



DETOUR PLAN

CONSTRUCTION SIGN LEGEND

| IDENTIFICATION NUMBER | SIZE OF SIGN | | TEXT | TEXT DIMENSIONS | | | COLOR | | |
|-----------------------|--------------|--------|-------------------------------------|-----------------|------------------|-------|-----------------------|-----------------------|--------|
| | WIDTH | HEIGHT | | LETTER HEIGHT | VERTICAL SPACING | ARROW | BACK-GROUND | LEGEND | BORDER |
| G20-2 | 36" | 18" | END ROAD WORK | | | | MUTCD STANDARD DETAIL | MUTCD STANDARD DETAIL | |
| D3-1 | 12" | 24" | ACCESS TO VASSAL LANE CLOSED | | | | | | |
| D3-3 | 12" | 24" | BIKE LANE CLOSED MERGE WITH TRAFFIC | | | | | | |
| D3-4 | 12" | 24" | DETOUR TO FRESH POND PARKWAY | | | | | | |
| D3-5 | 12" | 18" | LOCAL TRAFFIC ONLY | | | | | | |
| M4-9L | 30" | 24" | DETOUR | | | | | | |
| M4-9R | 30" | 24" | DETOUR | | | | | | |
| M4-9V | 30" | 24" | DETOUR | | | | | | |
| M4-8A | 24" | 18" | END DETOUR | | | | | | |
| R2-10a | 48" | 36" | WORK ZONE SPEEDING FINES DOUBLED | | | | | | |
| R4-11 | 30" | 30" | MAY USE FULL LANE | | | | | | |
| R5-9L | 30" | 24" | PEDESTRIANS | | | | | | |
| R5-9R | 30" | 24" | PEDESTRIANS | | | | | | |
| R9-9 | 48" | 24" | SIDEWALK CLOSED | | | | | WHITE/BLACK/BLUE | |
| R11-4 | 60" | 30" | ROAD CLOSED TO THRU TRAFFIC | | | | MUTCD STANDARD DETAIL | MUTCD STANDARD DETAIL | |

CONSTRUCTION SIGN LEGEND CONT.

| IDENTIFICATION NUMBER | SIZE OF SIGN | | TEXT | TEXT DIMENSIONS | | | COLOR | | |
|-----------------------|--------------|---------|------------------------------|-----------------|------------------|-------|-----------------------|-----------------------|--------|
| | WIDTH | HEIGHT | | LETTER HEIGHT | VERTICAL SPACING | ARROW | BACK-GROUND | LEGEND | BORDER |
| | 36" | 36" | CAUTION PAVED CASTINGS AHEAD | | | | MUTCD STANDARD DETAIL | MUTCD STANDARD DETAIL | |
| W1-4L | 30" | 30" | | | | | | | |
| W1-4R | 30" | 30" | | | | | | | |
| W4-2L | 36" | 36" | | | | | | | |
| W4-2R | 36" | 36" | | | | | | | |
| W5-1 | 48" | 48" | ROAD NARROWING | | | | | | |
| W6-1 | 36" | 36" | | | | | | | |
| W6-3 | 48" | 48" | | | | | | | |
| W6-5 | 24" | 18" | TWO WAY TRAFFIC | | | | | | |
| W8-8 | 36" | 36" | ROUGH ROAD | | | | | | |
| W11-1/W16-1 | 24"/18" | 24"/24" | SHOW THE ROAD | | | | | | |
| W11-2 | 36" | 36" | | | | | | | |
| W13-1 | 24" | 24" | XX MPH | | | | | | |
| W20-2 | 48" | 48" | DETOUR AHEAD | | | | | | |
| W20-4 | 48" | 48" | ONE LANE ROAD AHEAD | | | | | | |
| W20-7b | 36" | 36" | BE PREPARED TO STOP | | | | | | |
| W20-8 | 36" | 36" | POLICE OFFICER AHEAD | | | | | | |
| W20-1a | 48" | 48" | ROAD WORK AHEAD | | | | | | |
| W20-1b | 48" | 48" | ROAD WORK 500 FT | | | | | | |
| W20-1c | 48" | 48" | ROAD WORK 1000 FT | | | | | | |
| W20-1d | 48" | 48" | STEEL PLATES 100 FT | | | | MUTCD STANDARD DETAIL | MUTCD STANDARD DETAIL | |

