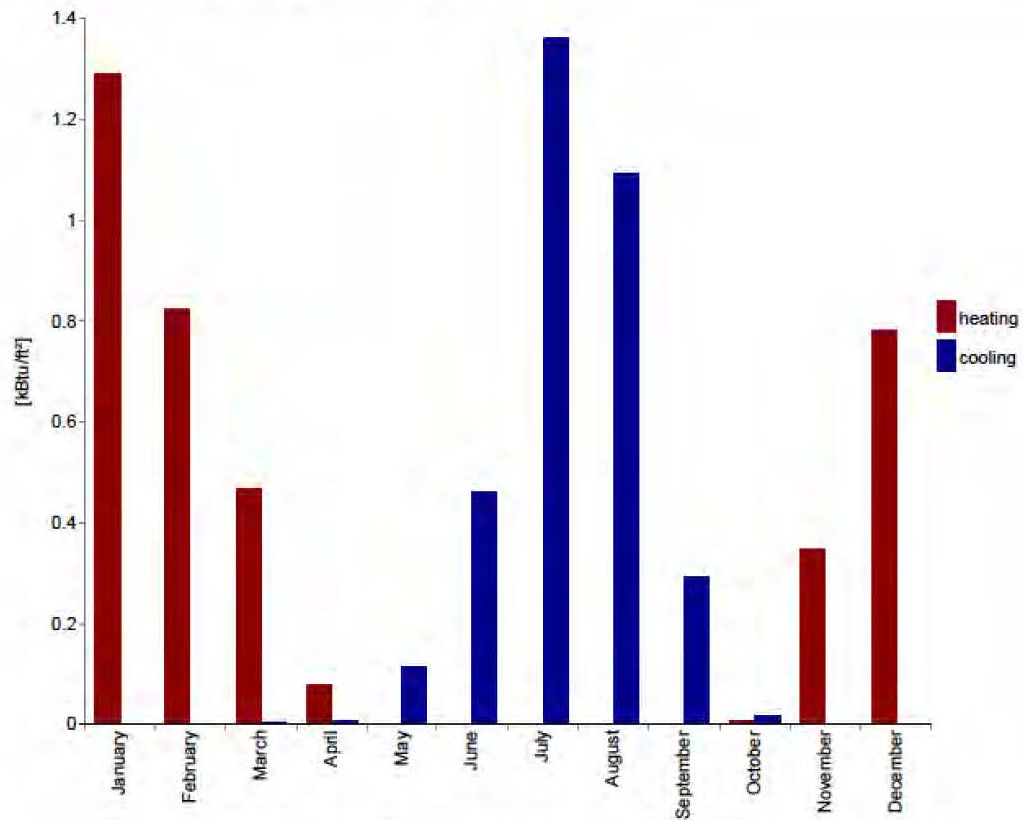
Transmission losses :	551,675 kBtu/yr
Ventilation losses:	164,937 kBtu/yr
Total heat losses:	716,611 kBtu/yr
Solar heat gains:	168,519 kBtu/yr
Internal heat gains:	399,611 kBtu/yr
Total heat gains:	568,130 kBtu/yr
Utilization factor:	86.2 %
Useful heat gains:	489,669 kBtu/yr
Annual heat demand:	226,943 kBtu/yr
Specific annual heat demand:	3,805.2 Btu/ft ² /yr

ANNUAL COOLING DEMAND

Solar heat gains:	297,293 kBtu/yr
Internal heat gains:	656,805 kBtu/yr
Total heat gains:	954,098 kBtu/yr
Transmission losses :	836,470 kBtu/yr
Ventilation losses:	828,133 kBtu/yr
Total heat losses:	1,664,603 kBtu/yr
Utilization factor:	46.8 %
Useful heat losses:	779,693 kBtu/yr
Cooling demand - sensible:	174,405 kBtu/yr
Cooling demand - latent:	25,356 kBtu/yr
Annual cooling demand:	199,760 kBtu/yr
Specific annual cooling demand:	3.3 kBtu/ft ² /yr



SPECIFIC HEAT/COOLING DEMAND MONTHLY



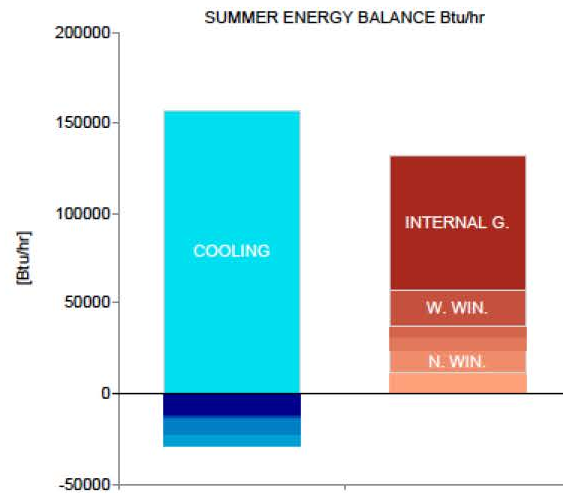
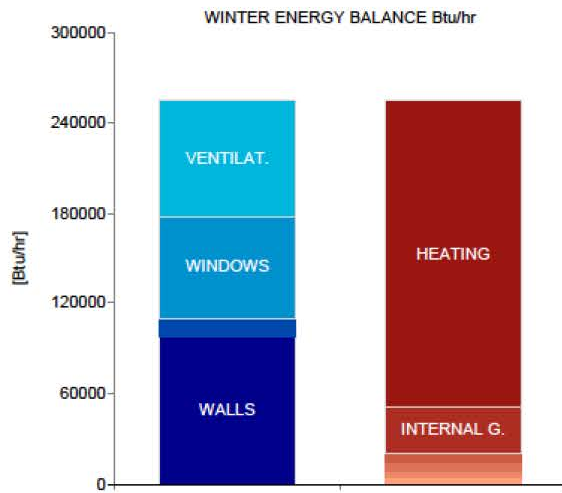
Month	Heating [kBtu/ft²]	Cooling [kBtu/ft²]
January	1.3	0
February	0.8	0
March	0.5	0
April	0.1	0
May	0	0.1
June	0	0.5
July	0	1.4
August	0	1.1
September	0	0.3
October	0	0
November	0.4	0
December	0.8	0

HEATING LOAD

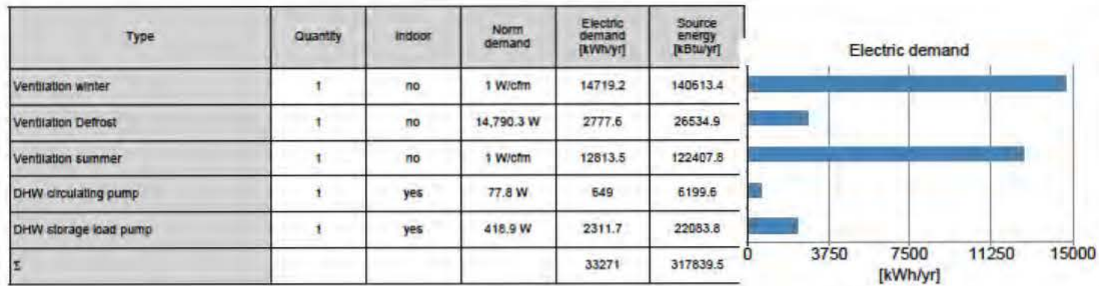
	First climate	Second climate
Transmission heat losses:	178,137.7 Btu/hr	126,703.6 Btu/hr
Ventilation heat losses:	76,794.1 Btu/hr	54,621.2 Btu/hr
Total heat loss:	254,931.8 Btu/hr	181,324.7 Btu/hr
Solar heat gain:	20,522.7 Btu/hr	11,605.6 Btu/hr
Internal heat gain:	30,252.3 Btu/hr	30,252.3 Btu/hr
Total heat gains heating:	50,775 Btu/hr	41,857.9 Btu/hr
Heating load:	204,156.8 Btu/hr	139,466.8 Btu/hr
Relevant heating load:	204,156.8 Btu/hr	
Specific heating load:	3.4 Btu/hr ft²	

COOLING LOAD

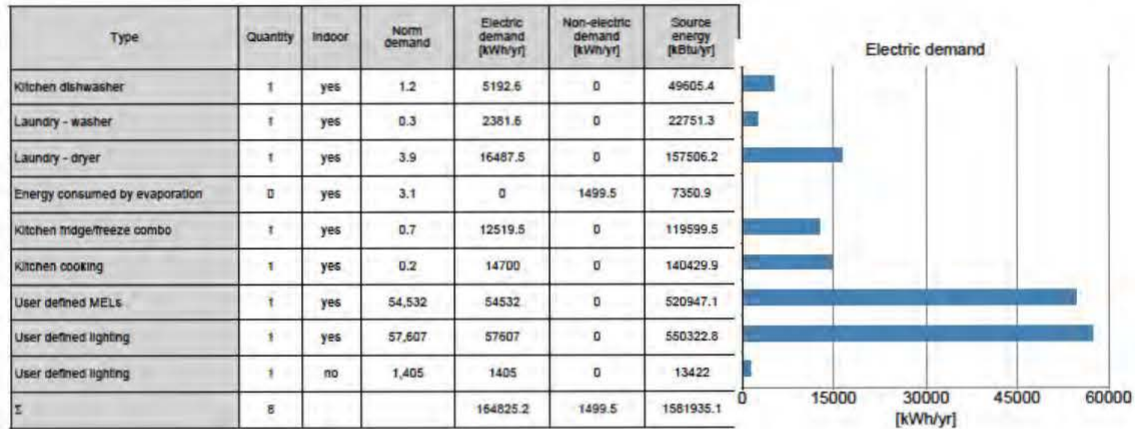
Solar heat gain:	56,811.3 Btu/hr
Internal heat gain:	74,985.1 Btu/hr
Total heat gains cooling:	131,796.4 Btu/hr
Transmission heat losses:	-18,131 Btu/hr
Ventilation heat losses:	-6,751.1 Btu/hr
Total heat loss:	-24,882.1 Btu/hr
Cooling load - sensible:	156,678.6 Btu/hr
Cooling load - latent:	0 Btu/hr
Relevant cooling load:	156,678.6 Btu/hr
Specific maximum cooling load:	2.6 Btu/hr ft²



ELECTRICITY DEMAND - AUXILIARY ELECTRICITY



ELECTRICITY DEMAND RESIDENTIAL BUILDING



DHW AND DISTRIBUTION

DHW consumption per person per day: **6.6 gal/Person/day**
Average cold water temperature supply: **52.8 °F**

Useful heat DHW: **249,984.9 kBtu/yr**
Specific useful heat DHW: **4,191.6 Btu/ft²/yr**

Total heat losses of the DHW system: **32,418 kBtu/yr**
Specific losses of the DHW system: **543.6 Btu/ft²/yr**

Performance ratio DHW distribution system and storage: **1.1**

Utilization ratio DHW distribution system and storage: **0.9**

Total heat demand of DHW system: **282,402.9 kBtu/yr**

Total specific heat demand of DHW system: **4,735.1 Btu/ft²/yr**

Total heat losses of the hydronic heating distribution: **0 kBtu/yr**

Specific losses of the hydronic heating distribution: **0 Btu/ft²/yr**

Performance ratio of heat distribution: **100 %**

BUILDING ENERGY PERFORMANCE MEASURES

Overview

Land Uses	The project is a mixed-use affordable housing transit-oriented development that is close to the Porter Square MBTA station. It also promotes walking and bicycling. Efficient use of limited building footprint includes approximately 525 square feet of amenity space, approximately 1040 square feet of neighborhood retail, and pedestrian scaled streetscapes.
Building Orientation/Massing	The proposed project is an eight (8) story tower with residential access to the building on Walden St, and retail space opening onto Mass. Ave.
Envelope Systems	High performance glazing and building envelope reduces the heating/cooling equipment sizes and low air infiltration rates improve indoor air quality and thermal comfort to the occupants.
Mech Systems	High efficiency mechanical systems include energy recovery ventilation, efficient air source heat pump technology, MERV 13 filtration, LED lighting, and low-flow plumbing fixtures.
Renewable Energy Systems	Preliminary WUFI energy models show that the project may meet the PHIUS site energy requirements without the inclusion of Solar PV. The project team will continue to track this item.
District Wide Energy Systems	N/A
Other	The project team has included 3 accessible parking spaces at the ground level and 2 temporary parking spaces to serve as a drop-off/pick-up area. The project has also focused on minimizing auto use, and has included 51 long term bike parking spaces (48 racks and 3 spaces for tandem or utility bikes).

INTEGRATIVE DESIGN PROCESS

The development team will present to the community on potential designs, design features, and the inclusion of affordable units as part of their early stage process. As part of the integrative design process, the developer, architect, mechanical engineer, and environmental consultant team have conducted a Green Charrette early on in the Schematic Design process and developed consensus on building systems and design that is consistent with PHIUS Passive House requirements.

SOLAR READY ROOF ASSESSMENT

To meet the very high level of performance required by the Passive House PHIUS+ CORE standard, results from preliminary energy models run by the project team indicated that this building likely does not require a PV array on the roof to meet the source energy goals. The project team is currently exploring placement of a mechanical room on the roof to enable a conversion to an all-electric DHW system in the future. This will limit available roof space for solar PV, but the building will be solar ready to allow for the potential addition of PV in the future. While this 49 unit building will be as energy efficient as possible, maximizing the potential to be a carbon neutral building in the future would likely involve purchasing renewable energy credits to offset the emissions from grid purchased power.

GREEN BUILDING INCENTIVE PROGRAM ASSISTANCE

Below is a description of programs applicable to this project that support improved energy performance or reduced greenhouse gas emissions, and which of those programs have been contacted and may be pursued.

The project plans to offset the costs of an energy efficient building envelope and electric heating and cooling system cost by utilizing all available rebate programs. The project is enrolled in the MassSave Passive House incentive program, and expects to use the Passive House Feasibility incentives from this program to offset the costs of energy modeling to meet Passive House standards. The project team plans to fully certify the building in order to be eligible for the full incentive package offered by MassSave. In addition, should the building systems qualify for the DOER Alternative Energy Certificate (AEC) incentive program, the project team will register for that incentive package as well.

NET ZERO SCENARIO TRANSITION

Below is a description of the technical framework by which the project can be transitioned to net zero greenhouse gas emissions in the future, acknowledging that such a transition might not be economically feasible at first construction. This description explains the future condition and the process of transitioning from the proposed design to the future condition.

	Net Zero Condition	Transition Process
Building Envelope	The building envelope will be built to PHIUS Passive House standards, making it an ideal structure to achieve Net Zero. The envelope will be well insulated and have a low level of air infiltration which will be tested and verified at construction.	This system will be a zero (site) emissions system at installation.
HVAC Systems	The heating system will be all-electric, with a highly efficient central VRF system installed at construction. In addition, central rooftop energy recovery ventilation will be used to capture energy from the ventilation system.	This system will have a zero (site) emissions system at installation.
Domestic Hot Water	A central gas-fired boiler plant will be included at construction. The project team is investigating the potential of locating these systems in a penthouse level mechanical room to allow for future conversion to an all-electric DHW system.	At the end of the system lifetime, the project team expects the all-electric DHW system technology to have advanced sufficiently to allow for conversion of this system to all-electric. The project team is investigating locating the DHW system on the roof specifically to allow for future conversion.
Lighting	The project will use LED lighting throughout at construction. The building energy model for this project, completed using WUFI modeling software for use in Passive House projects, does factor in and measure Lighting Power Density as a calculation in overall building energy	The building and management team will include updated technology as it is available and will update systems at the end of service life of the lighting systems.

	consumption. Fixtures have been modeled and will be specified in project documents to meet or exceed the energy requirement of the WUFI model.	
Renewable Energy Systems	The building will be Solar Ready at construction.	In order to become fully carbon neutral, this project will likely have to purchase renewable energy credits given the building footprint and limited roof area.
Other Strategies	The project is actively considering and modeling the use of window shading to reduce building energy consumption during summer months, while also allowing solar thermal gains during winter months.	

RESILIENCY

The project team has considered various resiliency strategies to reduce the project's and the residents' overall vulnerability. These resilience strategies are identified and summarized below in five sections.

EXISTING SITE ASSESSMENT

The project team has evaluated the flood risk based on current maps and future projections for the site and surrounding area. While the site is not located in a FEMA flood zone, and is not projected to be exposed to flood risk in forward looking models through the year 2070, the project team is actively considering resilience and risk mitigation strategies.

The project is at an average site elevation of ~35.95 ft-CCB, which translates to ~25.11 NGVD (ft-CCB is at 10.84 below NGVD). Based on the 2010 FEMA / FIRM Map, the site is located in Zone X Area of Minimal Flood Hazard, and is determined to be outside the 0.2% annual chance floodplain boundary (500-year flood scenario). Since the project is outside of the FEMA flood zones, FEMA has not determined a base flood elevation for this project site. The image below shows the project's location on the FEMA flood map.

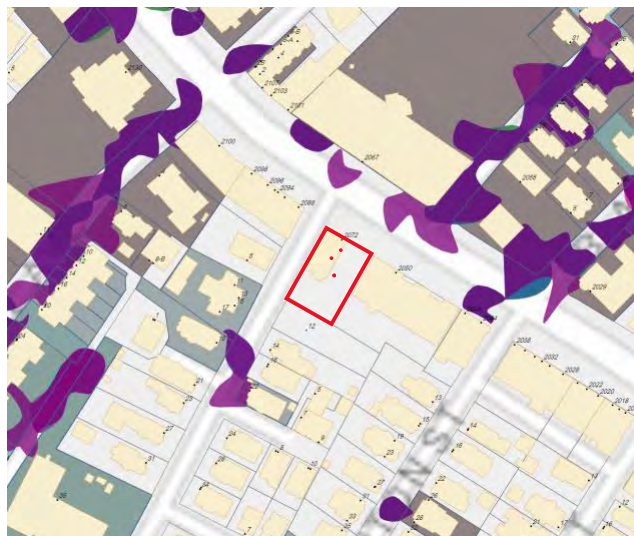


Based on the City of Cambridge FloodViewer v2.1, the site is outside the boundary of both the 2070 – 10 and 100 Year flood elevations. The below diagram indicates the extents of 2070 – 100 Year Precipitation nearby further down Walden St, and across Massachusetts Avenue. Although the project is not projected to be impacted by flooding, the project team has considered how to mitigate impacts from extreme events, and has outlined a number of resilience strategies and actions below.

Address: 12 Walden St

Ground Elevation Min:	35.70 ft-CCB
Ground Elevation Max:	38.90 ft-CCB
2070- 100 Year- SLR/SS	N/A
2070- 100 Year - Precip	N/A
2070- 10 Year - SLR/SS	N/A
2070- 10 Year - Precip	N/A
2030- 100 Year - Precip	N/A
2030- 10 Year - Precip	N/A
Present Day - 100 Year	N/A
Present Day - 10 Year	N/A
FEMA 500 Year	N/A
FEMA 100 Year	N/A

Selected Map-Lot: 200-22
Selected Address: 12 Walden St



PROTECTION STRATEGIES

The project will use strategies to reduce the building's vulnerability to extreme weather. The basement area will be waterproofed, as the building is outside of future projected flood risk zones. Flood resistant materials will be used in the basement, with concrete being the main material used. Sealants will be applied as needed and any cracks and penetrations will be sealed. Drywall use will be minimized in the basement, and any drywall used in the basement will be moisture, mold and mildew resistant purple board.

The transformer room will be waterproofed as needed based on its location in the building. Equipment located in the basement that must be on the floor will be located on 6" or 12" concrete pads as appropriate. Electrical outlets will be located 3'-0" above basement floor level. To prevent water intrusion, backwater valves will be installed as appropriate. In order to remove any water that does enter the basement, a sump pump will be installed and connected to emergency power. The project team will also work with the owner to develop an O&M manual which will include steps to take during flooding events.

ADAPTATION STRATEGIES

The project team is exploring the following strategies to improve the facility's ability to adapt to changing climate conditions. These strategies include both building elements and mechanical systems.

Building adaptation strategies include, but are not limited to, the following: Passive House level building envelope with operable windows to help with "passive survivability" – keeping the building habitable during extended power outages in any season. Reduced urban heat island effect enabled by the use of light colored and vegetated roofs. Window sunshades installed on the southwest façade to reduce solar heat gain during the summer, and interior window treatments (blinds) to reduce heat gain while allowing in light as needed.

Building mechanical system adaptation strategies include, but are not limited to, the following: Decentralized VRF heat pump mechanical system for both heating and cooling locates mechanical equipment on roof and in units instead of an area that can be potentially damaged by flooding. Other mechanical equipment, including air handlers, energy recovery ventilators, and emergency generator, are located on the roof and away from flood risk. All residential living areas are elevated, all are located on the second floor or above. The project team is also actively considering the inclusion of a stormwater capture system, including infiltration tanks.

BACKUP STRATEGIES

The project team is exploring how to provide for critical needs if the building loses power or other services. The project team plans to include an emergency generator located on roof and appropriate emergency lighting for evacuation and “sheltering-in-place”. Storage space will be available to provide access to water storage containers and access to potable water in the event of a power outage.

COMMUNITY RESILIENCE STRATEGIES

The project team is exploring how to encourage behavior which enhances resilience through cooperation. To enable this, the building’s amenity room encourages community building among residents. The planned amenity room includes communications resiliency measures e.g. phone charging, emergency refrigeration, and access to potable water as mentioned above. The project team will also evaluate the creation of an emergency operations manual for residents.