GENERAL NOTES

- ALL CONSTRUCTION SIGNING, DRUMS, BARRICADES AND OTHER DEVICES SHALL CONFORM WITH THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (M.U.T.C.D.) AS AMENDED.
- ALL DRUMS SHALL BE SET @ 20' O.C. MAX. UNLESS OTHERWISE NOTED OR ADJUSTED BY THE ENGINEER.
- ALL DRUMS SHALL BE APPROXIMATELY PLACED AND MOVED AS NECESSARY TO MAINTAIN ADEQUATE ABUTTER ACCESS AT ALL TIMES. WORK MAY REQUIRE ADDITIONAL SIGNS, DRUMS AND OTHER TRAFFIC CONTROL DEVICES, GRADING AND TEMPORARY PAVEMENT FOR PASSAGE OF PEDESTRIAN, VEHICULAR AND EMERGENCY TRAFFIC THROUGH THE WORK AREAS, BOTH DURING AND AFTER WORKING HOURS, TO MAINTAIN SUCH ACCESS. THESE SHALL BE PROVIDED AT NO ADDITIONAL COST TO THE OWNER.
- THE CONTRACTOR SHALL NOTIFY EACH ABUTTER AT LEAST 24 HOURS IN ADVANCE OF THE START OF ANY WORK THAT WILL REQUIRE THE TEMPORARY CLOSURE OF ACCESS, SUCH AS EXISTING PAVEMENT EXCAVATION, TEMPORARY DRIVEWAY PAVEMENT PLACEMENT, PARKING RESTRICTIONS, AND SIMILAR OPERATIONS.
- DURING WORK IN INTERSECTIONS A MINIMUM OF ONE LANE OF ALTERNATING TRAFFIC SHALL BE ALLOWED TO PASS AT ALL TIMES. PLEASE REFER TO DETAIL ON THIS SHEET.
- GRADE SEPARATIONS IN EXCESS OF 2" DURING NON-WORKING HOURS WILL REQUIRE DELINEATION BY USE OF DRUMS.
- EXCAVATION EDGES IN EXCESS OF 4" DEEP SHALL BE PROTECTED DURING NON-WORKING HOURS BY BACKFILLING WITH A WEDGE OF GRAVEL OR SOIL TO COMPACTED 4:1 SLOPE.
- 11' MINIMUM LANE WIDTHS SHALL BE MAINTAINED AT ALL TIMES.
- EXCAVATIONS SHALL BE PROTECTED DURING NON WORK HOURS BY STEEL PLATES PINNED WITH EDGES PAVED OR BARRICADES AS APPROVED BY THE ENGINEER.
- ADVISORY SPEED PLATES (W13-1) SHALL BE USED IF APPROPRIATE AND AS REQUIRED BY THE ENGINEER.
- ALL SIGN LOCATIONS ARE SHOWN SCHEMATICALLY. FINAL LOCATIONS SHALL BE DETERMINED BASED ON ACTUAL FIELD CONDITIONS AND ENGINEER APPROVAL. PERMISSION AND RELATED PERMITS FOR THE LOCATION OF SIGNS AND VMB'S (VARIABLE MESSAGE BOARDS) ARE THE CONTRACTOR'S RESPONSIBILITY.
- THE CONTRACTOR IS REQUIRED TO USE POLICE DETAILS TO MAINTAIN SAFETY OF PEDESTRIAN AND VEHICULAR TRAFFIC.
- THE CONTRACTOR SHALL COVER OR REMOVE SIGNAGE AT THE END OF WORK HOURS OR WORK PHASES AS APPROPRIATE.
- WORK HOURS ARE FROM 7 AM TO 4 PM.
- THE CONTRACTOR SHALL PROTECT ALL TRAVEL WAYS FROM DUST AND CONSTRUCTION DEBRIS AT ALL TIMES.
- CONSTRUCTION SIGNING SHALL BE ORANGE BACKGROUND WITH BLACK LEGEND AND BLACK BORDER UNLESS OTHERWISE NOTED.
- TRAFFIC MANAGEMENT PLANS SHOW MINIMUM REQUIREMENTS. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL TRAFFIC SAFETY.
- PARKING SHALL BE RESTRICTED WITHIN WORK ZONES, AND BUFFER AND TAPER LENGTHS CONTRACTOR TO COORDINATE NO PARKING RESTRICTIONS WITH THE CITY OF CAMBRIDGE.