COMPREHENSIVE PERMIT APPLICATION

2072 MASS AVE APARTMENTS
2072 MASSACHUSETTS AVENUE, CAMBRIDGE, 02140

SECTION 10
TREE PROTECTION PLAN



Plant Healthcare Consultants

American Society of Consulting Arborist • International Society of Arboriculture
Massachusetts Arborist Association • Massachusetts Tree Wardens and Foresters Association
TREE INVENTORIES • APPRAISALS • DIAGNOSIS • TREE RISK ASSESSMENTS



Tree Protection Plan 2072 Massachusetts Avenue Cambridge, MA 02140

Prepared for:

CC HRE 2072 MASS AVE LLC
c/o Capstone Communities LLC
1087 Beacon Street, Suite 302
Newton, MA 02459

Prepared by:

Daniel E. Cathcart
Certified Consulting Arborist
Plant Healthcare Consultants
76 Stony Brook Road
Westford, MA 01886

November 4, 2020

Carl A. Cathcart • Daniel E. Cathcart
Plant Healthcare Consultants, Partnership
76 Stony Brook Rd. Westford, MA. 01886 • Phone (978) 764-6549 ~ (617) 237-7695
carl.phc@verizon.net • dan.cathcart@gmail.com • www.treeconsultant.com

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Summary

I was retained by CC HRE 2072 MASS AVE LLC for perform an evaluation of a Tilia cordata (Littleleaf linden) on the property line between 2050 and 2072 Massachusetts Avenue, Cambridge, MA. The focus of the evaluation was to assess the health and condition of the tree and, if preservation is an option, develop a Tree Protection Plan.

It is my opinion that the tree can be preserved and the specifications for the Tree Protection Plan are included in this report.

Introduction

On October 20, 2020 Jason Korb, of CC HRE 2072 MASS AVE LLC, contacted my office inquiring to retain consulting arborists in regard to a redevelopment project in Cambridge, MA. Mr. Korb informed me that there was a tree on the property line between the property he was redeveloping at 2072 Mass Ave. and the abutter at 2050 Mass Ave. He expressed a desire to preserve the tree and requested specification for a Tree Protection Plan.

I agreed to assist on the project. A site visit was scheduled for October 23, 2020 at 10:00 am.

Background & History

CC HRE 2072 MASS AVE LLC has acquired 2072 Mass Avenue, Cambridge, MA an ~8,500 sq ft property on the corners of Mass Ave and Walden Street. CC HRE 2072 MASS AVE LLC plans to renovate the property. There is one mature tree on the property line between 2072 and 2050 Mass Ave, a 12" diameter at breast height (DBH) Littleleaf linden. CC HRE 2072 MASS AVE LLC's goal is to preserve this tree.

A Tree Protection plan will be implemented to maximize the likelihood of the Littleleaf linden surviving the construction.

Assignment

The scope of the assignment is to assist CC HRE 2072 MASS AVE LLC in creating a Tree Protection Plan for the Littleleaf linden. This plan will have recommendations and specifications to provide the tree with the best chance of surviving the construction project.

The plan is included in this report.

Limits of Assignment

The recommendations and conclusions provided in this report are based on visual observations only. No examinations of the tree's interiors were taken nor were and soil or plant tissue taken and submitted for laboratory testing unless otherwise stated.

Purpose and Use of Report

This report is intended to provide CC HRE 2072 MASS AVE LLC with as much information regarding the tree at 2072 Mass Ave. It will outline the tree protection plan, tree management plan and provide recommendations and specifications for care of the tree in all phases of the site development.

This report is the property of CC HRE 2072 MASS AVE LLC and can be used and shared as they see fit.

Observations

On October 23, 2020 at approximately 10:00 am I visited the site and inspected the Littleleaf linden. At this visit I observed the tree and its surrounding and took measurements and photographs. I also examined the construction plans for the site to determine impact in regard to the tree.

I identified the tree as a 12" DBH *Tilia cordata*, Littleleaf linden. It is located on the west property line of 2070 Mass Ave that abuts 2050 Mass Ave. It is growing in a strip of land approximately 10' x 55' running in roughly a north-south orientation.



Planting Strip

The Littleleaf linden appears to be in good health and has established itself well in this area.



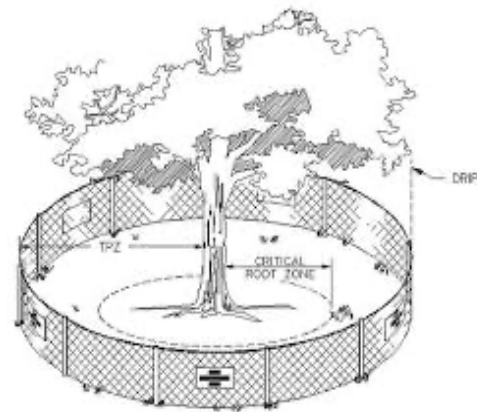
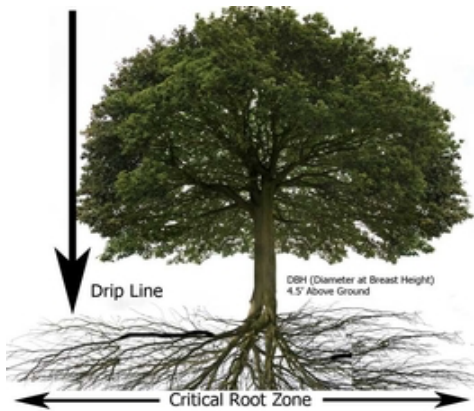
Due to the fact that the roots zone is confined by a retaining wall to the west and the paved parking area to the east, the majority, if not all, of the viable roots of the tree are located in the planting strip. As such, if this area is to be protected and proper steps taken, the root system should remain viable and sustain the tree through construction.

I also reviewed the attached Support of Excavation (SOE), (See page XXX). The proposed soldier piles and lagging, denoted by the red-dashed line on the plans, will allow for the following tree protection plan to be implemented.

Discussion

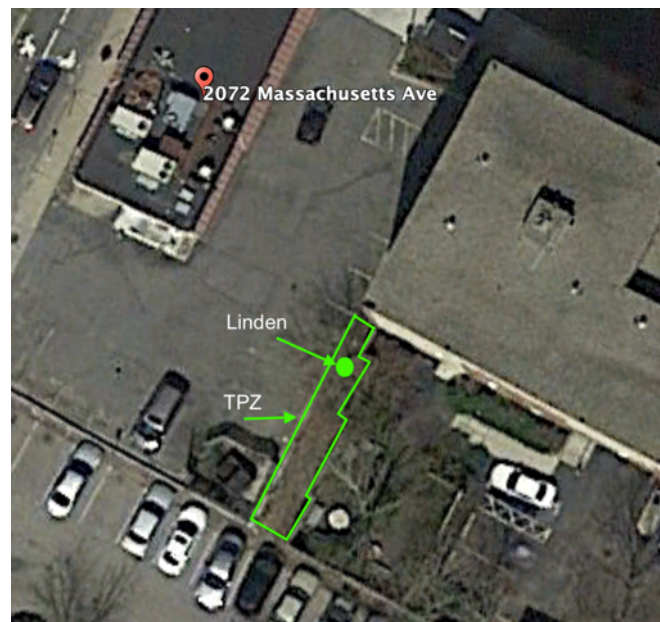
Tree Projection Zone

A Tree Preservation Plan has several components, all focusing on giving the tree the best chance for surviving the construction project. The majority of the components involve protection the Critical Root Zone (CRZ). The CRZ is the minimum area beneath the canopy of a tree which must be left undisturbed in order to preserve a sufficient root mass to give a tree a reasonable chance of survival. The CRZ should be defined, at a minimum, of the tree's dripline, the area represented by the outer canopy of the tree. This is crucial because the absorbing roots, the roots that take in water and nutrients, must be undisturbed or the tree will suffer stress and may decline and even die. The Tree Protection Plan includes the establishment of a Tree Protection Zone (TPZ), ideally, the TPZ must include the CRZ. The larger the TPZ the better as the root zone of a tree could extend as much as two or three times the width of the canopy. This is an area that is enclosed by a semi-permanent fence with appropriate signage. Within the CRZ, trenching, pavement, soil compaction, mechanical injury, storing of materials and spoils and any change in grade should be avoided.



Ideal Tree Protection Zone

In this case the CRZ has been encroached upon by the pavement and retaining wall and the tree has adjusted its root growth accordingly by concentrating its roots into the planting strip. As a result of the tree's natural compensation to its environment a modified TPZ should be made to include as much of the planting strip as possible.

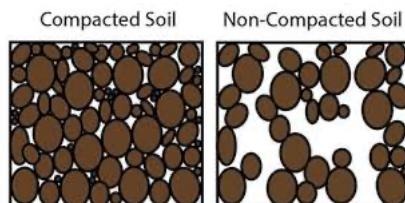


Proposed TPZ

Soil Compaction

All protected trees in the construction zone are subject to soil compaction from heavy vehicles, and any heavy debris placed in the Critical Root Zone (CRZ). Soil compaction occurs when the pore space

between soil particles is greatly reduced. This causes the reduction of oxygen available to the roots and can lead to decline in trees. Use of equipment, grading, digging, and heavily used walking paths can cause soil compaction in a construction area. Use protective fencing, mulching within the protective fencing, and limiting the amount of access routes will minimize soil compaction.



As the root system of trees is far more extensive than just the dripline, in this case in the entire planting strip, all equipment and materials should be kept out of the TPZ.

Mechanical Injury

There will be heavy equipment and vehicles used near the trees that are to be protected. Wounds to the tree's branches and trunk, caused by mechanical damage, may reduce tree stability by decreasing the wood strength, the internal movement of water and nutrients, and the ability to compartmentalize against decay. Enclosing the Critical Root Zone with protective fencing will prevent damage from construction equipment.



Change in Grade

Lowering or rising of the grade within the root zone can damage or kill a tree. The normal exchange of moisture and gases within the root zone is disrupted with the change in grade. The original grade should be maintained as far out from the trunk as possible. As little as four inches of soil placed over the root system can kill some species of trees. The change in grade can have either immediate or long-term adverse effects on the tree. If grade change is required use of retaining walls or soil cuts can improve the tree's tolerance to the grade change.

Excavation & Trenching

This project will require excavation for foundations. Excavation & trenching within the CRZ can damage the root system of a tree. Practicality requires the need to encroach on the CRZ, but care should be taken to excavate as little of the area adjacent to the tree as possible.

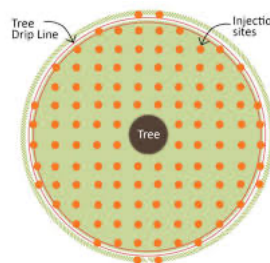
Irrigation

Irrigation should be provided within the CRZ as needed. A deep watering of the trees should take place before construction begins. During construction, the soil in the CRZ should be watered regularly and deeply so water penetrates the root area at least six to eight inches deep. A watering schedule will vary with climatic conditions, but a rule of thumb is 1" of water weekly during construction.

Soil Treatment

I am prescribing a non-nitrogen fertilizer that is high in phosphorus and potassium (0-20-20 fertilizer analysis) to promote root development. I recommend a fertilization in the spring. Applying the fertilizer in the early spring will prepare the trees for a flush of root development. Root development is most critical for the trees to prepare themselves for construction impact. The healthier and abundant the root system the more water and nutrients the tree can take in which is the best defense against stress.

The fertilizer shall be applied in a water solution, injected directly into the CRZ, in this case the entire TPZ, by means of an application needle under pressure. Injections should be made about every foot in a grid-like pattern.



Plant Healthcare

At this stage there does not appear to any major pest concerns on the trees. This will be monitored regularly (monthly) to see if conditions change. If there is a need to address insect, mite or disease pest a proper course of action will be prescribed at that time.

All plant healthcare treatments shall be performed by a certified arborist who is also a licensed pesticide applicator and supervised by an ISA Board Certified Master Arborist.

Conclusion

Based on my education, training and experience it is my opinion that taking this proactive approach to tree preservation will provide the Littleleaf linden at 2070 Mass Ave the best chance of surviving the construction. Setting up tree protection zones around the trees to retain, managing the flow and access of heavy equipment, performing required tree work prior to commencing construction and regular monitoring of the work site to ensure all practices are adhered to should make for a successful worksite.

Recommendations

Pre-Construction

Prior to construction the Tree Protection Zone should be established. A six-foot chain-link fence (or suitable alternative, i.e. snow fence), with signage designating a Tree Protection Zone, Keep Out, should be erected around all the protected tree and encompass the modified Critical Root Zone as explained above. Once installed this fence should not be moved nor the CRZ disturbed for the duration of the construction project.

The access way for heavy equipment should be established, as well as where equipment and materials will be stored. This should be as far away as possible from all protected trees and their root systems. No equipment or material may be stored on the root systems of the protected trees.

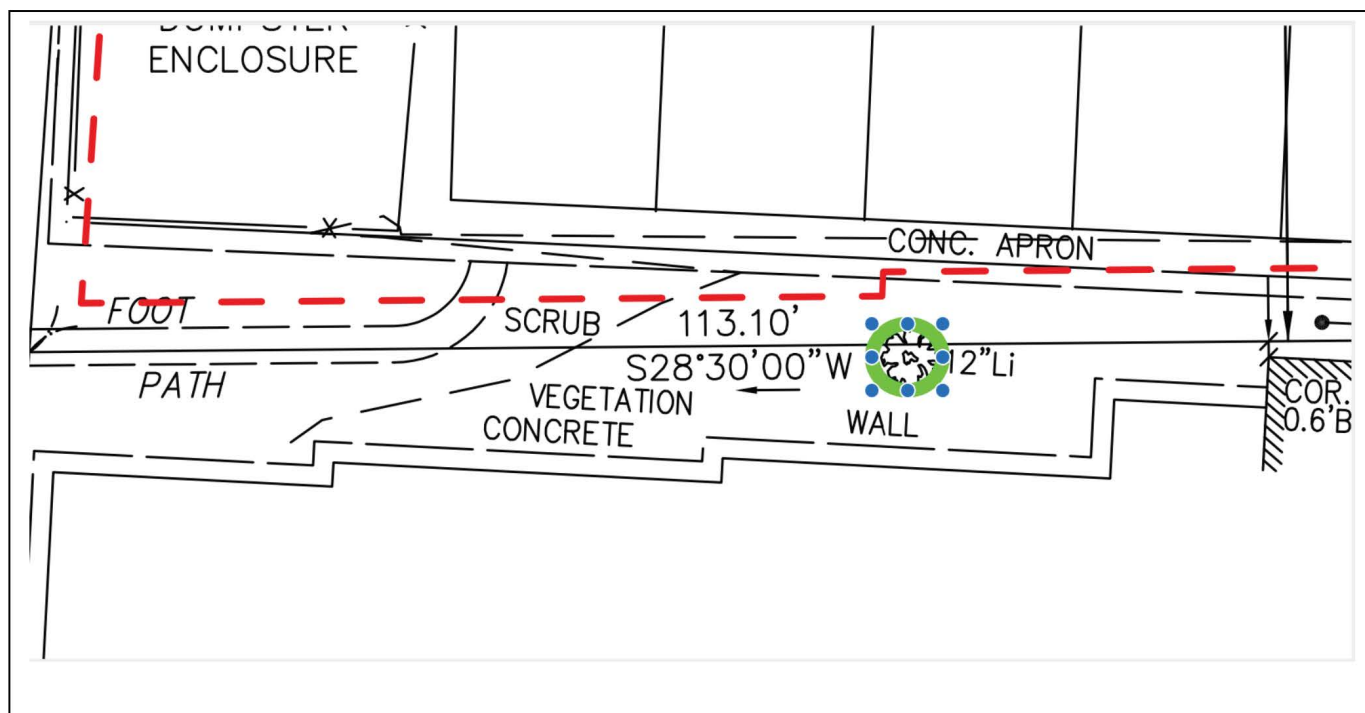
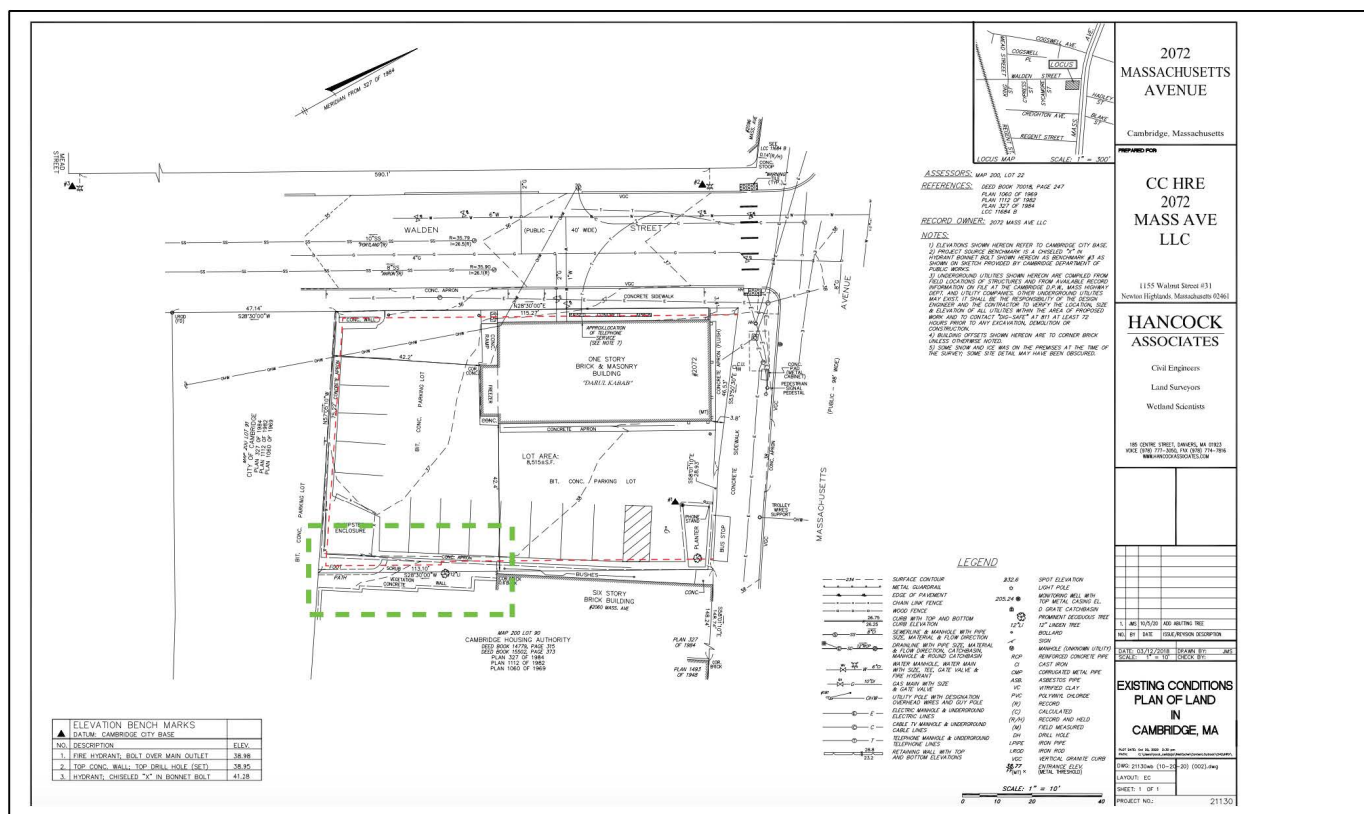
Construction

During the construction Phase of the project monitoring of the site is crucial. An ISA Board Certified Master Arborist should inspect the site monthly. The purpose of those visits is to ensure that the Tree Preservation Plan is being adhered to, adequate watering is taking place, trenching and excavations are following plan, inspect the trees for pest issues and make observations regarding any changes to the trees on the site.

Post-Construction

Monitoring after the construction is completed is very important to the long-term health of the trees. For a period of one growing season (starting the April following construction completion through that September) monthly monitoring will continue as during the construction period.

Support of Excavation Plan



Carl A. Cathcart ▪ Daniel E. Cathcart
Plant Healthcare Consultants, Partnership

76 Stony Brook Rd. Westford, MA. 01886 ▪ Phone (978) 764-6549 ~ (617) 237-7695
carl.phc@verizon.net ▪ dan.cathcart@gmail.com ▪ www.treeconsultant.com

Glossary of Terms

Absorbing Roots	Fine, fibrous roots that take up water and minerals; most of them are within the top 12 inches of soil
Branch Union	The structural union of a lateral branch to the tree stem.
Caliper	Is measured approximately 6-12" from the root collar. Caliper is an American Nursery Standard measurement. Synonym for trunk diameter used to measure the size of nursery stock; by convention, measured 6" above the ground.
Canopy	The part of the crown composed of leaves and small twigs.
Certified Arborist	A professional arborist possessing current certification issued by the Massachusetts Arborists Association (MAA) and/or the International Society of Arboriculture (ISA)
Clinometer	A device used to measure the height of an object
Co-dominant	equal in size and relative importance usually associated with either the trunk/stems or scaffold limbs/ branches in the crown.
Critical Root Zone (CRZ)	The minimum area beneath the canopy of a tree which must be left undisturbed in order to preserve a sufficient root mass to give a tree a reasonable chance of survival. The CRZ is represented by a concentric circle centering on the tree's trunk and extending outward towards the tree's drip-line. The minimum area of the CRZ shall be dependent on the required minimum radius of the CRZ; the required CRZ shall be determined by multiplying a tree's DBH (in inches) by eighteen (18) inches, with the resulting product constituting the minimum radius of the CRZ.
Compost	Organic matter that has been intentionally subjected to decay processes and is more or less decomposed.
Crown	The upper part of a tree, measured from the lowest branch, including all the branches and foliage
DBH	Stands for Diameter Breast Height. The diameter of a tree measured at 4.5 feet above the ground.
Drip-line	Perimeter of the area under a tree including the branches and leaves
Establishment	The process of a tree becoming acclimated to a new environment, usually correlating the new root development that can sustain normal biological functions of the tree

Monitoring	A holistic approach to plant healthcare that includes inspecting plants for cultural problems, proper soil moisture and nutrient content and insect and disease issues-treating as necessary
Parity	The time, usually in years, that it takes for a replacement tree to provide similar attributes and benefits of a removed tree
Pruning	Systematic removal of branches of a plant usually a woody perennial
Restoration/Maintenance Program	A plan of maintenance and monitoring of trees to maximize survival or recovery rate of damaged or newly planted trees
Root Collar	Area at the base of the tree where the roots and the stem merge
Soil Compaction	Compression of the soil resulting in a reduction of the total air or pore space
Specimen Tree	A tree of high perceived value attributed to location, size, aesthetics, form or function
Stress	Any change in environment conditions that produce a less than ideal plant response
Transplant Shock	The stress a tree undergoes as a result of planting in a new location
Tree Protection Plan	Report to identify and protect trees indicated to remain. Procedures shall include protective measures to be used for both above and below grade.
Tree Protection Zone	An area usually defined by the drip-line of a tree. To protect a tree, no construction should ever occur within this area.

Assumptions and Limited Conditions

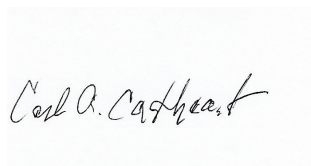
1. It is assumed that any property is not in violation of any applicable codes, ordinances, statutes or other governmental regulations.
2. Care has been taken to obtain all information from reliable sources. All data has been verified insofar as possible; however, the consultant can neither guarantee nor be responsible for the accuracy of information provided by others.
3. The consultant shall not be required to give testimony or to attend court by reason of this report unless subsequent contractual arrangements are made, including payment of an additional fee for such services as described in the fee schedule and contract of engagement.
4. Unless required by law, otherwise, possession of this report or a copy thereof does not imply right of publication or use for any purpose by any other than the person to whom it is addressed, without the prior expressed written or verbal consent of the consultant.
5. Unless required by law, neither all nor any part of the contents of this report, nor copy thereof, shall be conveyed by anyone, including the client, to the public through advertising, public relations, news, sales or other media, without the prior expressed written or verbal consent of the consultant-particularly as to value conclusions, identity of the consultant, or any reference to any professional society or institute or to any initialed designation conferred upon the consultant as stated in his qualifications.
6. This report expressed herein represent the opinion of the consultant, and the consultant's fee is in no way contingent upon the reporting of a specified value, a stipulated result, the occurrence of a subsequent event, nor upon any finding to be reported.
7. Sketches, drawings, and photographs in this report, being intended as visual aids, are not necessarily to scale and should not be construed as engineering or architectural reports or surveys unless expressed otherwise. The reproduction of any information generated by architects, engineers, or other consultants on any sketches, drawings, or photographs is for the express purpose of coordination and ease of reference only. Inclusion of said information on any drawings or other documents does not constitute a representation by *Plant Healthcare Consultants* as to the sufficiency or accuracy of said information.
8. Unless expressed otherwise: 1) information contained in this report covers only those items that were examined and reflects the condition of those items at the time of inspection; and 2) the inspection is limited to visual examination of accessible items without dissection, excavation, probing, or coring unless otherwise specified. There is no warranty or guarantee, expressed or implied, that problems or deficiencies of the plants or property in question may not arise in the future.
9. Loss or alteration of any part of this report invalidates the entire report.

Certification of Performance

Plant Healthcare Consultants certify that:

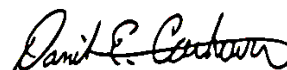
1. We have personally inspected the tree and property referred to in this report and have stated our findings accurately.
2. We have no current or prospective interest in the trees or the property that is the subject of this report and have no personal interest or bias with respect to the parties involved.
3. The analysis, opinions and conclusions stated herein are our own and are based on current scientific procedures and facts.
4. Our analysis, opinions and conclusions were developed and this report has been prepared according to commonly accepted arboricultural practices.
5. No one provided significant professional assistance to us, except as indicated within the report.
6. Our compensation is not contingent upon the reporting of a predetermined conclusion that favors the cause of the client or any other party or upon the results of the assessment, the attainment of stipulated results, or the occurrence of any subsequent events.

We further certify that Plant Healthcare Consultants is a member in good standing of the Massachusetts Arborist Association, American Society of Consulting Arborists, the International Society of Arboriculture and Massachusetts Tree Wardens and Foresters Association. We have been involved in the field of Arboriculture for over 60 years



Carl A. Cathcart

A.S.C.A. Registered Consulting Arborist RCA #606
Massachusetts Certified Arborist #1114
International Society of Arboriculture #WE-0716A
ISA Tree Risk Assessment Qualified



Daniel E. Cathcart

American Society of Consulting Arborists
Massachusetts Certified Arborist #41801
ISA Board Certified Master Arborist #TX-1357B
ISA Tree Risk Assessment Qualified
Massachusetts Qualified Tree Warden #1097

Carl A. Cathcart • Daniel E. Cathcart
Plant Healthcare Consultants, Partnership
76 Stony Brook Rd. Westford, MA. 01886 • Phone (978) 764-6549 ~ (617) 237-7695
carl.phc@verizon.net • dan.cathcart@gmail.com • www.treeconsultant.com

COMPREHENSIVE PERMIT APPLICATION

2072 MASS AVE APARTMENTS
2072 MASSACHUSETTS AVENUE, CAMBRIDGE, 02140

SECTION 11
PLANS



2072 MASS AVE
2072 MASSACHUSETTS AVENUE
CAMBRIDGE, MA 02140

COMPREHENSIVE PERMIT

NOVEMBER 10, 2020

CC HRE 2072 MASS AVE TENANT LLC
OWNER

C/O CAPSTONE COMMUNITIES LLC
1087 BEACON ST, SUITE 302
NEWTON CENTRE, MA 02459

C/O HOPE REAL ESTATE ENTERPRISES LLC
907 MASSACHUSETTS AVE, SUITE 300
CAMBRIDGE, MA 02139

BRUNER/COTT ARCHITECTS
ARCHITECT

225 FRIEND ST, SUITE 701
BOSTON, MA 02114

NITSCH ENGINEERING INC.
CIVIL ENGINEER

2 CENTER PLAZA, #430
BOSTON, MA 02108

PETERSEN ENGINEERING INC.
MEP/FP ENGINEER

127 PARROTT AVE
PORTSMOUTH, NH 03801

L.A. FUESS PARTNERS
STRUCTURAL ENGINEER

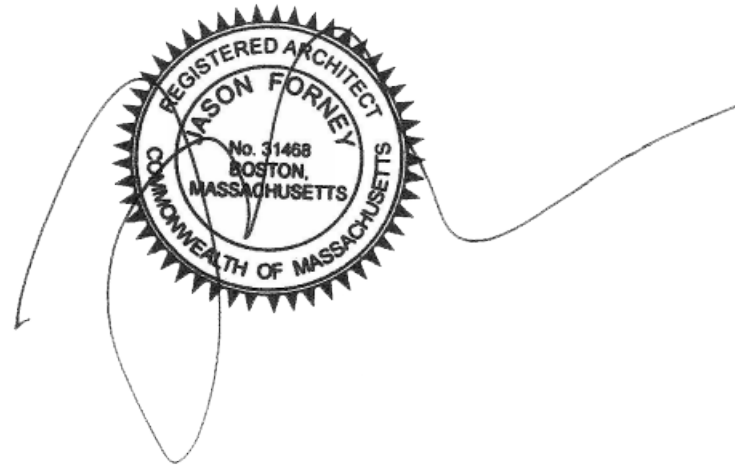
211 CONGRESS ST, SUITE 810
BOSTON, MA 02110

NEW ECOLOGY INC
SUSTAINABILITY CONSULTANT

15 COURT SQ, SUITE 420
BOSTON, MA 02108

CODE RED CONSULTANTS LLC
CODE CONSULTANT

154 TURNPIKE RD, SUITE 200
SOUTHBOROUGH, MA 01772





Date	NOVEMBER 10, 2020
Scale	
Project Number	20.001
Drawn By	Author

2072 Massachusetts
Avenue, Cambridge, MA
02140

G-100

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DRAWING LIST	
GENERAL	
G-100	NEIGHBORHOOD MAP/ SITE PLAN
G-101	DRAWING LIST, UNIT MATRIX, ZONING SUMMARY
EC	EXISTING CONDITIONS PLAN OF LAND IN CAMBRIDGE, MA
CIVIL	
C-100	CIVIL SITE UTILITY AND LAYOUT PLAN
C-101	PROPOSED EASEMENT PLAN
ARCHITECTURAL	
A-100	BASEMENT FLOOR PLAN
A-101	FIRST FLOOR PLAN
A-102	TYPICAL FLOOR PLAN (FLRS 2-8)
A-109	ROOF PLAN
A-300	EXTERIOR ELEVATIONS
A-301	EXTERIOR ELEVATIONS
A-302	EXTERIOR ELEVATIONS
A-303	EXTERIOR ELEVATIONS
A-304	EXTERIOR ELEVATIONS
A-305	PERSPECTIVES
A-306	PERSPECTIVES
A-307	PERSPECTIVES
A-308	PERSPECTIVES
A-309	PERSPECTIVES
A-310	PERSPECTIVES
A-400	STREET SECTIONS
A-401	BUILDING SECTION
A-402	BUILDING SECTION

2072 MASSACHUSETTS AVENUE, CAMBRIDGE, MA
BUILDING TABULATIONS
PREPARED BY BRUNER/COTT ARCHITECTS

SITE AND BUILDING

BUILDING GROSS FLOOR AREA (GFA) (PER ZONING)

BASEMENT	Excluded
FIRST FLOOR	4,580
SECOND FLOOR	7,545
THIRD FLOOR	7,545
FOURTH FLOOR	7,545
FIFTH FLOOR	7,545
SIXTH FLOOR	7,545
SEVENTH FLOOR	7,545
EIGHTH FLOOR	7,545
TOTAL GFA	57,395

BUILDING GROSS SQUARE FOOTAGE (GSF)

BASEMENT	5,465
FIRST FLOOR	4,580
SECOND FLOOR	7,545
THIRD FLOOR	7,545
FOURTH FLOOR	7,545
FIFTH FLOOR	7,545
SIXTH FLOOR	7,545
SEVENTH FLOOR	7,545
EIGHTH FLOOR	7,545
TOTAL GSF	62,860

TOTAL PARCEL SIZE (SF)	8,515
FAR	6.74
PARKING RATIO (EXCLUDES DROP-OFF/PICK-UP)	6%
LONG TERM BIKE PARKING RATIO	104%
OPEN SPACE	0%
CONSTRUCTION TYPE *	Type IA construction (2-story podium, basement and first floor), Type IIA construction (above podium, second to eighth floors) * Alternative option of seeking a variance to permit the use of the new construction type classifications found in the 2021 IBC and classifying the tower as Type IV-B

PARKING

	HC	TOTAL *	HC UNIT : SPACE RATIO
COVERED PARKING	3	3	1:1

* Commercial parking is waived under Article 6.36 based on actual quantity required being below four (4) required spots

* Two (2) pick-up/drop-off spaces will also be provided

	LONG-TERM	TANDEM	SHORT-TERM	TOTAL
BIKE PARKING	48	3	0*	51

* Project team is seeking alternative public contribution option of satisfying short-term bike parking requirements per Article 6.104.2 (b)

UNIT MIX

	STUDIO	1 BR	2 BR	3 BR	TOTAL
SECOND FLOOR	0	2	3	2	7
THIRD FLOOR	0	2	3	2	7
FOURTH FLOOR	0	2	3	2	7
FIFTH FLOOR	0	2	3	2	7
SIXTH FLOOR	0	2	3	2	7
SEVENTH FLOOR	0	2	3	2	7
EIGHTH FLOOR	0	2	3	2	7
TOTAL	0	14	21	14	49

% PER UNIT TYPE	0%	29%	42%	29%
% 2 AND 3 BR COMBINED	71%			

	STUDIO	1 BR	2 BR	3 BR	TOTAL
TOTAL NUMBER OF BEDROOMS	0	14	42	42	98

UNIT NUMBER *	UNIT TYPE	UNIT AREA (SF)
UNIT X01	1 BR	569
UNIT X02	1 BR	681
UNIT X03	3 BR	1144
UNIT X04	2 BR	800
UNIT X05	2 BR	850
UNIT X06	3 BR	1031
UNIT X07	2 BR	823

* For typical floors (second through eight floors)

UNIT TYPE	AVERAGE AREA (SF)
STUDIO	N/A
1 BR	625
2 BR	824
3 BR	1088

Bruner/Cott
ARCHITECTS

225 Friend St., Suite 701
Boston, MA 02114
617.492.8400
www.brunercott.com

Rev	Date	Remarks

Date	NOVEMBER 10, 2020
Scale	
Project Number	20.001
Drawn By	Author

2072 MASS AVE

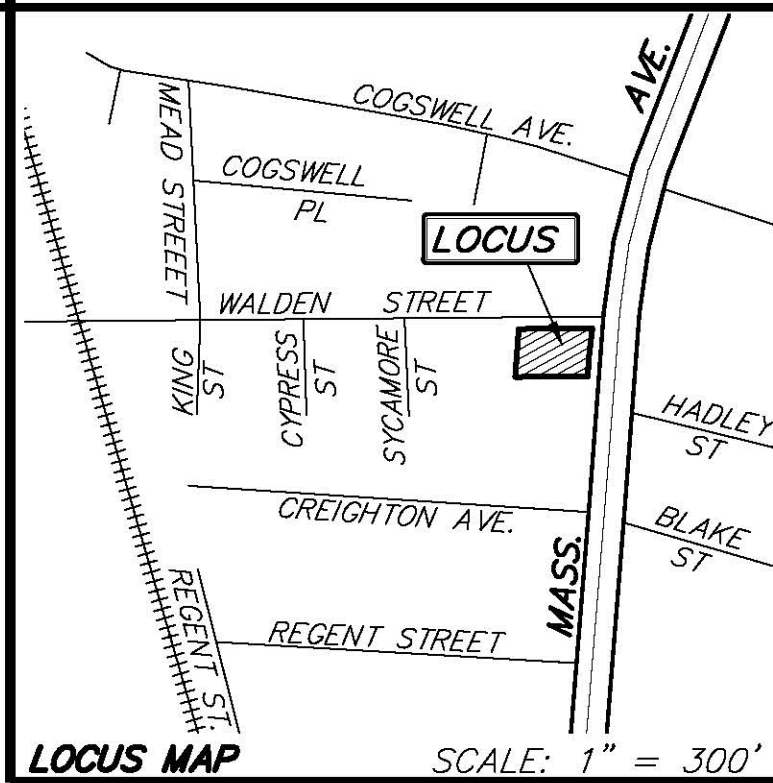
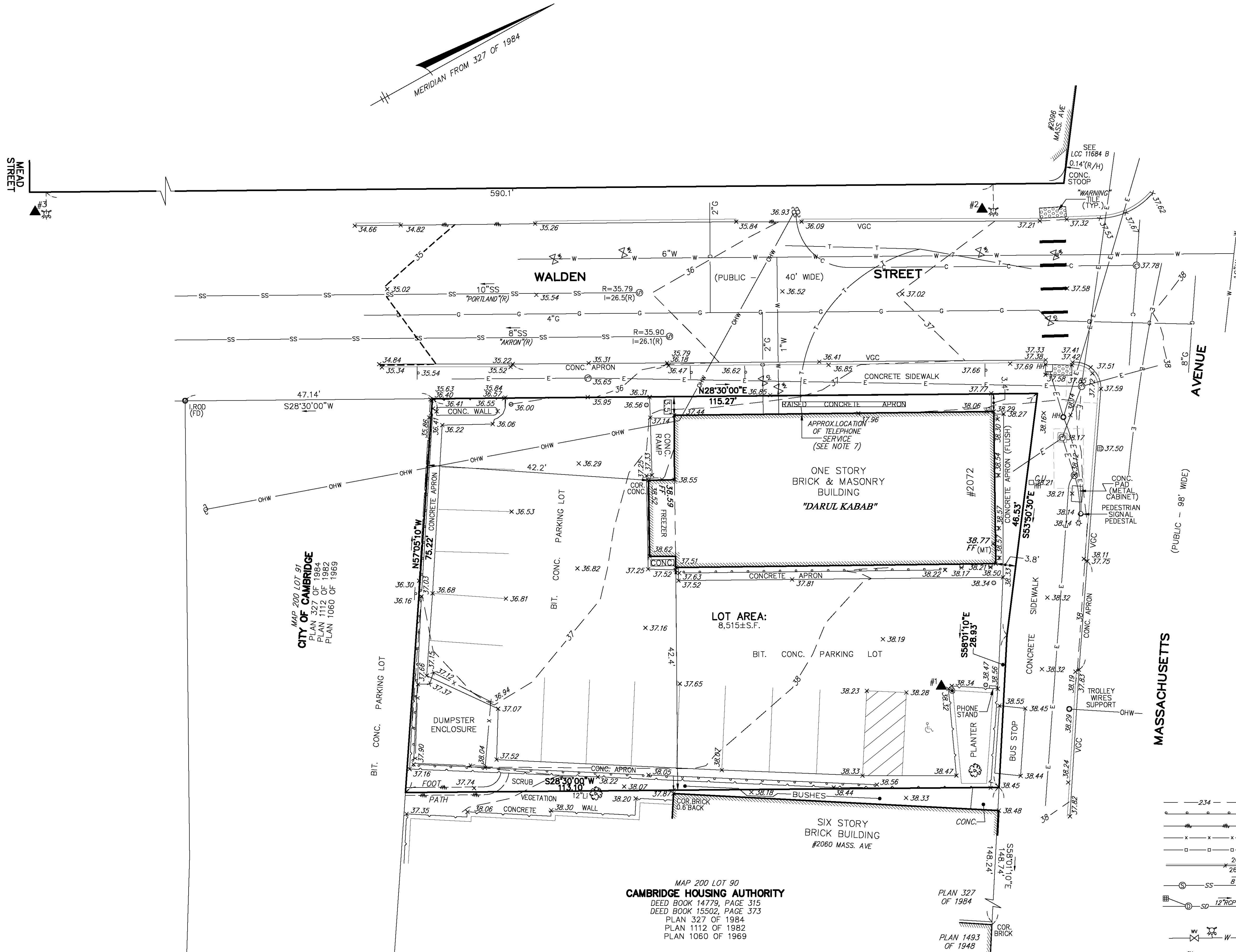
2072 Massachusetts
Avenue, Cambridge, MA
02140

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DRAWING LIST, UNIT
MATRIX, ZONING
SUMMARY

G-101



ASSESSORS: MAP 200, LOT 22

REFERENCES: DEED BOOK 70018, PAGE 247
PLAN 1060 OF 1969
PLAN 1112 OF 1982
PLAN 327 OF 1984
LCC 11684 B

RECORD OWNER: 2072 MASS AVE LLC

NOTES:

- 1) ELEVATIONS SHOWN HEREON REFER TO CAMBRIDGE CITY BASE.
- 2) PROJECT SOURCE BENCHMARK IS A CHISELED "X" IN HYDRANT BONNET BOLT SHOWN HEREON AS BENCHMARK #3 AS SHOWN ON SKETCH PROVIDED BY CAMBRIDGE DEPARTMENT OF PUBLIC WORKS.
- 3) UNDERGROUND UTILITIES SHOWN HEREON ARE COMPILED FROM FIELD LOCATIONS OF STRUCTURES AND FROM AVAILABLE RECORD INFORMATION ON FILE AT THE CAMBRIDGE D.P.W., MASS HIGHWAY DEPT. AND UTILITY COMPANIES. OTHER UNDERGROUND UTILITIES MAY EXIST. IT SHALL BE THE RESPONSIBILITY OF THE DESIGN ENGINEER AND THE CONTRACTOR TO VERIFY THE LOCATION, SIZE & ELEVATION OF ALL UTILITIES WITHIN THE AREA OF PROPOSED WORK AND TO CONTACT "DIG-SAFE" AT 811 AT LEAST 72 HOURS PRIOR TO ANY EXCAVATION, DEMOLITION OR CONSTRUCTION.
- 4) BUILDING OFFSETS SHOWN HEREON ARE TO CORNER BRICK UNLESS OTHERWISE NOTED.
- 5) SOME SNOW AND ICE WAS ON THE PREMISES AT THE TIME OF THE SURVEY; SOME SITE DETAIL MAY HAVE BEEN OBTAINED.

2072
MASSACHUSETTS
AVENUE

Cambridge, Massachusetts

PREPARED FOR:

CC HRE
2072
MASS AVE
LLC

1155 Walnut Street #31
Newton Highlands, Massachusetts 02461

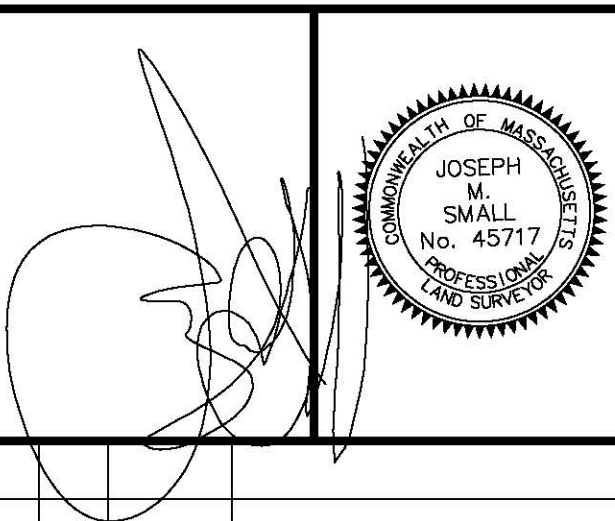
HANCOCK
ASSOCIATES

Civil Engineers

Land Surveyors

Wetland Scientists

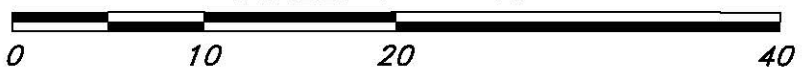
185 CENTRE STREET, DANVERS, MA 01923
VOICE (978) 777-3050, FAX (978) 774-7816
WWW.HANCOCKASSOCIATES.COM



LEGEND

— 234 —	SURFACE CONTOUR	232.6	SPOT ELEVATION
— — —	METAL GUARDRAIL	*	LIGHT POLE
— — —	EDGE OF PAVEMENT	205.24	MONITORING WELL WITH TOP METAL CASING EL.
— x — x —	CHAIN LINK FENCE	12" LI	D GRATE CATCHBASIN
— — —	WOOD FENCE	12" LI	PROMINENT DECIDUOUS TREE
— — —	CURB WITH TOP AND BOTTOM CURB ELEVATION	12" LI	12" LINDEN TREE
— — —	SEWERLINE & MANHOLE WITH PIPE SIZE, MATERIAL & FLOW DIRECTION	12" LI	BOLLARD
— — —	DRAINLINE WITH PIPE SIZE, MATERIAL & FLOW DIRECTION, CATCHBASIN, MANHOLE & ROUND CATCHBASIN	12" LI	SIGN
— — —	WATER MANHOLE, WATER MAIN WITH SIZE, TEE, GATE VALVE & FIRE HYDRANT	RCP	MANHOLE (UNKNOWN UTILITY)
— — —	GAS MAIN WITH SIZE & GATE VALVE	CI	REINFORCED CONCRETE PIPE
— — —	UTILITY POLE WITH DESIGNATION OVERHEAD WIRES AND GUY POLE	CMP	CAST IRON
— — —	ELECTRIC MANHOLE & UNDERGROUND ELECTRIC LINES	VC	CORRUGATED METAL PIPE
— — —	CABLE TV MANHOLE & UNDERGROUND CABLE LINES	ASB	ASBESTOS PIPE
— — —	TELEPHONE MANHOLE & UNDERGROUND TELEPHONE LINES	VC	VITRIFIED CLAY
— — —	RETAINING WALL WITH TOP AND BOTTOM ELEVATIONS	PVC	POLYVINYL CHLORIDE
— — —		(R)	RECORD
		(C)	CALCULATED
		(R/H)	RECORD AND HELD
		(M)	FIELD MEASURED
		I.P.I.P.E	DRILL HOLE
		I.ROD	IRON PIPE
		VGC	IRON ROD
		38.77 FF(MT) x	VERTICAL GRANITE CURB
			ENTRANCE ELEV. (METAL THRESHOLD)

SCALE: 1" = 10'



ELEVATION BENCH MARKS		
DATUM: CAMBRIDGE CITY BASE		
NO.	DESCRIPTION	ELEV.
1.	FIRE HYDRANT; BOLT OVER MAIN OUTLET	38.98
2.	TOP CONC. WALL; TOP DRILL HOLE (SET)	38.95
3.	HYDRANT; CHISELED "X" IN BONNET BOLT	41.28

MAP 200 LOT 90
CAMBRIDGE HOUSING AUTHORITY
DEED BOOK 14779, PAGE 315
DEED BOOK 15502, PAGE 373
PLAN 327 OF 1984
PLAN 1112 OF 1982
PLAN 1060 OF 1969

EXISTING CONDITIONS
PLAN OF LAND
IN
CAMBRIDGE, MA

PLOT DATE: Oct 20, 2020 10:20 am
PATH: F:\CH 30 Projects\21130 - Capstone Communities - Cambr

DWG: 21130sv.dwg

LAYOUT: EC

SHEET: 1 OF 1

PROJECT NO.:

21130

2072 Massachusetts
Avenue, Cambridge, MA
02140

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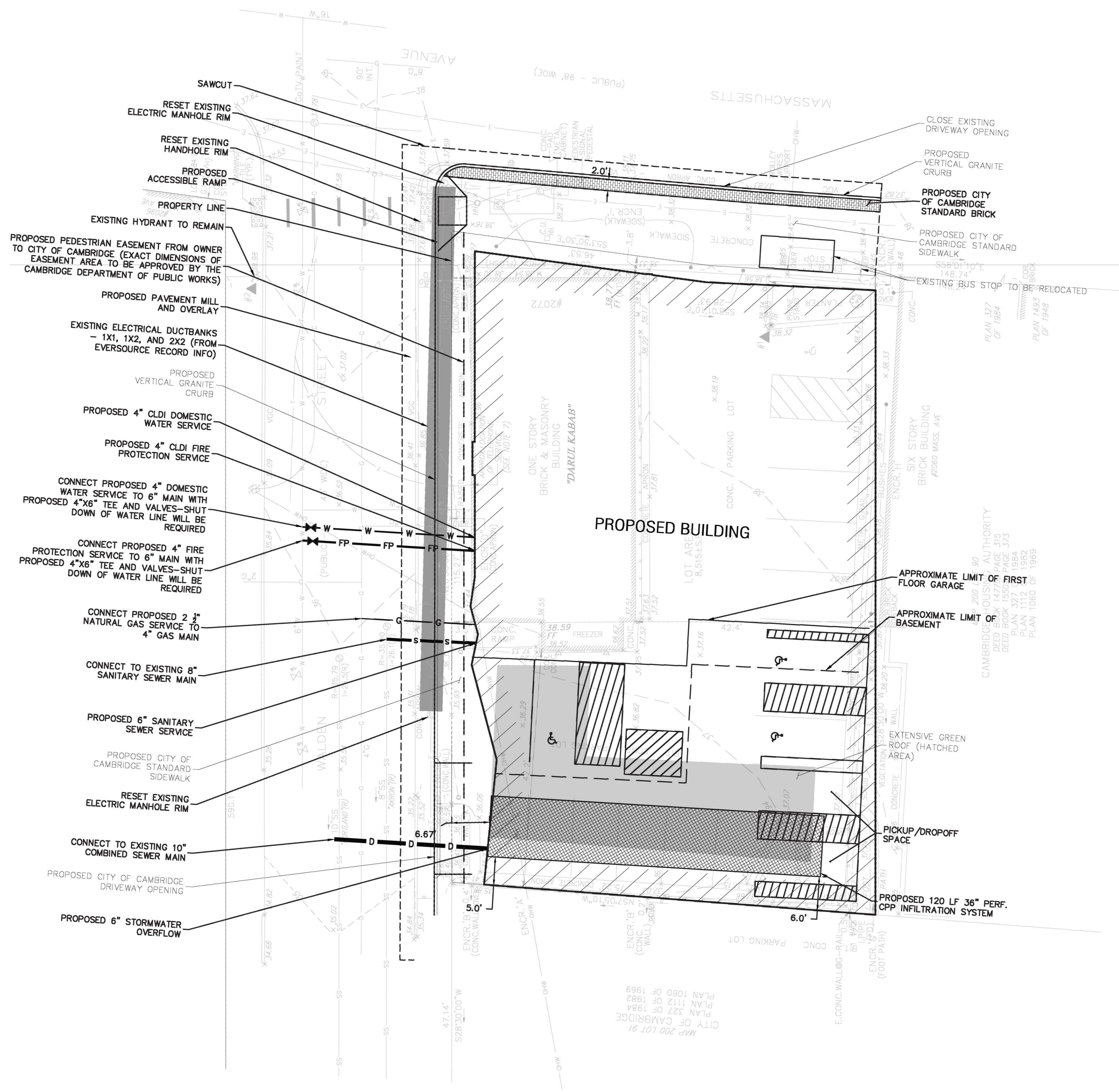
NITSCH PROJECT #14047

COMMONWEALTH OF MASSACHUSETTS
MICHELLE L. CALLAHAN
CIVIL
No. 19959
REGISTERED
PROFESSIONAL ENGINEER

November 9, 2020

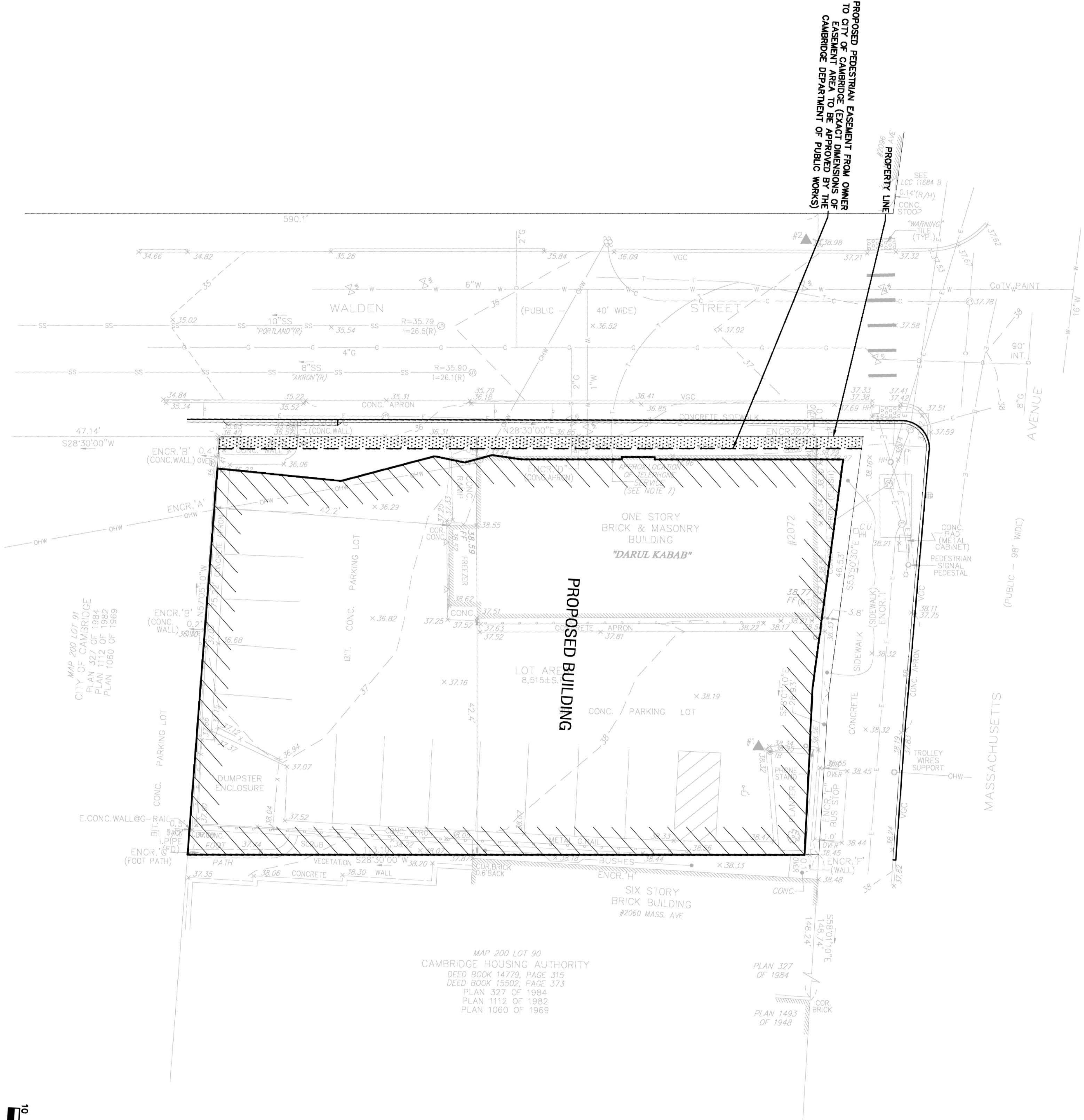
CIVIL STRUCTURAL
FOR CONSTRUCTION
AND LAYOUT PLAN

C-100



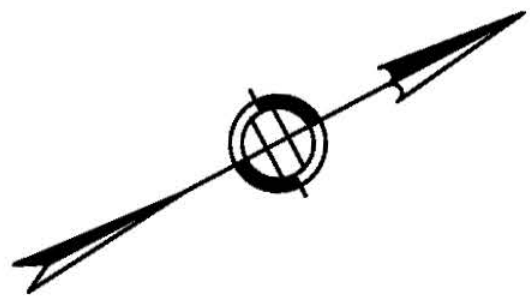
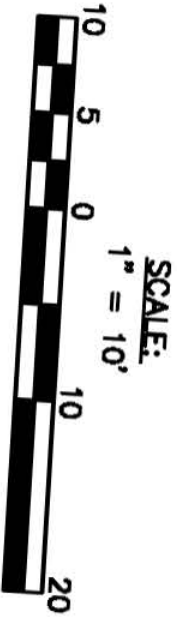
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LEGEND:

EASEMENT AREA



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ARCHITECTS
225 Friend St., Suite 701
Boston, MA 02114
617.492.8400
www.brunercott.com

2 Center Plaza, Suite 430
Boston, MA 02108
T: (617) 339-0035
F: (617) 338-9472
Nitsch Engineering
www.nitsch-engineering.com
Professional Engineer
November 3, 2020
FOR PRESENTATION - NOT
FOR CONSTRUCTION
**PROPOSED
EASEMENT PLAN**

Date	NOVEMBER 10, 2020
Scale	1"=10'
Project Number	NTSCH #1007
Drawn By	MLC

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C-101

Date	NOVEMBER 10, 2020
Scale	1/8" = 1'-0"
Project Number	20.001
Drawn By	Author

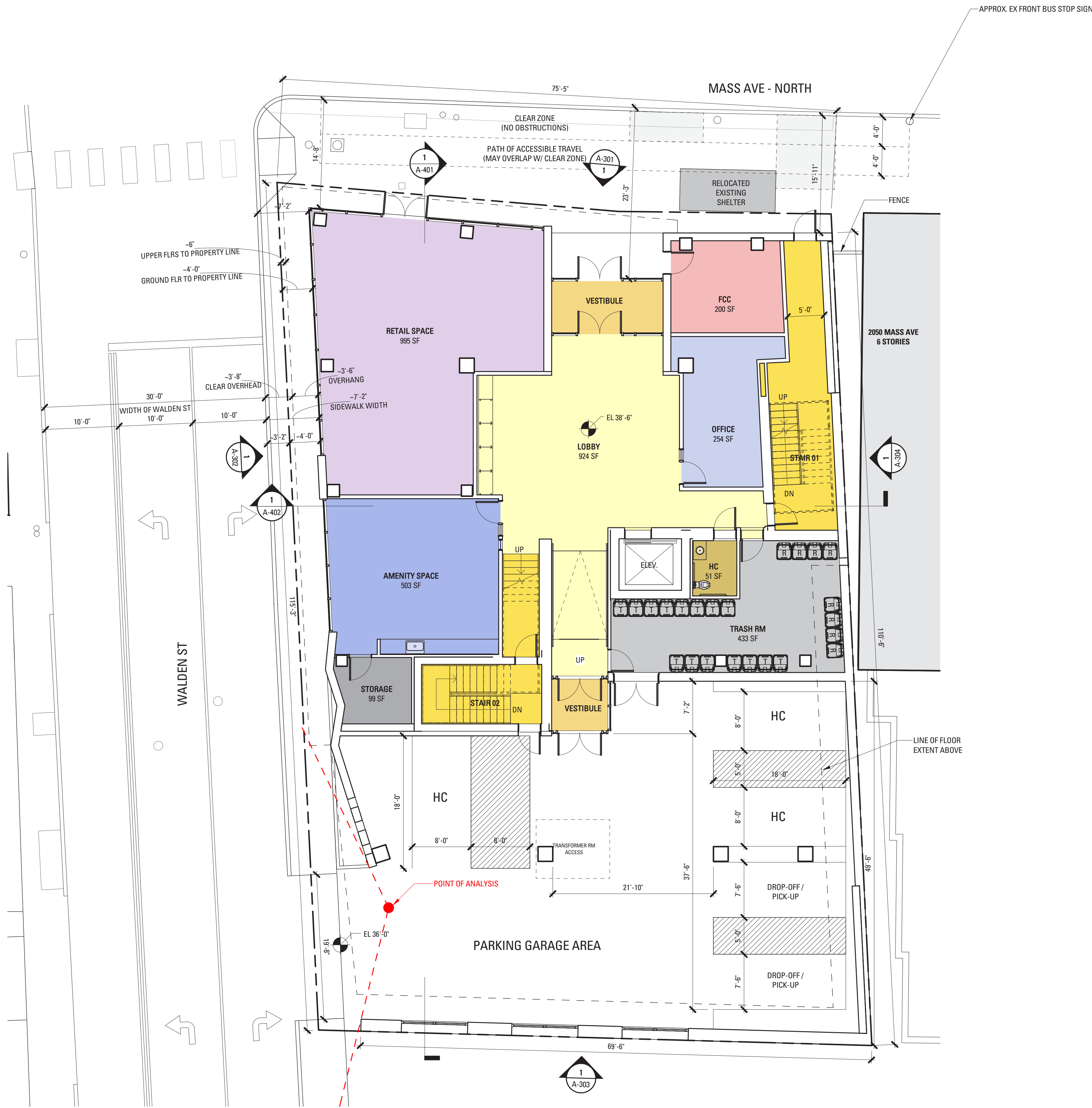
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A-100



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1 FLOOR PLAN - FIRST FLOOR
SCALE: 1/8" = 1'-0"

FLOOR AREA PLAN

AMENITY SPACE	503 SF
LOBBY	200 SF
RETAIL SPACE	51 SF
FCC	924 SF
HC	254 SF
OFFICE	995 SF
STAIR 01	315 SF
STAIR 02	259 SF
STORAGE	99 SF
TRASH RM	433 SF
VESTIBULE	174 SF



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Rev	Date	Remarks

Date	NOVEMBER 10, 2020
Scale	1/8" = 1'-0"
Project Number	20.001
Drawn By	Author

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FIRST FLOOR PLAN

A-101

Date	NOVEMBER 10, 2020
Scale	1/8" = 1'-0"
Project Number	20.001
Drawn By	Author

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A-102



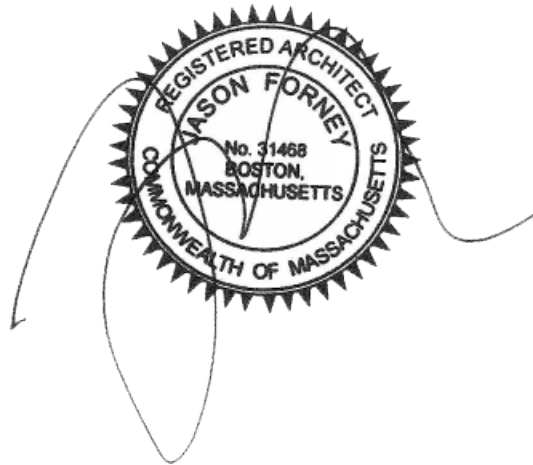
Rev	Date	Remarks

Date	NOVEMBER 10, 2020
Scale	1/8" = 1'-0"
Project Number	20.001
Drawn By	Author

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ROOF PLAN





Date	NOVEMBER 10, 2020
Scale	1/16" = 1'-0"
Project Number	20.001
Drawn By	Author

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Avenue, Cambridge, MA
02140

A-300

Date	NOVEMBER 10, 2020
Scale	1/8" = 1'-0"
Project Number	20.001
Drawn By	Author

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02140

A-301



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1 BUILDING ELEVATION - WEST
SCALE: 1/8" = 1'-0"

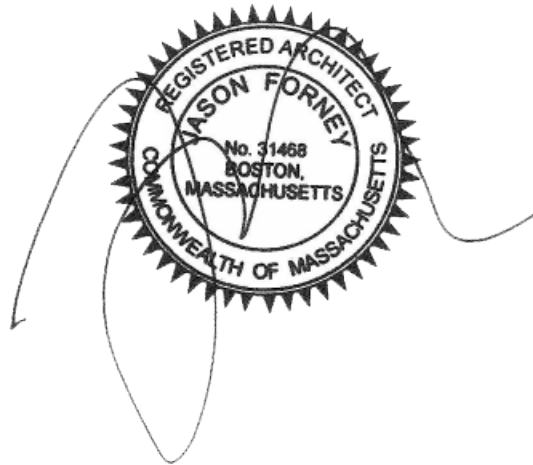
Rev	Date	Remarks

Date	NOVEMBER 10, 2020
Scale	1/8" = 1'-0"
Project Number	20.001
Drawn By	Author

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EXTERIOR ELEVATIONS

Rev	Date	Remarks

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Project Number	20.001
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EXTERIOR ELEVATIONS

A-303



1 BUILDING ELEVATION - SOUTH
SCALE: 1/8" = 1'-0"



Rev	Date	Remarks

Date	NOVEMBER 10, 2020
Scale	1/8" = 1'-0"
Project Number	20.001
Drawn By	Author

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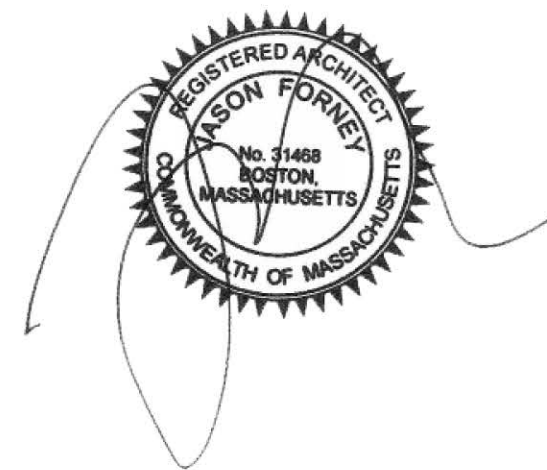
EXTERIOR ELEVATIONS

A-304

[illegible]

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Avenue, Cambridge, MA
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A-305

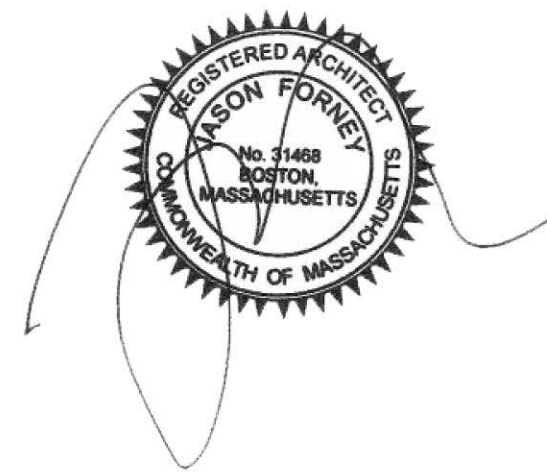


RENDERING - VIEW AT MASS AVE TOWARDS WALDEN ST LOOKING NORTH-WEST

Rev	Date	Remarks
Date	NOVEMBER 10, 2020	
Scale		
Project Number	20.001	
Drawn By	Author	

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A-306

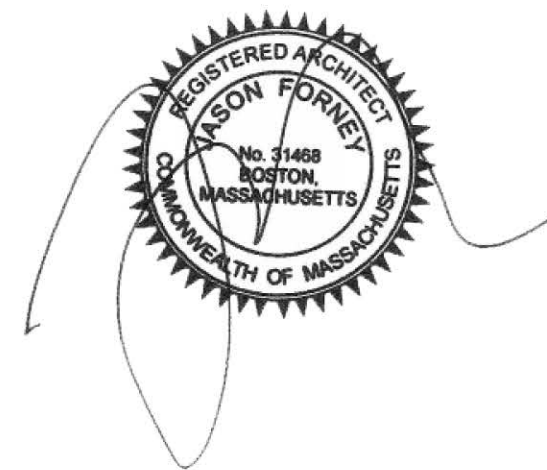


RENDERING - VIEW OF MASS AVE & WALDEN ST INTERSECTION LOOKING SOUTH-EAST

[illegible]

2072 Massachusetts
Avenue, Cambridge, MA
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A-310



RENDERING - VIEW ALONG MASS AVE LOOKING SOUTH-EAST

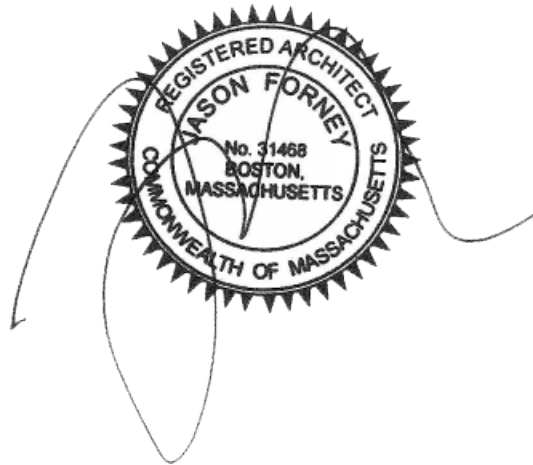
Rev	Date	Remarks

Date	NOVEMBER 10, 2020
Scale	1/8" = 1'-0"
Project Number	20.001
Drawn By	EP

2072 MASS AVE

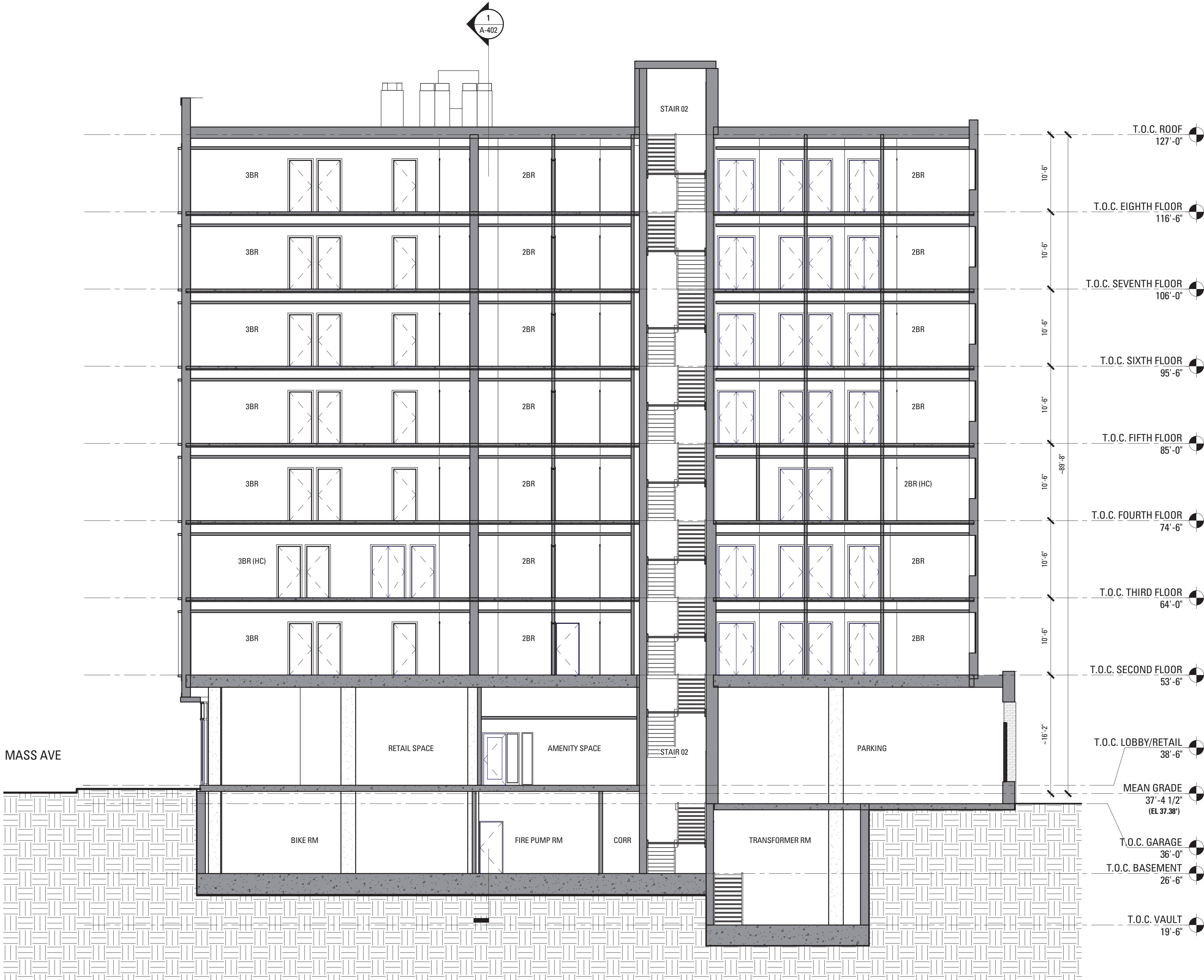
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BUILDING SECTION

A-401

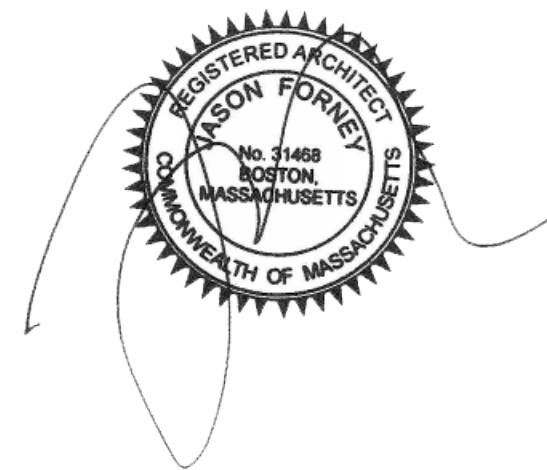


1 BUILDING SECTION - NORTH-SOUTH
SCALE: 1/8" = 1'-0"



2072 MASS AVE

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A-402



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2072 MASS AVE APARTMENTS
2072 MASSACHUSETTS AVENUE, CAMBRIDGE, 02140

SECTION 12
PERSPECTIVES AND MATERIALS

Date	NOVEMBER 10, 2020
Scale	
Project Number	20.001
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A-307

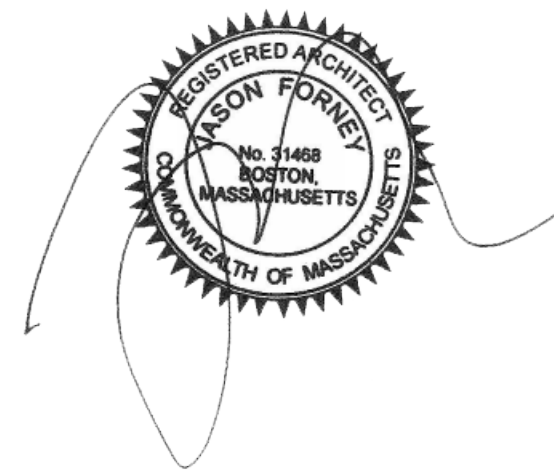
225 Friend St., Suite 701
Boston, MA 02114
617.492.8400
www.brunercott.com

[illegible]

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Scale	
Project Number	20.001
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PERSPECTIVES

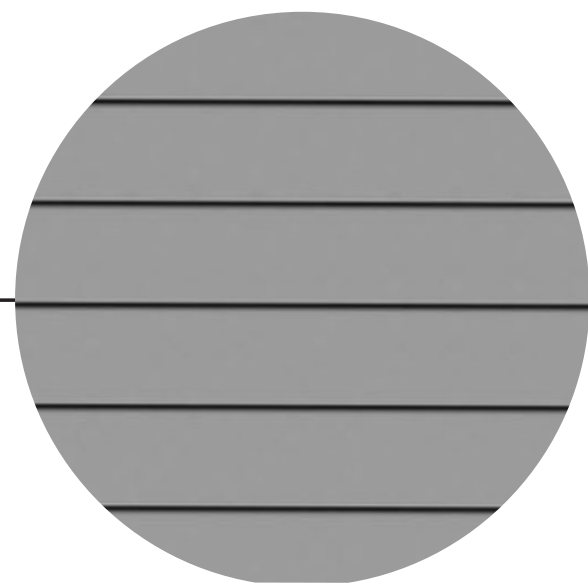
A-308



RENDERING - VIEW OF WALDEN ST LOOKING WEST



MASONRY SOLDIER COURSE AT HEADER



FIBER CEMENT CLAPBOARD
SIDING, PTD



OPENINGS IN MASONRY FACADE

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Date NOVEMBER 10, 2020

Scale

Project Number	20.001
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Drawn By	Author
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2072 Massachusetts
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02140

PERFORATED MTL SUN SHADES

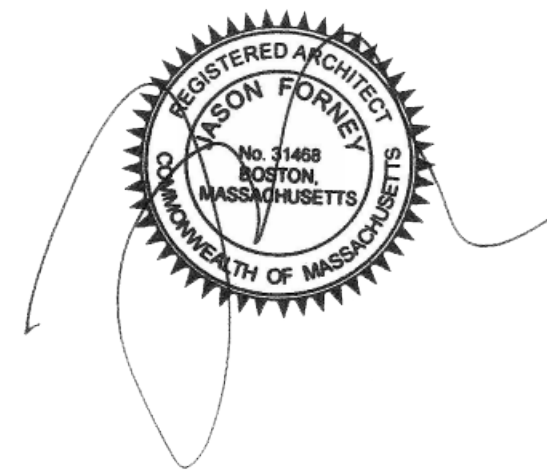
GREEN ROOF

HALF ROUND FIBER CEMENT SHINGLES, PTD

FIBER CEMENT CLAPBOARD SIDING, PTD

FIBER CEMENT SHAKE SHINGLES, F

COMPREHENSIVE PERMIT
NOT FOR CONSTRUCTION



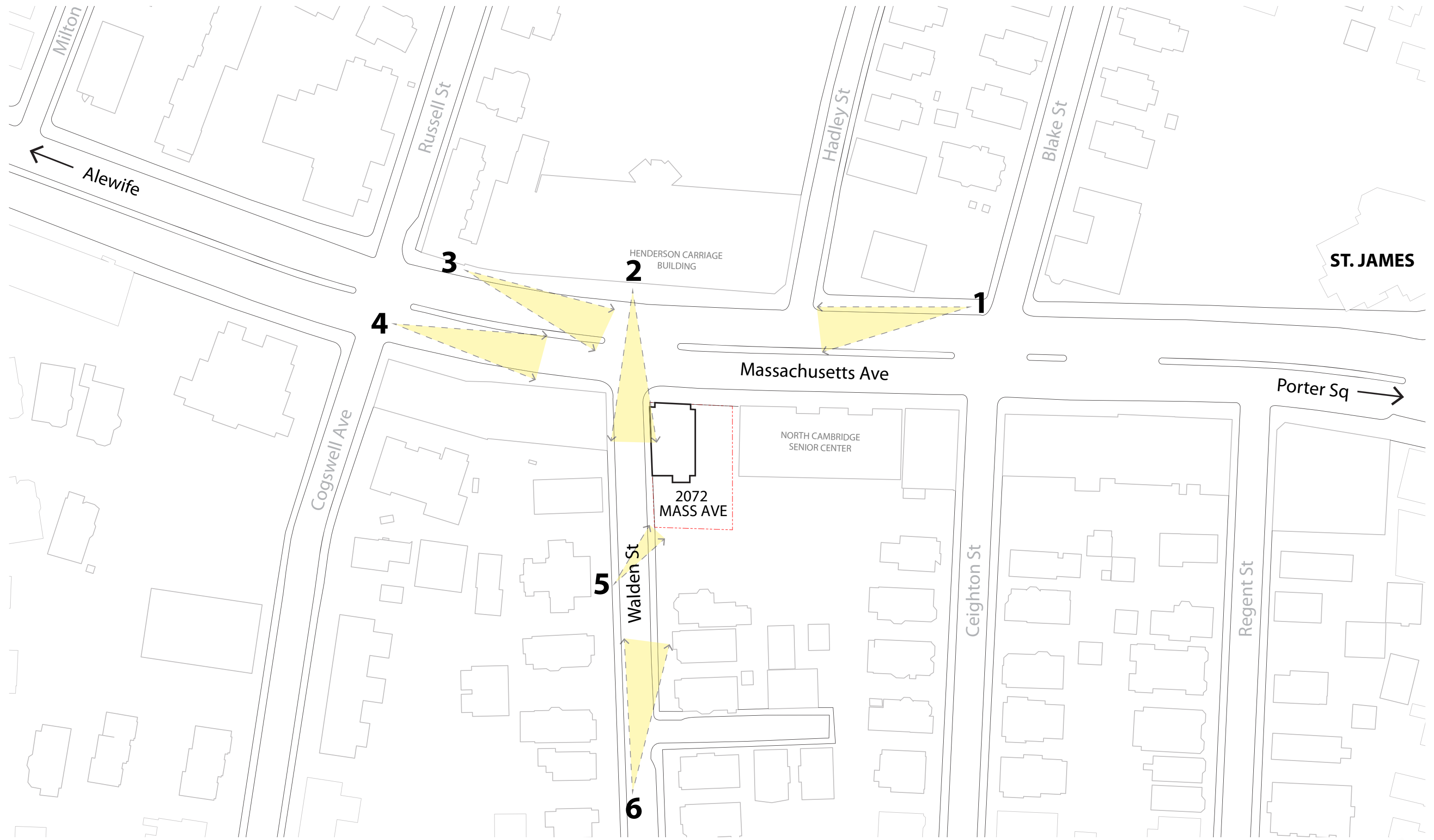
PERSPECTIVES

A-309

COMPREHENSIVE PERMIT APPLICATION

2072 MASS AVE APARTMENTS
2072 MASSACHUSETTS AVENUE, CAMBRIDGE, 02140

SECTION 13
PHOTOGRAPHS



SITE PLAN



View on Massachusetts Avenue
Looking North-West

SITE PHOTOS



View towards Walden Street
Looking South-East

SITE PHOTOS



Corner of Massachusetts Avenue & Walden Street
Looking South

SITE PHOTOS



View on Massachusetts Avenue
Looking South-East

SITE PHOTOS



View of property rear & adjacent city parking lot
Looking East

SITE PHOTOS



View of Walden Street towards Massachusetts Avenue
Looking North

SITE PHOTOS

COMPREHENSIVE PERMIT APPLICATION

2072 MASS AVE APARTMENTS
2072 MASSACHUSETTS AVENUE, CAMBRIDGE, 02140

SECTION 14
SHADOW STUDIES



LEGEND

- Existing Shadow
- New Shadow

SUMMER SOLSTICE | June 21, 9:00am

SHADOW STUDIES



LEGEND

- Existing Shadow
- New Shadow

SUMMER SOLSTICE | June 21, 12:00pm

SHADOW STUDIES

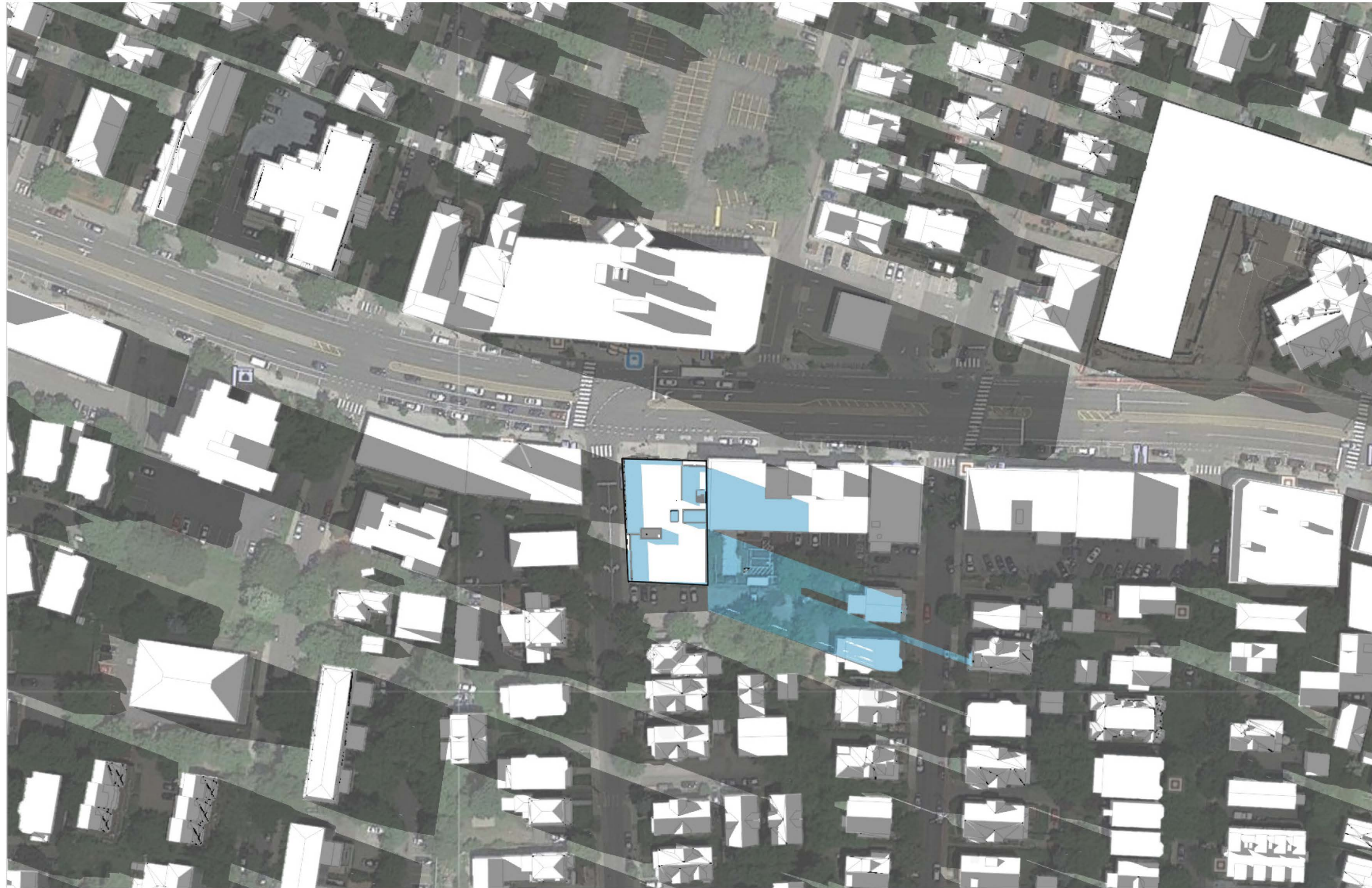


LEGEND

- Existing Shadow
- New Shadow

SUMMER SOLSTICE | June 21, 3:00pm

SHADOW STUDIES



LEGEND

- Existing Shadow
- New Shadow

SUMMER SOLSTICE | June 21, 6:00pm

SHADOW STUDIES



LEGEND

- Existing Shadow
- New Shadow

WINTER SOLSTICE | December 21, 9:00am

SHADOW STUDIES



LEGEND

- Existing Shadow
- New Shadow

WINTER SOLSTICE | December 21, 12:00pm

SHADOW STUDIES



LEGEND

- Existing Shadow
- New Shadow

WINTER SOLSTICE | December 21, 3:00pm

SHADOW STUDIES



LEGEND

- Existing Shadow
- New Shadow

EQUINOX | March 21 / September 21, 9:00am

SHADOW STUDIES



LEGEND

- Existing Shadow
- New Shadow

EQUINOX | March 21 / September 21, 12:00pm

SHADOW STUDIES



LEGEND

- Existing Shadow
- New Shadow

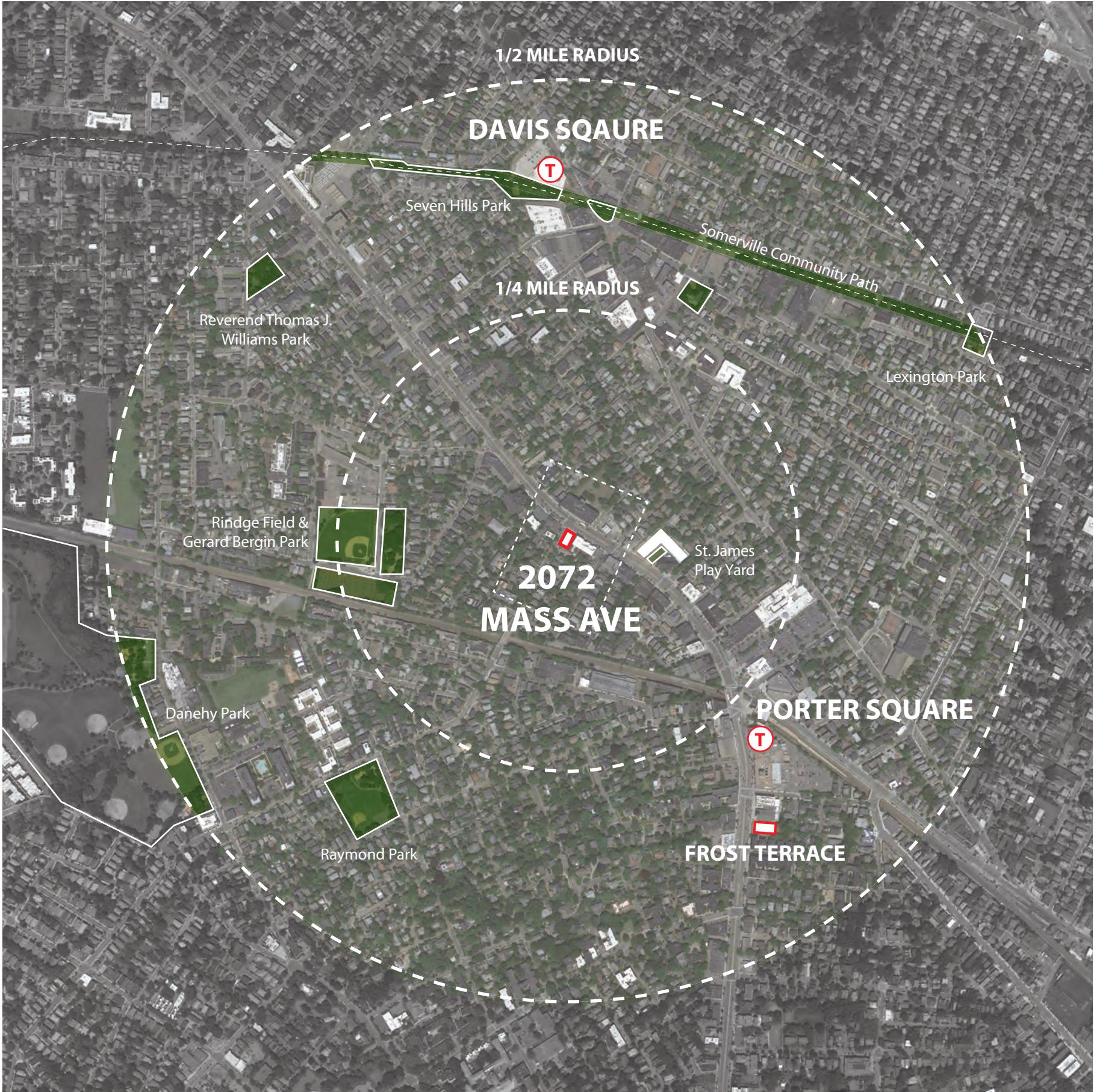
EQUINOX | March 21 / September 21, 3:00pm

SHADOW STUDIES

COMPREHENSIVE PERMIT APPLICATION

2072 MASS AVE APARTMENTS
2072 MASSACHUSETTS AVENUE, CAMBRIDGE, 02140

SECTION 15
LOCUS MAP AND ASSESSOR PLAT



LOCUS MAP

Bruner/Cott
ARCHITECTS

APPLICANT: CC HRE 2072 MASS AVE TENANT LLC



2072 MASSACHUSETTS AVENUE
NOVEMBER 10, 2020



Map-Lot
199-90

Map-Lot
181-88

Map-Lot
199-89

Map-Lot
199-87

Map-Lot
199-85

Map-Lot
199-86

Map-Lot
199-112

Map-Lot
199-113

Walden St

Hadley St

Massachusetts Ave

Map-Lot
181-80

Map-Lot
200-90

Map-Lot
200-91

Map-Lot
200-81

Map-Lot
200-74

Map-Lot
200-80

Map-Lot
200-88

Map-Lot
200-79

Creighton St

Map-Lot
200-68

Map-Lot
200-28

Houston Pk

COMPREHENSIVE PERMIT APPLICATION

2072 MASS AVE APARTMENTS
2072 MASSACHUSETTS AVENUE, CAMBRIDGE, 02140

SECTION 16
EVIDENCE OF SITE CONTROL

Middlesex South Registry of Deeds

Electronically Recorded Document

This is the first page of the document - Do not remove

Recording Information

Document Number	: 48349
Document Type	: DEED
Recorded Date	: April 10, 2018
Recorded Time	: 01:57:08 PM
Recorded Book and Page	: 70850 / 295
Number of Pages(including cover sheet)	: 4
Receipt Number	: 2196120
Recording Fee (including excise)	: \$16,541.00

MASSACHUSETTS EXCISE TAX
Southern Middlesex District ROD # 001
Date: 04/10/2018 01:57 PM
Ctrl# 279156 10927 Doc# 00048349
Fee: \$16,416.00 Cons: \$3,600,000.00

Middlesex South Registry of Deeds
Maria C. Curtatone, Register
208 Cambridge Street
Cambridge, MA 02141
617-679-6300
www.middlesexsouthregistry.com

After recording return to:
 Day Pitney LLP
 One International Place
 Boston, MA 02110
 Attn: Gemma Cashman, Esq.

QUITCLAIM DEED

2072 Mass Ave LLC, a Massachusetts limited liability company whose address is 33 Church Street, Cambridge, Massachusetts ("Grantor")

for consideration paid of Three Million Six Hundred Thousand and 00/100 Dollars (\$3,600,000.00),

grants to CC HRE 2072 Mass Ave LLC, a Massachusetts limited liability company whose address is c/o Capstone Communities LLC, 1155 Walnut Street #31, Newton Highlands, Massachusetts ("Grantee"),

WITH QUITCLAIM COVENANTS

A certain parcel of land situated in Cambridge, Middlesex County, Massachusetts, bounded and described as follows:

A certain parcel of land with the buildings thereon situated in said Cambridge on the Southeasterly corner of Massachusetts Avenue and Walden Street, and more particularly bounded and described as follows:

NORTHWESTERLY:	By said Walden Street, one hundred and fifteen and 27/100 (115.27) feet more or less;
SOUTHWESTERLY:	By land of Ferguson, seventy-five and 22/100 (75.22) feet more or less;
SOUTHEASTERLY:	By land of Daniel O'Connell, one hundred and thirteen and 10/100 (113.10) feet more or less;
NORTHEASTERLY:	By said Massachusetts Avenue, seventy-five and 46/100 (75.46) feet more or less in two lines.

Containing eight thousand five hundred and fifteen (8,515) square feet of land more or less.

Grantor is not taxed as a business corporation, but rather as a partnership, and therefore is not subject to the provisions of M.G.L. c. 62C.

The within conveyance is made subject to rights of existing tenants, and easements, rights, reservations and restrictions of record, if any, insofar as the same are in force, applicable, and survive the sale described herein, however not intending to revive any of the same hereby.

For title see deed in Book 70018, Page 247.

[Signature Page to Follow.]

WITNESS my hands and seals this 9th day of April, 2018.

2072 MASS AVE LLC

By: [Signature]
Name: William Senne
Title: Authorized Signatory

COMMONWEALTH OF MASSACHUSETTS

Middlesex, ss

On this 9th day of April, 2018, before me, the undersigned notary public, William Senne as Authorized Signatory for 2072 Mass Ave LLC, personally appeared, proved to me through satisfactory evidence of identification, which were personal knowledge, to be the person whose name is signed on the preceding or attached document, and acknowledged to me that he signed it voluntarily for its stated purpose as Authorized Signatory of 2072 Mass Ave LLC as the voluntary act of the limited liability company.

[Signature]
Notary Public Gemma R. Cashman
My Commission Expires 12/10/2021

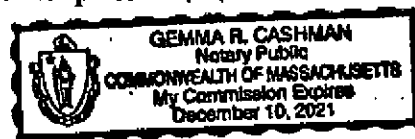


EXHIBIT C
NOTICE OF LEASE

Pursuant to Massachusetts General Laws, Chapter 183, Section 4, as amended, notice is hereby given of a ground lease (the "Lease") as follows:

EFFECTIVE DATE:	November 9, 2020
LANDLORD:	CC HRE 2072 Mass Ave LLC, a Massachusetts limited liability company
TENANT:	CC HRE 2072 Mass Ave Tenant LLC, a Massachusetts limited liability company
DATE OF EXECUTION OF LEASE:	November 9, 2020
DESCRIPTION OF PREMISES:	The parcel of land located in Cambridge, Massachusetts, more particularly described in Exhibit A attached hereto.
COMMENCEMENT DATE OF ORIGINAL TERM:	November 9, 2020
TERM:	99 years expiring on November 9, 2119

Signed, sealed and delivered as of November 9, 2020.

LANDLORD:

CC HRE 2072 Mass Ave LLC, a Massachusetts
limited liability company

By: Capstone 2072 Mass Ave LLC, its managing
member

By: 

Name: Jason Korb

Title: Managing Member

By: HRE 2072 Mass Ave LLC, its managing
member

By: 

Name: Sean D. Hope

Title: Managing Member

TENANT:

CC HRE 2072 MASS AVE TENANT LLC, a
Massachusetts limited liability company

By: Capstone 2072 Mass Ave LLC, its managing
member

By: 

Name: Jason Korb

Title: Managing Member

By: HRE 2072 Mass Ave LLC, its managing
member

By: 

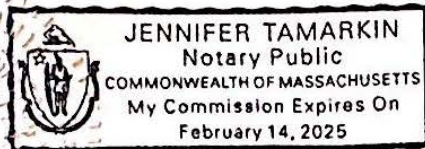
Name: Sean D. Hope

Title: Managing Member

COMMONWEALTH OF MASSACHUSETTS

COUNTY OF SUFFOLK

On this 9th day of November, 2020, before me, the undersigned notary public, personally appeared Jason Korb, managing member of Capstone 2072 Mass Ave LLC, as managing member of CC HRE 2072 Mass Ave LLC, proved to me through satisfactory evidence of identification, which was personal knowledge, to be the person whose name is signed on the preceding or attached document, and acknowledged to me that he signed it voluntarily for its stated purpose in my presence as managing member of Capstone 2072 Mass Ave LLC, as managing member of CC HRE 2072 Mass Ave LLC, as the voluntary act of CC HRE 2072 Mass Ave LLC.

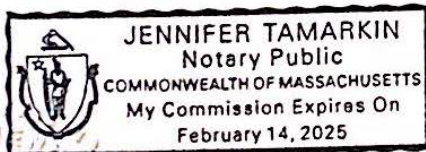




(Official signature and seal of Notary)
My Commission Expires:

COMMONWEALTH OF MASSACHUSETTS

COUNTY OF SUFFOLK

On this 9th day of November, 2020, before me, the undersigned notary public, personally appeared Sean D. Hope, managing member of HRE 2072 Mass Ave LLC, as managing member of CC HRE 2072 Mass Ave LLC, proved to me through satisfactory evidence of identification, which was personal knowledge, to be the person whose name is signed on the preceding or attached document, and acknowledged to me that he signed it voluntarily for its stated purpose in my presence as managing member of HRE 2072 Mass Ave LLC, as managing member of CC HRE 2072 Mass Ave LLC, as the voluntary act of CC HRE 2072 Mass Ave LLC.

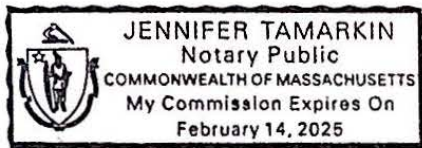


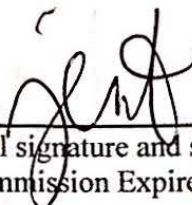

(Official signature and seal of Notary)
My Commission Expires:

COMMONWEALTH OF MASSACHUSETTS

COUNTY OF SUFFOLK

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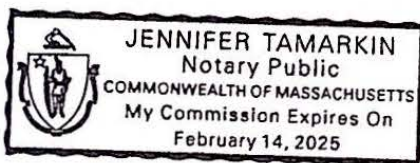



(Official signature and seal of Notary)
My Commission Expires:

COMMONWEALTH OF MASSACHUSETTS

COUNTY OF SUFFOLK

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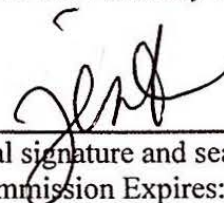

(Official signature and seal of Notary)
My Commission Expires:

EXHIBIT A TO NOTICE OF LEASE
Description of the Premises

2072 Massachusetts Avenue, Cambridge, Massachusetts

A certain parcel of land with the buildings thereon situated in said Cambridge on the Southeasterly corner of Massachusetts Avenue and Walden Street, and more particularly bounded and described as follows:

- NORTHWESTERLY: By said Walden Street, one hundred and fifteen and 27/100 (115.27) feet more or less;
- SOUTHWESTERLY: By land of Ferguson, seventy-five and 22/100 (75.22) feet more or less;
- SOUTHEASTERLY: By land of Daniel O'Connell, one hundred and thirteen and 10/100 (113.10) feet more or less;
- NORTHEASTERLY: By said Massachusetts Avenue, seventy-five and 46/100 (75.46) feet more or less in two lines.

Containing eight thousand five hundred and fifteen (8515) square feet of land more or less.



City of Cambridge

MASSACHUSETTS

BOARD OF ZONING APPEAL

831 Mass Avenue, Cambridge, MA.
(617) 349-6100

COMPREHENSIVE PERMIT APPLICATION PROCESS

ADDRESS OF PROPERTY: 2072 Mass Avenue

A copy of the proposed plan with description of the project be submitted to the following City Agencies. Please provide evidence of submission to these agencies. Upon completion, this sheet must be submitted to the Board Zoning Appeals case file.

Signature and Date

[Signature] 11/17/2020

City Department/Address

Community Development Department,
344 Broadway

Conservation Commission,
147 Hampshire Street

Fire Department,
491 Broadway

Historical Department,
831 Massachusetts Avenue

Law Department,
795 Massachusetts Avenue

Public Works Department,
147 Hampshire Street

Traffic and Parking Department,
344 Broadway



City of Cambridge

MASSACHUSETTS

BOARD OF ZONING APPEAL

831 Mass Avenue, Cambridge, MA.
(617) 349-6100

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Community Development Department,
344 Broadway



Conservation Commission,
147 Hampshire Street

Jennifer Betancourt

Fire Department,
491 Broadway

Historical Department,
831 Massachusetts Avenue

Law Department,
795 Massachusetts Avenue

Public Works Department,
147 Hampshire Street

Traffic and Parking Department,
344 Broadway



City of Cambridge

MASSACHUSETTS

BOARD OF ZONING APPEAL

831 Mass Avenue, Cambridge, MA.
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Community Development Department,
344 Broadway

Conservation Commission,
147 Hampshire Street

CAPT Paul Marinelli

Fire Department,
491 Broadway

Historical Department,
831 Massachusetts Avenue

Law Department,
795 Massachusetts Avenue

Public Works Department,
147 Hampshire Street

Traffic and Parking Department,
344 Broadway



City of Cambridge

MASSACHUSETTS

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344 Broadway

Conservation Commission,
147 Hampshire Street

Fire Department,
491 Broadway

C. Smith 11.17.20

Historical Department,
831 Massachusetts Avenue

Law Department,
795 Massachusetts Avenue

Public Works Department,
147 Hampshire Street

Traffic and Parking Department,
344 Broadway



City of Cambridge

MASSACHUSETTS

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147 Hampshire Street

Fire Department,
491 Broadway

Historical Department,
831 Massachusetts Avenue

Law Department,
795 Massachusetts Avenue

Public Works Department,
147 Hampshire Street

Traffic and Parking Department,
344 Broadway



City of Cambridge

MASSACHUSETTS

BOARD OF ZONING APPEAL

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Conservation Commission,
147 Hampshire Street

Fire Department,
491 Broadway

Historical Department,
831 Massachusetts Avenue

Law Department,
795 Massachusetts Avenue

Kao Wao

Public Works Department,
147 Hampshire Street

Traffic and Parking Department,
344 Broadway



City of Cambridge

MASSACHUSETTS

BOARD OF ZONING APPEAL

831 Mass Avenue, Cambridge, MA.
(617) 349-6100

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Conservation Commission,
147 Hampshire Street

Fire Department,
491 Broadway

Historical Department,
831 Massachusetts Avenue

Law Department,
795 Massachusetts Avenue

Public Works Department,
147 Hampshire Street



11/16/20

Traffic and Parking Department,
344 Broadway

Cambridge Affordable Housing Trust

December 1, 2020

Mr. Constantine Alexander
Chairman, Board of Zoning Appeal
c/o City of Cambridge Inspectional Services Division
831 Massachusetts Avenue
Cambridge, MA 02139

RE: 2072 Massachusetts Avenue - Comprehensive Permit Application

Dear Mr. Alexander and Members of the Board:

On behalf of the Cambridge Affordable Housing Trust, we are writing to share our strong support for Capstone Communities LLC and Hope Real Estate Enterprises LLC's application for a Comprehensive Permit for their proposed redevelopment of 2072 Massachusetts Ave.

Capstone/Hope's thoughtful design will result in a development which will transform an underutilized site into 49 units of vibrant family housing. The Trust has already financed the acquisition of this site and is looking forward to reviewing requests for additional funding at an upcoming Trust meeting as the project proceeds towards development.

The acquisition and redevelopment of this site represents a unique and important opportunity to create a substantial number of affordable family-sized apartments. As you know, the need for affordable housing is as great as ever, as market rents in Cambridge continue to far outpace what low and moderate-income families can afford. Unfortunately, we anticipate that the need for affordable housing will only intensify as an increasing number of residents experience job and housing insecurity due to the COVID-19 and the resulting economic fallout. Capstone/Hope's plan to create almost 50 new affordable rental units, including 21 two-bedroom and 14 three-bedroom apartments, will go a long way toward helping to address this need.

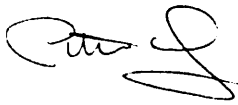
The site is ideally located for housing, in close proximity to the many amenities of Porter Square, including the MBTA and Commuter Rail Station, shopping center, and many restaurants and retail along Mass. Ave, and with Davis Square only a few blocks further away.

In addition to providing new affordable housing, the proposal to redevelop 2072 Massachusetts Avenue also includes a plan to widen Walden Street, as well as the sidewalk in front of the new building, improvements which will greatly benefit the broader community.

Affordable housing development opportunities such as this are rare, given the challenges of finding sites and assembling feasible development plans. We support the decision to pursue a comprehensive permit in order to obtain the relief needed for the proposed design and are hopeful that this project will be able to move forward quickly so that families can benefit from this badly needed housing.

We appreciate the Board's longstanding support of affordable housing and thank you for your consideration of this important project.

Sincerely,



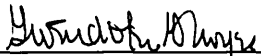
Peter Daly




Florrie Darwin



Elaine DeRosa



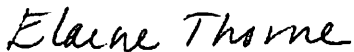
Gwen Noyes



Susan Schlesinger



James Stockard



Elaine Thorne




William Tibbs

Fw: dangerous project--action needed



Daglian, Sisia

To Pacheco, Maria

 If there are problems with how this message is displayed, click here to view it in a web browser.

From: Ausra Kubilius <ausmkub@gmail.com>

Sent: Monday, December 7, 2020 3:22 PM

To: Daglian, Sisia <sdaglian@cambridgema.gov>

Subject: dangerous project--action needed

Dear Sisia Daglian, Zoning Division of Institutional Services Department:

Please help avoid more deaths and accidents at the notoriously dangerous intersection of Mass Ave and Walden St in North Cambridge.

As you know, an affordable housing high-rise--to house many children--has been proposed for 2072 Mass Ave at this corner. **And no Traffic Impact Study has been done.** Without any evidence, the developers claim their widening of Walden St will solve the problem.

Please help ensure that the Zoning Board of Appeals, planning to meet on Dec. 10, honors Article 19 and requires a full Traffic Impact Review and Study.

Also, the proposed looming project abuts (only 4 feet away) the Russell affordable housing apartments for seniors/disabled and the North Cambridge Senior Center. The safety (and unhindered access to sunshine) of seniors must be taken into account. Again, please enforce all provisions of Article 19.

Many, many, many Cambridge neighbors and commuters are concerned about this issue.

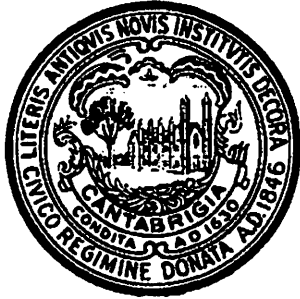
Thank you, A.M. Kubilius (close neighbor at 21 Cogswell Ave)

PS: Our North Walden Neighbors group's petition for action from the BZA/city government has already been signed by over 220--and counting.



ReplyForward

Liza Paden



CITY OF CAMBRIDGE, MASSACHUSETTS

PLANNING BOARD

CITY HALL ANNEX, 344 BROADWAY, CAMBRIDGE, MA 02139

December 8, 2020

To: The Board of Zoning Appeal

From: The Planning Board

RE: BZA-017326-2020 2072 Mass Avenue

The Planning Board reviewed this application for a comprehensive permit at its meeting on December 1, 2020. The Board heard a summary of the project by the applicant, received written and oral testimony from the public, and received written materials from the Community Development Department (CDD), Traffic, Parking & Transportation Department (TP&T), and Department of Public Works (DPW).

Planning Board members were supportive of the proposed project and recommended sending a favorable recommendation to the Board of Zoning Appeal (BZA) to grant the requested relief by a vote of 5-0.

Generally, Planning Board members were encouraged by the provision of affordable housing units, particularly family-sized units, in a development with convenient access to nearby transit. Board members commented that the proposed building is well-designed and will be an improvement over the current one-story commercial building and associated surface parking, which does not contribute positively to the intersection. Board members noted that the height of the proposed building is greater than the immediately surrounding buildings, but also noted that there are examples of buildings of this height elsewhere on northern Massachusetts Avenue. Board members also expressed the view that the additional density is not a concern in this location, and that the acute need for affordable housing justifies an exceptional approach in order to make developments feasible and serve the community to the greatest extent possible.

Pacheco, Maria

From: Alex Wang <awswim@gmail.com>
Sent: Tuesday, December 8, 2020 10:56 PM
To: Pacheco, Maria
Subject: Board of Zoning Appeal BZA-017326-2020

Hello board members,

I write in support of the 100% affordable housing being built at 2072 Massachusetts Ave. We are in the midst of a housing crisis and the more units we can build, the better. I'm in support of a waiver for height and parking requirements. This spot is on the busy Mass Ave. corridor and a close walking distance to both Porter and Davis T stops. Many bus stops are closer, along with restaurants and Star Market. As long as our city and state continue to support the services and businesses in the area, the walkability makes this one of the most desirable spots to live in Cambridge and the whole country. We have such a large waiting list for affordable housing, surely a significant majority of the units can be made up of new residents that want to take advantage of this walkability and don't need a car.

Alex Wang
45 Market St.

Pacheco, Maria

From: Rebekah E Bjork <bjork@mit.edu>
Sent: Tuesday, December 8, 2020 10:20 PM
To: Pacheco, Maria
Cc: jtamarkin@capstonecommunities.com; jkorb@capstonecommunities.com;
sean@hopelegal.com
Subject: ZBA 017326-2020.

Members of the Board of Zoning Appeals

I am writing to express support for the affordable housing development proposed to be built at 2072 Massachusetts Avenue. Cambridge is in desperate need of more affordable housing and I am very impressed with the proposed building for 2072 Massachusetts Ave. North Cambridge is a logical part of the city in which to increase density in order to meet our growing demand for housing, especially for affordable housing. I am pleased to see this proposal moving forward in North Cambridge and specifically in this location between Porter and Davis Square. This particular location is particularly appealing for those opting to live car-free or car-minimum use lifestyle since its between 2 subway stops near multiple bus lines and also within walking distance of both the stores at Porter square and Pemberton farms and other stores along Mass ave. It is also within easy biking distance of the plazas at the Fresh Pond Rotary.

In addition to being 100% affordable housing I am pleased that the proposed building includes a range of apartment sizes from studios through 3 bedrooms so that individuals and families of varying sizes will all be represented in the new community. I was also pleased to see that 70% of the units will be reserved for those already living or working in Cambridge. I think a city is most vibrant and equitable when those who work in the city, in particular in retail, restaurant and other jobs that create a vibrant city, can afford to live in the city.

I am also excited by the environmentally friendly green roof, solar panels and other sustainability features of the design. Additionally the inclusion of bike parking, along with proximity to public transit, and electric vehicle charge will allow people to limit their contribution to climate change by opting to bike, walk and take the T for more trips and enable residents to opt for an electric car if they choose to own a car. Additionally taller buildings, which allow for more density in a given footprint, are a powerful way to lower the per person contribution to climate change through more efficient energy use.

Thank you
Rebekah Bjork
20 Concord Ave Unit C
Cambridge, MA 02138

December 8, 2020

City of Cambridge Board of Zoning Appeal
831 Massachusetts Avenue
Cambridge, MA 02139

Via email to: mpacheco@cambridgema.gov

RE: Support for 2072 Massachusetts Avenue Case Number BZA-017326-2020

Dear Members of the Board of Zoning Appeal,

We are writing to express our strong support for the 49-unit 100% affordable housing apartment building proposed at 2072 Massachusetts Avenue. At a time when housing prices are out of reach for most families and many of our neighbors are unemployed and living with the increased threat of eviction, it is essential that Cambridge approve the 2072 Mass Ave project and others like it.

We are long-time residents of Cambridge (one of us grew up on Lakeview Ave). We chose to make Cambridge our home and raise our kids here (currently Graham and Parks and VLUS) because of the rich socioeconomic and ethnic diversity of this city. Building projects like 2072 are key to preserving this diversity. We live directly across the street from the Daniel Burns apartments, a 200 unit complex for seniors and know that many of our neighbors in these units (some of whom grew up in the neighborhood) would not be able to live in Cambridge if it weren't for this kind of affordable housing.

Residents opposed to this project point to congested traffic through the intersection as one reason the project should not go forward. In non-Covid times, we traverse the Walden Street/Mass Avenue intersection 2-3 daily. We experience this intersection as both drivers and pedestrians and are delighted the Developer is going to improve safety here for the entire community. Rush hour congestion in this area occurs because it is one of the few ways for regional commuters to traverse the train tracks (Mass Ave and Sherman St have the exact same issue). We would sometimes wait 10 minutes or more to make the left onto Mass Ave from Walden. A bit of traffic is part of life in a vibrant city! As residents, we should be far more concerned about making good on our commitment to giving low income families an opportunity to live in the city and attend its excellent schools.

We hope you will support this beautiful, site-appropriate project that will enhance our streets and provide much needed housing to our residents.

Sincerely,

Miranda Pearce and Matt Goldstein
Owners/Residents of 52 Clarendon Ave
415-215-2396

Pacheco, Maria

From: Michael Brandon <mjbrandon@gmail.com>
Sent: Tuesday, December 8, 2020 8:33 PM
To: Pacheco, Maria
Cc: Singanayagam, Ranjit; O'Grady, Sean; Daglian, Sisia; Richard Clarey
Subject: URGENT: 2072 Mass. Ave. Apts (Case. No. 17326)

Chairman Alexander and Members of the BZA:

Without delay, please **CANCEL** the announced public hearing on this comprehensive permit application, which was scheduled for December 10 in violation of MGL Chapter 40B and its related regulations.

The City of Cambridge has failed to comply with the procedures and processes detailed in the Massachusetts Housing Partnership's [Chapter 40B Handbook](#) and has been proceeding in flagrant violation of the statute.

The permit application was hurriedly submitted in chunks while the project was undergoing major design changes and it is deficient on its face. Among other shortcomings, no Project Eligibility Letter from a state funding agency was included pursuant to 760 CMR 56.05(2)(g). The applicants thus have no legal standing to appear before the Board of Zoning Appeal seeking a comprehensive permit at this time.

If the hearing is not canceled in advance, the Board seemingly will have no choice but to summarily reject the application after opening the proceeding on Thursday. No one will benefit from that outcome or if this matter winds up in the courts, as seems inevitable if the City and its developer continue to cut corners and pursue an unreasonable timeline that stymies the due process and equal protection under the law that Cambridge citizens are constitutionally guaranteed.

Please **STOP** the unjustified rush to judgment and protect the public interest by canceling Thursday's proceeding as soon as you can.

Sincerely,

Michael Brandon

Michael Brandon, Clerk
North Cambridge Stabilization Committee
[About the NCSC](#)
Tel. 617-864-3520
Fax 617-948-5971

Pacheco, Maria

From: Aaron Homer <aaron.homer@gmail.com>
Sent: Tuesday, December 8, 2020 5:43 PM
To: Pacheco, Maria
Subject: Supporting Proposal BZA-017326-2020

Hello,

As a Cambridge resident, I'm writing to express my support for the proposed new affordable housing development at 2072 Mass Ave. More housing close to public transportation is exactly what Cambridge needs.

Aaron Homer
90 Grozier Rd, Cambridge, MA 02138

Alice K. Wolf

48 Huron Avenue
Cambridge, MA 02138
Phone (617) 868 - 9653
E-mail Alicekwolf@gmail.com
www.alicewolf.org

December 8 2020

Mr. Constantine Alexander Chairman, Cambridge Board of Zoning Appeal
831 Massachusetts Avenue
Cambridge MA 02139

Re: 2072 Mass Ave Affordable Housing Development (ZBA 017326-2020)

Dear Mr. Alexander and Members of the Board of Zoning Appeal:

Please support the comprehensive permit application of Capstone Communities for a 100% affordable housing building at 2072 Massachusetts Avenue. I live about a half a mile from the corner of Walden and Mass. Ave — I get my medical care directly across Mass Ave from that corner.

I support this housing for several reasons:

*Housing affordable for low and moderate income people and families is in very short supply. Particularly, during this time of crisis, we see how close to homelessness many people are. 49 new homes is meaningful.

*This housing will be permanently affordable — in contrast to some earlier generations of affordable housing whose uses expired — an ultimately harmful way of producing affordable housing.

*For future tenants, the location affords a bus stop right in front, a subway line pretty close, and food and other shopping opportunities within walking distance — ideal for at least some carless persons or families.

*Capstone communities has had a history of success in creating 100% affordable housing in Cambridge, most recently at Frost Terrace, on which the BZA also had to act.

I know that you, the members of the BZA, have to take into account many technical aspects of the proposal as well as the question of whether the controversy it has generated has any merits.

I want to speak to my experience as a community member and a former public official in Cambridge with regard to controversy around affordable housing. — In my experience, there is virtually always controversy around the creation of new affordable housing, especially of any size, but not only large buildings.

The aforementioned Frost Terrace and the CHA housing at Garfield street come to mind because they are each about the same 1/2 mile distant from my house as 2072. You are familiar with Frost Terrace. You may not be familiar with the controversy around the original Garfield/Mass Ave CHA construction.

In the mid to late 1980s, the Garfield/Mass Ave site was transferred to the City by the MBTA. The CHA proposed building affordable housing on the site. The need for such housing was great then, too. There was strong opposition from the neighborhood for at least a couple of years. — maybe more. I will not go through the arguments, but basically the neighbors fought for a park. As you know from the housing there now, the proposal was for low—rise buildings.

Within the first month after I became Mayor, in 1990, Vice-Mayor Kenneth Reeves and I went to have a meeting with neighbors. We told them that this site would be affordable housing. Period. That ended the controversy and the housing was built. So, since the early 1990s, many low income families have been successfully housed in a convenient location.

Thank you for your consideration of my concerns. I hope 2072 Mass ave. proposal is approved by the BZA.

Sincerely yours,

Alice Wolf



CITY OF CAMBRIDGE

TRAFFIC, PARKING, + TRANSPORTATION

MEMORANDUM

To: Cambridge Planning Board and Cambridge Board of Zoning Appeal (BZA)

From: Joseph E. Barr, Director

Date: November 20, 2026

Subject: 2072 Massachusetts Avenue Comprehensive Permit Application

The Cambridge Traffic, Parking, and Transportation Department (TP+T) has been working with Sean Hope and Jason Korb on the proposed affordable housing project at 2072 Massachusetts Avenue submitted for CC HRE 2072 Mass Ave Tenant LLC, an affiliate of Capstone Communities LLC and Hope Real Estate Enterprise LLC).

The Project proposes to construct 49 affordable housing rental apartments and first floor retail space for community use at the corner of Massachusetts Avenue and Walden Street. The site is currently occupied by a 1,860 sf Darul Kabab restaurant and was known as the Kentucky Fried Chicken site for many years. The Project proposes three accessible on-site parking spaces, two on-site short-term drop-off/pick-up parking spaces, and 51 long-term bicycle parking spaces located on the lower floor of the building (48 regular and 3 tandem bike spaces) accessible from an elevator.

Mr. Hope and Mr. Korb has been working very cooperatively with TP+T on the Project. TP+T has reviewed the Comprehensive Permit Application and would like to provide the Cambridge Planning Board and Board of Zoning Appeal (BZA) the following comments on the Project:

- TP+T strongly supports this Project. We are particular very supportive of the plan to accommodate three ten-foot wide travel lanes on Walden Street, which we have worked closely on with this developer. This will make the Massachusetts Avenue/Walden Street intersection more functional and safer for traffic moving through the intersection. We believe the change will make the intersection operate more efficiently and more safely by reducing vehicle queuing, it will better fit large vehicles and trucks, and it will create a more comfortable condition for bicyclists.
- At the approach to Massachusetts Avenue, Walden Street is currently 26'8" wide curb to curb, which is substandard for a three-lane intersection approach with a northbound left-turn lane and right-turn lane, and one southbound receiving lane. Typically, TP+T supports narrowing streets for traffic calming measures, not widening streets. However, in this case we believe the modification will help make the intersection dimensions "just right". If intersection lanes are too narrow (which is the current condition), then the intersection will not function well, causing safety issues and traffic impacts (e.g., unnecessary queuing of vehicles because they cannot fit in narrow lanes or do not line-up efficiently at the red light). The condition

causes delay when the light turns green in order for vehicles to proceed through the intersection. TP+T believes that the proposed plan will be beneficial, and it is the right time to make the adjustment as part of the 2072 Massachusetts Avenue site being redeveloped.

- Along with the adjustment to Walden Street, the Walden Street sidewalk adjacent to the Project will also be widened (from approximately 6.5 feet to 7.2 feet). This will be accomplished by setting back the first floor of the building from the property line to maintain an acceptable sidewalk width. Furthermore, in order to maintain the number and size of the affordable housing units, which is a key goal for the City, there will be a slight overhang of the building above the sidewalk, but the Applicant will provide a sidewalk easement. The site plan was carefully developed and reviewed by City Departments, including the Community Development Department, TP+T, and DPW. TP+T and the other departments believe that the plan is well balanced and is an approach that we support. The final details or adjustments will be reviewed by the City as part of the Building Permit process, including any underground utilities.
- TP+T believes that the Project's traffic impacts will be minimal, especially considering that the previous use of the site was a fast food restaurant use which generates more vehicle trips than the proposed Project.
- TP+T believes that the proposed location for the curb cut on Walden Street is the preferred location for various reasons. It allows vehicles to enter or exit the site to either direction on Walden Street or Massachusetts Avenue by using the Massachusetts Ave./Walden Street traffic signal. A curb cut on Massachusetts Avenue would only allow a right turn in or out (providing fewer options) and it would create a conflict with a bus stop and any existing or future bicycle or bus facilities on Massachusetts Avenue. Massachusetts Avenue currently carries the highest volumes of people walking and bicycling in the city, and conflicts with turning vehicles and people on foot and bicycles present safety problems. Eliminating this conflict provides a significant safety benefit. Even though the site has an existing curb cut on both Walden Street and Massachusetts Avenue, eliminating the curb cut on Massachusetts Avenue and consolidating to one curb cut off Walden Street is TP+T's strongly preferred design. Furthermore, TP+T does not believe that three accessible parking spaces and two drop-off/pick-up spaces will cause a significant impact or safety concern. As part of the Building Permit process, TP+T will work with the Applicant and DPW to make sure that the curb cut and parking spaces are properly designed.
- The Comprehensive Permit included a Parking and Traffic Assessment by Vanasse & Associates Inc. which TP+T believes was acceptable for this Project, although we have not formally reviewed or approved that assessment. The Project is located near public transportation including the Porter Square MBTA Red Line Station (located a 5 to 10 minute walk from the site) and the MBTA Bus Routes #77 and #83, including a bus shelter in front of the site (which will need to be temporarily relocated during construction of the Project). There is also a Bluebikes Station located directly across the street and carshare spaces nearby. Given the availability of the various nearby transportation services, and the findings of the Parking and Traffic Assessment, TP+T believes that the Project will not have a substantial traffic or parking impact. Data from other affordable housing projects in Cambridge have shown low

automobile ownership in affordable housing buildings compared to other uses, such as office, research and development projects or market rate housing projects.

- To mitigate the Project's traffic and parking impacts, the Project proposes a package of Transportation Demand Management (TDM) measures including the following items:
 - Designate an on-site employee as the site's Transportation Coordinator to oversee marketing and promoting of transportation options at the site.
 - Provide new residents transportation information packets with information on getting around Cambridge sustainably.
 - Install a real-time transit display screen in the lobby to make it simpler for residents, visitors and employees to access real-time transit and Bluebikes availability information in the area. The screens will also post other useful information such as carpool/vanpools to supermarkets, etc.
 - Subsidize 100% of the cost of an MBTA T pass for employees (building property management/maintenance staff) or \$240 annual reimbursement for bike maintenance for employees who choose to commute by bike.
 - Organize orientation sessions with residents to teach biking rules, safe biking measures, basic maintenance and repairs and help identify bike routes to various locations.
 - Provide a bicycle repair station on-site.
 - Annually, upon initial move-in and lease renewal, residents will be offered the choice of: (1) annual Bluebikes membership (including one-time discount helmet through Bluebikes), (2) \$90 credit for ride share services, (3) 1-month adult MBTA Monthly LinkPass, and/or (4) 3-month Student or Senior Monthly LinkPass. This will be provided per resident (not per household) on an annual basis.
- TP+T expects that when repaving the sidewalks as part of the Project construction, the Applicant and their construction contractors, in coordination and as approved by the City, will remove the curb extension at the northwest corner of the Massachusetts Ave./Walden Street intersection. That curb extension is not needed at the signalized intersection and its removal will provide the City with more flexibility for that intersection in the future for bus or bicycle facility designs. TP+T and DPW will work with the Applicant on this item.
- Finally, overall TP+T supports the Project and the Applicant's application for a Comprehensive Permit.

NOAH MASLAN & JOCELYN KASPER

194 WALDEN STREET, CAMBRIDGE MA 02140

December 8, 2020

City of Cambridge Board of Zoning Appeal
831 Massachusetts Avenue
Cambridge, MA 02139
Via email to: mpacheco@cambridgema.gov

RE: Support for 2072 Massachusetts Avenue Case Number BZA-017326-2020

Dear Members of the Board of Zoning Appeal,

We are writing to express our strong support for the 49-unit 100% affordable housing apartment building proposed at 2072 Massachusetts Avenue. We live at 194 Walden Street with our children, just a few blocks down from the proposed building. This project will improve the corner of Walden Street and Mass Ave, address the housing shortage, and help fight climate change.

This affordable housing is critically needed considering that thousands of families routinely apply for any newly built affordable housing in our city. Additionally, by increasing residential units with access to strong bus service and the Porter Square T Station, a resident of 2072 Massachusetts Avenue will not need a car to commute around Cambridge and/or to adjacent cities—what better way to combat climate change than to reduce the number of single-occupant vehicles on the road?

Not only will this building provide 49 homes for families near public transit at reduced rents, but it will improve the safety of the intersection. It is no secret that the corner of Walden and Massachusetts Avenue is difficult to navigate due to the narrow width. The project proposes to provide several feet of their private land for public sidewalk in order to widen the two narrow lanes on Walden by over three feet and widening the sidewalk by approximately one foot. We experience this intersection as cyclists, drivers, and pedestrians almost every day and we are delighted the Developer is going to improve safety here for the entire community.

The Developer has worked diligently to address issues raised by some concerned neighbors and has a strong track record in Cambridge building great buildings that enhance our streets and provide much needed housing to our residents. Cambridge is a city striving to fight climate change and provide more housing on so many fronts. What better way than to approve well-designed building close to excellent transit that will enhance the built environment?

Please support this effort.

Sincerely,
Noah Maslan & Jocelyn Kasper, Owners/Residents of 194 Walden Street

Pacheco, Maria

From: Seymour Kellerman <seymourkellerman@gmail.com>
Sent: Monday, December 7, 2020 4:12 PM
To: Pacheco, Maria; ranjits@cambridgema.go; O'Grady, Sean; Daglian, Sisia
Subject: CASE NO. BZA-017326-2020 - 2072 Mass Ave Comprehensive Permit Application
Attachments: 2072 Mass Ave Safety Petition Signed.pdf

To: Cambridge Board of Zoning Appeal
From: North Walden Neighbors
Date: December 7, 2020
Re: Petition regarding Capstone Communities application for Comprehensive Permit

To date, 228 Cambridge residents have signed the attached petition. Many of the signers live in close proximity to the intersection of Mass Ave and Walden St. Signatures keep coming in.

Please see the petition for our recommendations with regard to the Comprehensive Permit application by Capstone Communities.

Respectfully,
Seymour Kellerman for North Walden Neighbors
21 Cogswell Ave Cambridge 02140
617.833.3963

PETITION TO IMPROVE SAFETY AND REDUCE DENSITY OF 2072 MASS. AVE. PROPOSED DEVELOPMENT

To the City of Cambridge Board of Zoning Appeals:

We, the undersigned, Cambridge residents many of whom live in close proximity to the intersection of Mass. Ave. and Walden St., have grave concerns regarding the proposed 100% affordable housing development at **2072 Mass. Ave.**

The building (9 stories facing Mass. Ave. and 6 stories facing the Walden neighborhood) has 49 units for up to 200 residents, a storefront, 3 restricted onsite parking spaces and 2 drop off spaces. The Floor Area Ratio (FAR) of the proposed development is 6.7, too dense for this neighborhood. By comparison, it is 315% higher than the average FAR of 2.1 for the other 4 affordable housing developments within a ½ mile radius of Porter Square.

The proposed development sits on a small 8,514 square foot lot, located on a dangerous, congested corner that poses safety issues for pedestrians, bicyclists, and drivers. There is a history of accidents at the intersection and even a tragic death of a young girl on a bicycle. The current proposal would likely exacerbate these long-standing problems as the building takes up the entire lot, without any setback or ground greenspace.

The developer's application to the City asks to waive 18 separate zoning and other regulations, many of which if waived would make the intersection even more dangerous. The developer's request is well beyond the recently passed Affordable Housing Overlay's maximums that were discussed over a two-year period.

As neighbors who will be directly impacted by this proposed development, we are asking the City of Cambridge to:

- Conduct a Traffic Impact Study to analyze the impact of the proposed development on the Mass. Ave./Walden St. intersection as well as the surrounding streets (using pre-pandemic conditions);
- Instruct the developers to resize the building in accordance with the Mass. Ave. Overlay and with the Affordable Housing Overlay guidelines on building size;
- Enforce the safety- and space-related zoning regulations.

NAME	ADDRESS	Date Signed
Mark Adams	2517 Mass Ave, Cambridge 02140	12/7/20
Eva Alpert, CPA	28 Regent St, Cambridge 02140	12/7/20
Helle Alpert, CPA	56 Winslow St #2, Cambridge 02140	12/7/20
Patricia Armstrong	36 Orchard St, Cambridge 02140	12/7/20
Silvia Marina Arrom	4 Shady Hill Square, Cambridge 02138	12/6/20
Madeleine Aster	67 Rice St, Cambridge 02140	12/7/20
Kristine H. Atkinson	98 Clifton St, Cambridge 02140	12/4/20
Asunción del Azar	700 Huron Ave, Cambridge 02138	12/3/20
Thomas Barfield	51 Chilton St. Cambridge 02138	12/6/20
Sylvia Barnes	196 Harvey St, Cambridge 02140	12/3/20
Trumbull Barrett	11 King St, Cambridge 02140	11/30/20
Elizabeth Bartle	45 Bellevue Ave, Cambridge 02140	12/6/20
Theodore C. Bestor	149 Upland Road, Cambridge 02140	12/2/20
Lynn Betlock	146 Rindge Ave, Cambridge 02140	12/7/20
Nick Blaisdell	16 Cogswell Ave, Cambridge 02140	12/7/20
William Bloomstein	16 Crescent St, Cambridge 02138	12/1/20
Philip Bodrock	10 Goodman Road, Cambridge 02139	12/1/20
Jaryna Bodrock	10 Goodman Road, Cambridge 02139	12/1/20
David Boufford	104 Jackson St, Cambridge 02140	12/7/20
Sari Boren	189 Walden St, Cambridge 02140	11/29/20
Ezekial Bowman	7 Cypress St, Cambridge 02140	11/30/20
Michael Brandon	27 Seven Pines Ave, Cambridge 02140	11/26/20

Paul Brennan	77 Tremont St, Cambridge 02139	12/6/20
Corlane Brewington	1. 2050 Mass Ave #307, Cambridge 02140	11/30/20
Tom Brewitt	40 Cogswell Ave, Cambridge 02140	11/27/20
Nancy Brickhouse	113 Walden Str, Cambridge 02140	11/29/20
Cy Britt	2 Shady Hill Square, Cambridge 02138	12/6/20
Michael Byrne	77 Kirkland St, Cambridge 02138	12/7/20
Robert Camacho	24 Corporal Burns Rd, Cambridge 02138	12/7/20
Ayesha Cammaerts	31 Wendell St, Cambridge 02138	12/7/20
Winthrop Carty	20 Cogswell Ave, Cambridge 02140	12/7/20
Roberta Caudill	2050 Mass Ave #408, Cambridge 02140	11/29/20
Davis Chaves, Jr.	44 Creighton St, Cambridge 02140	12/6/20
Lisa Ceremsak	8 Cypress Street, Cambridge 02140	11/29/20
Mary Chaves	29 Creighton St, Cambridge 02140	12/2/20
Nick Chouairi	19 Creighton St, Cambridge 02140	12/2/20
Susan Ciccone	14 Creighton St, Cambridge 02140	12/6/20
Cheryl Clifford	64 Clifton Street, Cambridge 02140	12/7/20
Kevin Clifford	64 Clifton Street, Cambridge 02140	12/7/20
Judy Clark	81 Orchard St, Cambridge 02140	12/7/20
Carol Colsell	35 Wendell St #2, Cambridge 02138	12/7/20
Brian Cook	14 Creighton St, Cambridge 02140	12/7/20
Jan Corash	84 Orchard St, Cambridge 02140	12/7/20
Stephanie Crayton	64 Matignon Rd, Cambridge 02140	12/7/20

Dewey Dellay	9 King St, Cambridge 02140	12/3/20
Steven Dickman	48 Mount Pleasant St, Cambridge 02140	11/28/20
Thomas Dinwoodie	57 Francis Ave, Cambridge 02140	12/7/20
Hillary Dorsk	1 Russell St, Cambridge 02140	12/7/20
Lisa Dreier	38 Cogswell Ave, Cambridge 02140	11/27/20
Costanza Eggers	47 Porter Road, Cambridge 02140	11/27/20
Sandra Fairbank	221 Mt. Auburn St #705, Cambridge 02138	12/6/20
Mitzi Fennell	37 Creighton St, Cambridge 02140	12/7/20
Pauline Fennell	35 Creighton St, Cambridge 02140	12/2/20
Ann Ferraro	35 Walden St, Cambridge 02140	12/4/20
Lois W. Fine	8 Sycamore Street, Cambridge 02140	11/29/20
JaneAnn Fisher	16 Creighton St, Cambridge 02140	12/7/20
Monique Fischer	47-19 Cogswell Ave, Cambridge 02140	11/30/20
Tony Flamand	23 Cogswell Ave, Cambridge 02140	11/26/20
Yael Flamand	23 Cogswell Ave, Cambridge 02140	11/26/20
Lawrence W Flint	49 Walker St, Cambridge 02140	11/29/20
Theo Forbath	21 Frost St, Cambridge 02140	12/7/20
Annmarie Flynn	341 Walden St, Cambridge 02138	11/28/20
Susan Frankle	3 Houston Park, Cambridge 02140	11/27/20
A. Lindsay Frazier MD	Harvard Medical School	12/4/20
Marie Gannon	15 Richdale Ave, Cambridge 02140	12/7/20
Ann Gantz	47 Pemberton St #3, Cambridge 02140	11/29/20

Cheryl Gault	47 Creighton St, Cambridge 02140	12/7/20
Betsey Germanotta	175 Harvey St. #2, Cambridge 02140	12/7/20
Douglas Gessler	16 Bigelow St, Cambridge 02139	12/6/20
Antoinette Gilligan	24 Pemberton St, Cambridge 02140	12/7/20
Michael Gilligan	24 Pemberton St, Cambridge 02140	12/7/20
Jean B. Gleason, Ph.D.	110 Larchwood Dr, Cambridge 02138	12/5/20
Merav Gold	7 Shady Hill Square, Cambridge 02138	11/6/20
Zachary Goldberg	118 Aberdeen Ave, Cambridge 02138	11/30/20
Byron Good	77 Raymond St, Cambridge 02140	12/6/20
Mary-Jo D. Good	77 Raymond St, Cambridge 02140	12/6/20
Nellie Goodwin	23 Mead Street, Cambridge 02140	11/27/20
Kristen Graves	28 Creighton St, Cambridge 02140	12/7/20
Paul Griffin	99 Rindge Ave, Cambridge 02140	12/7/20
Scott Haas	27 Gibson St, Cambridge 02138	12/6/20
Bonnie Haddad	175 Richdale Ave, Cambridge 02140	12/1/20
Sarah E. Hall	1 Russell St #101, Cambridge 02140	12/7/20
Ann B Hannum	9 Walden Mews, Cambridge 02140	11/29/20
Hurst Hannum	9 Walden Mews, Cambridge 02140	11/29/20
Helen Hardacre	1 Walden Mews, Cambridge 02140	11/29/20
Catherine Hayner	2050 Mass Ave #406, Cambridge 02140	11/29/20
Alice Heller	22 Corporal Burns Rd, Cambridge 02138	12/7/20
Jean Hermann	9 Sycamore St, Cambridge 02140	11/26/20

Mark Hessler	47 Cogswell Ave #24, Cambridge 02140	11/28/20
Marjorie Hilton	141 Upland Rd, Cambridge 02140	12/7/20
D. Hives	54 Creighton St, Cambridge 02140	12/6/20
Cathy Korsgren	10 Hollis St, Cambridge 02140	12/7/20
Prof. Gerald Holton	64 Francis Ave, Cambridge 02138	12/6/20
Maria Hottelet	17 Walden St, Cambridge 02140	11/29/20
Sue Howard	111 Walden St, Cambridge 02140	11/29/20
Jean Jackson	52 Dana St, Cambridge 02138	12/2/20
Deborah Jancourtz	41 Fresh Pond Place, Cambridge 02138	12/3/20
Anna Jeffers	37 Creighton St, Cambridge 02140	12/7/20
Douglas Jeffers	37 Creighton St, Cambridge 02140	12/7/20
Chris Jeffrey	29 Chauncy St #3, Cambridge 02138	12/3/20
Eleanor Jewett	85 Walden St, Cambridge 02140	12/6/20
Lois Josimovich	32 Loomis St #1, Cambridge 02138	11/29/20
Billie Jo Joy	2 Sherman St #3, Cambridge 02138	12/6/20
Emily Kahn-Boesel	53 Creighton St, Cambridge 02140	12/6/20
Jane Kamine	5 Scott St, Cambridge 02138	12/7/20
Peter Katz	41 Creighton St, Cambridge 02140	12/7/20
Ailish Keating	41 Creighton St, Cambridge 02140	12/2/20
Mekonnen Kebede	14 Walden St, Cambridge 02140	11/28/20
Seymour Kellerman	21 Cogswell Ave, Cambridge 02140	11/26/20
Michael P. Kennedy	8B Cogswell Ave, Cambridge 02140	11/27/20

Elizabeth Kenney	33 Creighton St, Cambridge 02140	12/2/20
Crystal Komm	10 Walden Mews, Cambridge 02140	11/29/20
Elizabeth Kon	23 Creighton St, Cambridge 02140	12/4/20
Joan Krizack	79 Pemberton St, Cambridge 02140	12/7/20
Ausra Kubilius	21 Cogswell Ave, Cambridge 02140	11/26/20
Jonathan Lehrich	15 Cogswell Ave, Cambridge 02140	11/27/20
Fred Leventhal	25 Vassal Lane #2, Cambridge 02138	11/27/20
Jean L. Leventhal	25 Vassal Lane #2, Cambridge 02138	11/27/20
Joyce Levine	2353 Mass Ave #91, Cambridge 02140	12/7/20
Ilan Levy	148 Spring St, Cambridge 02141	12/7/20
Kris Ellis-Levy	148 Spring St, Cambridge 02141	12/7/20
Dennis Like	5 Walden St #3, Cambridge 02140	11/27/20
Stace Lindsay	8 Cypress Street, Cambridge 02140	11/29/20
John Malmstad	8A Cogswell Ave, Cambridge 02140	11/27/20
Gabriel Malseptic	31 Wendell St, Cambridge 02138	12/7/20
Marcelo Marchetti	7 Walden Mews, Cambridge 02140	11/29/20
Jean Ann Martin	13 Leonard Ave #2, Cambridge 02139	12/5/20
Chris Matthews	43 Creighton St, Cambridge 02140	12/2/20
Stephen McCabe	1 Russell St, Cambridge 02140	12/7/20
Elizabeth McCann	28 Clarendon Ave, Cambridge 02140	12/7/20
Peter McCann	28 Clarendon Ave, Cambridge 02140	12/7/20
Cathleen McCormick	9 King St, Cambridge 02140	11/30/20

Hugh McManus	17 Creighton St, Cambridge 02140	12/2/20
John McVey	47 Vassal Lane #2, Cambridge 02138	11/26/20
Kuniko McVey	47 Vassal Lane #2, Cambridge 02138	11/26/20
James Mercer	51 Cogswell Ave, Cambridge 02140	11/27/20
Diana Meservey	57 Francis Ave, Cambridge 02138	12/7/20
Lia Monahan	11 King St, Cambridge 02140	11/30/20
Mark Morley	1 Russell St #400, Cambridge 02140	12/7/20
Alejandra Morterini	20 Cogswell Ave, Cambridge 02140	12/7/20
Christie Morrison	15 Creighton St, Cambridge 02140	12/2/20
Christopher Morse	26 Creighton St, Cambridge 02140	12/7/20
Judith Motzkin	307 Pearl St, Cambridge 02139	12/2/20
Mariette Murphy	24 Walden St, Cambridge 02140	11/26/20
Lorraine C. Murphy	11 Cypress St, Cambridge 02140	11/27/20
Francis Murphy	11 Cypress St, Cambridge 02140	11/29/20
April Nadeau	2050 Mass Ave #311, Cambridge 02140	11/30/20
John B. Nelson	175 Richdale Ave #102, Cambridge 02140	12/1/20
Jero Nesson	1 Russell St #305, Cambridge 02140	12/7/20
Joe O'Boyle	24 Walden St, Cambridge 02140	11/26/20
Marisa O'Boyle	24 Walden St, Cambridge 02140	11/26/20
Jacqueline O'Brien	11 Walden Mews, Cambridge 02140	11/29/20
Katherine O'Brien	11 Walden Mews, Cambridge 02140	11/29/20
Richard P. O'Brien	11 Walden Mews, Cambridge 02140	11/29/20

Joshua Orr	3 Chetwynd Road, Cambridge 02140	11/29/20
Tracy Orr	3 Chetwynd Road, Cambridge 02140	11/29/20
Janet Patterson	1 Russell St #100, Cambridge 02140	12/7/20
Diane B. Paul	1716 Cambridge St #17, Cambridge 02138	12/6/20
Elizabeth J. Perry	119-B Huron Ave, Cambridge 02138	12/6/20
Matt Pesci	47 Cogswell Ave #20, Cambridge 02140	11/27/20
Deborah Peterson	2050 Mass Ave, Cambridge 02140	11/28/20
Megan Postal	25 Hubbard Ave, Cambridge 02140	11/29/20
Christopher Potter	10 Walden Mews, Cambridge 02140	11/29/20
Barbara S. Powell	18 Scott St, Cambridge 02138	12/6/20
Julia S. Powell	18 Scott St, Cambridge 02138	12/6/20
Phyllis Pownall	17 Rindgefield St, Cambridge 02140	12/7/20
Lucie Prinz	31 Creighton St, Cambridge 02140	12/7/20
Gus Rancatore	18 Amory Street, Cambridge 02139	11/26/20
Warren Rhodes	217 Thorndike St, Cambridge 02141	12/6/20
Adalicia Roth	2 Warwick Pk, Cambridge 02140	12/7/20
Robert Roth	2 Warwick Pk, Cambridge 02140	12/7/20
Margaret Rueter	2050 Mass Ave #210, Cambridge 02140	11/27/20
Alison Sanders-Fleming	77 Pemberton St, Cambridge 02140	11/27/20
F. Duncan Sanders-Fleming	77 Pemberton St, Cambridge 02140	12/1/20
Luisa San Juan	7 Walden Mews, Cambridge 02140	11/29/20
Max Schenkman	14 Creighton St, Cambridge 02140	12/6/20

Thomas A. Scialdone	2050 Mass Ave #303, Cambridge 02140	11/29/20
Elizabeth Scott	2050 Mass Ave, Cambridge 02140	11/28/20
Dana Schaefer	47 Cogswell Ave #20, Cambridge 02140	11/27/20
Harry Shapiro	41 Orchard St, Cambridge 02140	11/30/20
Guillemette Simmers	8 Alpine St, Cambridge 02138	12/7/20
Sarah Slaughter	11 Stearns St Cambridge 02138	12/7/20
Daniel Smith	45 Regent St, Cambridge 02140	12/6/20
Jamal Smith	2050 Mass Ave #508, Cambridge 02140	11/28/20
Lou Soltys	26 Creighton St, Cambridge 02140	12/6/20
Elaine Soo Hoo	45 Regent St, Cambridge 02140	12/6/20
Jeffrey Spenser	22 Blake St, Cambridge 02140	12/7/20
Marcia Stein	19 Walden St #1, Cambridge 02140	12/5/20
Sharon Stichter	108 Walden St, Cambridge 02140	12/3/20
Fang Shen	25 Cogswell Ave, Cambridge 02140	11/2/20
Adam Simha	84 Rice St, Cambridge 02140	12/6/20
Ovadia R. Simha	84 Rice St, Cambridge 02140	12/6/20
Susan Sklan	109 Jackson St #1, Cambridge 02140	12/7/20
Lou Soltys	26 Creighton St, Cambridge 02140	12/7/20
Linda Stein	1 Walden Mews, Cambridge 02140	11/29/20
Arielle Stanford	12 Sycamore St, Cambridge 02140	11/28/20
Anna Stothart	25 Wood St, Cambridge 02140	12/7/20
Ronald Suleski	32 Clarendon Ave, Cambridge 02140	12/7/20

Dan Sullivan	12 Milton St, Cambridge 02140	12/7/20
Stefan Tassoulas	43 Creighton St, Cambridge 02140	12/6/20
Brian Tavares	1 Russel St, Cambridge 02140	12/7/20
Lahra Tillman	150 Dudley St, Cambridge 02140	12/7/20
Lein Tung	28 Creighton St, Cambridge 02140	12/7/20
Donna Tutein	2050 Mass Ave #409, Cambridge 02140	11/29/20
John Uzzolino	3 Houston Park, Cambridge 02140	11/27/20
Deborah Valenze	1 Shady Hill Square, Cambridge 02138	12/6/20
Elisabeth VanderWeele	9 Ellery Square, Cambridge 02138	12/4/20
Kevin Yearwood	15 Cameron Ave, Cambridge 02140	12/7/20
Ann Warner	21 Grozier Rd, Cambridge 02138	12/7/20
Dan Lynn Watt	175 Richdale Ave #315, Cambridge 02140	11/30/20
Molly Lynn Watt	175 Richdale Ave #315, Cambridge 02140	12/1/20
Jennifer Webb	64 Clifton St, Cambridge 02140	12/7/20
Chuck Weed	109 Avon Hill Street, Cambridge 02140	12/6/20
Louise Weed	109 Avon Hill Street, Cambridge 02140	12/6/20
Peter Weiler	606 Huron Ave, Cambridge 02138	12/7/20
Susan Wellington	58 Sacramento St, Cambridge 02138	12/4/20
Gatewood West	63 Creighton St, Cambridge 02140	12/6/20
Merry White	6 Cypress St, Cambridge 02140	11/26/20
Ellen Widmer	120 Fayerweather St #1, Cambridge 02138	12/6/20
Matthew Widmer	120 Fayerweather St #1, Cambridge 02138	12/6/20

James M. Williamson	1000 Jackson Place, Cambridge 02140	12/7/20
Pamela Winters	41 Orchard St, Cambridge 02140	11/30/20
Jean True Woodward	49 Walker St, Cambridge 02138	11/29/20
Benjamin Aldes Wurgaft	6 Cypress St, Cambridge 02140	11/29/20
Lewis Wurgaft	35 Wendell St #2, Cambridge 02140	12/6/20
Charles M. Wyzanski	75 Francis Ave, Cambridge 02138	12/6/20
Seth Yarden	164 Vassal Lane, Cambridge 02138	11/29/20
Le Zou	25 Cogswell Ave, Cambridge 02140	11/27/20

Electronic and printed signatures are available on request.



City of Cambridge

MASSACHUSETTS

BOARD OF ZONING APPEAL

831 Mass Avenue, Cambridge, MA.
(617) 349-6100

2 Bdz

BZA

POSTING NOTICE – PICK UP SHEET

The undersigned picked up the notice board for the Board of Zoning Appeals Hearing.

Name: Mariano Dewington-Hope (Print) Date: 11/23/2020

Address: 2072 Mass Ave

Case No. BZA-017326-2020

Hearing Date: 12/10/20

Thank you,
Bza Members



CAMBRIDGE HISTORICAL COMMISSION

831 Massachusetts Avenue, 2nd Fl., Cambridge, Massachusetts 02139

Telephone: 617 349 4683 TTY: 617 349 6112 Fax: 617-349-6165

E-mail: histcomm@cambridgema.gov URL: <http://www.cambridgema.gov/Historic>

Bruce A. Irving, *Chair*; Susannah Barton Tobin, *Vice Chair*; Charles Sullivan, *Executive Director*
Joseph V. Ferrara, Chandra Harrington, Elizabeth Lyster, Caroline Shannon, Jo M. Solet, *Members*
Gavin W. Kleespies, Paula A. Paris, Kyle Sheffield, *Alternates*

November 19, 2020

To: Maria Pacheco, Secretary
Board of Zoning Appeal

From: Charles Sullivan, Executive Director *CWS*
Cambridge Historical Commission

Re: BZA-017326-2020, 2072 Massachusetts Avenue

The Cambridge Historical Commission is in receipt of a Comprehensive Permit application for the above referenced property, which is proposed for redevelopment as a new multi-family affordable housing building with 49 residential rental units.

The existing building on this site is a former fast-food restaurant constructed in 1971 that has no historic or architectural significance. Historical Commission staff will sign off on the demolition permit application at the appropriate time. No public hearing will be necessary.

I also find that this project will have "no adverse effect" on the adjacent Henderson Carriage Building, which is listed on the National Register of Historic Places, or on any other designated historic resource in the vicinity.

cc: Jason Korb and Sean Hope, CC HRE 2072 Mass Ave Tenant LLC, c/o Capstone Communities LLC

Pacheco, Maria

From: Jae Storozum <jsstoroz@gmail.com>
Sent: Monday, November 30, 2020 11:37 AM
To: Pacheco, Maria
Cc: Clerk
Subject: Case BZA-017326-2020: Support Affordable Housing

Dear Board of Zoning Appeal members,

I support the affordable housing project at 2072 Mass Ave near Porter Square (Case number #BZA-017326-2020). I have lived in the Porter Square area for four years and have always wanted to see larger, denser housing near the T station in the square. The project will be close enough to the Porter T and along several major bus routes that any claims that the development will negatively affect parking and traffic in the neighborhood are baseless, rooted in irrational fear of change and willfully ignorant of the need for affordable housing in Cambridge.

I would like to remind the Board that many of the local buildings were actually originally built quite tall compared to their "neighborhood context", but were downsized during the Great Depression to avoid property taxes and then down-zoned as a product of racist redlining policies in the mid-20th century. If the Board cares at all about housing justice, they should approve this project as soon as possible.

Thank you,

Jae Storozum
16 Whittier St.

Pacheco, Maria

From: steve bardige <sbardige@gmail.com>
Sent: Monday, November 30, 2020 1:28 PM
To: Pacheco, Maria
Subject: 2072 MasssAve. BZA-017326-2020

Cambridge Board of Zoning Appeals

Re: 2072 Massachusetts Ave, BZA-017326-2020

November 30, 2020

We support the application for a comprehensive permit to build new multi-family affordable housing located at 2072 Massachusetts Avenue Cambridge MA

My wife and I have lived in Cambridge, about half mile from the corner of Walden and Mass Ave. for 47 years, and we have walked and driven by that corner thousands of times.

The need for affordable housing, especially for families, in Cambridge is clear. Building affordable housing on Mass Ave, near bus and subway, makes sense. We applaud the developer for his innovative efforts to take some of his land on Walden and use it to widen Walden. While this in and of itself will not solve the long-standing traffic issues at the corner of Walden and Mass Ave, it will help. And moving the primary entrance from Walden to Mass Ave. was a big improvement.

We like the reduction to six stories in the back of the building to address some of the neighbors concerns. We also like the passive house attributes, and the green amenities on the roof.

Advocating increased density is not something We do lightly, but in this case, given the location on Mass Ave, the need for affordable housing, the proximity to public transportation, the efforts the developer has made to accommodate many of the neighborhood concerns. We support this application.

Kay Hurley and Stephen Bardige

55 Stearns Street

sbardige@gmail.com

617-230-0030

Pacheco, Maria

From: Ira Nichols-Barrer <nicholsbarrer@yahoo.com>
Sent: Monday, November 30, 2020 4:20 PM
To: Pacheco, Maria
Subject: support for BZA-017326-2020 (2072 Mass Ave)

Greetings,

I am writing to express my strong and heartfelt support for the proposed affordable housing development at 2072 Mass Ave. I believe our community is facing a housing affordability crisis, and allowing these much-needed homes to be built would help families in our community who need it most. Moving forward would be a wonderful idea. Since this site is on the busy Mass Ave. corridor, replaces an unsightly parking lot, and is very close to transit (and a grocery store), it appears to be an ideal location to allow more much-needed density.

I am a homeowner who lives around the corner from this site and walk by it daily, and I do not think any hypothetical concerns about access to on-street parking justify slowing down or downscaling the project. The developers have already reduced the scale of their original plan to add an attractive rear roof-deck that will create a 'step down' to nearby homes, and it would be a sad and shortsighted loss to see the project downscaled even further.

I hope you see fit to grant approval, and bring more desperately-needed homes to my neighborhood.

Sincerely,
Ira

Ira Nichols-Barrer
175 Richdale Ave., #105
Cambridge MA, 02140