GENERAL TRAFFIC NOTES

- THE FOLLOWING SUGGESTED TRAFFIC MANAGEMENT AND CONSTRUCTION PHASING PLAN IS INTENDED TO BE A GUIDE ONLY. THE CONTRACTOR SHALL SUBMIT A DETAILED PROPOSED TRAFFIC MANAGEMENT AND CONSTRUCTION PHASING PLAN AT LEAST 30 DAYS PRIOR TO COMMENCEMENT OF CONSTRUCTION ACTIVITIES ON CONCORD AVENUE.
- ALL WORK ALONG CONCORD AVENUE SHALL BE COORDINATED WITH CITY AND OTHER CITY CONTRACTORS TO MINIMIZE TRAFFIC DISRUPTION TO TOBIN SCHOOL AND AREA RESIDENTS.
- ALL WORK IN HYDRAULIC MODIFICATIONS AREAS III AND IV SHALL BE COORDINATED WITH CITY AND OTHER CITY CONTRACTORS TO MINIMIZE TRAFFIC DISRUPTIONS TO TOBIN SCHOOL.

CONFORMED SET



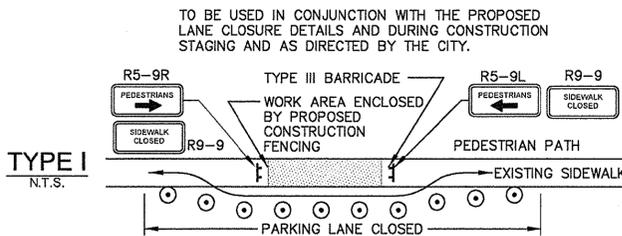
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| Job No. | 20120256.001A | | | |
| Designed by | KWG | | | |
| Drawn by | NFT | | | |
| Checked by | AML | No. | Description | Date |
| Approved by | EO | | REVISIONS | |



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| Client | CITY OF CAMBRIDGE, MASSACHUSETTS |
| Project | CONCORD (CONTRACT 9) SEWER SEPARATION AND SURFACE IMPROVEMENTS PROJECT |
| Drawing | TRAFFIC GENERAL GENERAL NOTES, DETOUR PLAN AND CONSTR. SIGN LEGEND |

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| Sheet | TG-1 |
| File No. | |

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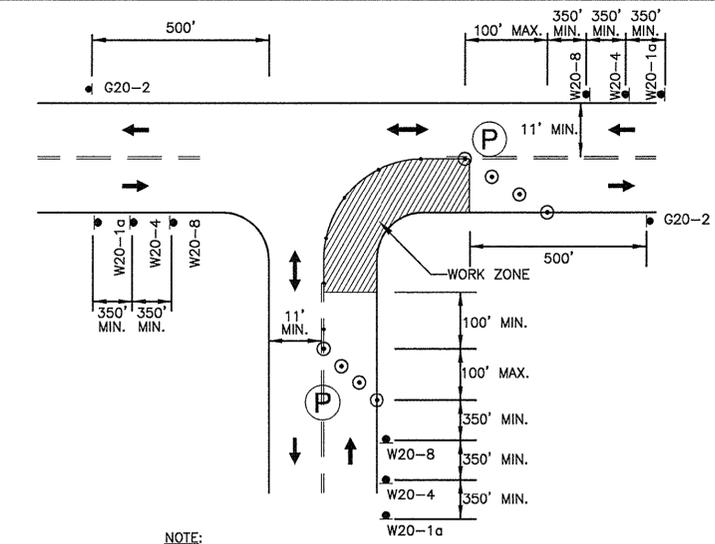


- PEDESTRIAN BYPASS NOTES:**
- ADDITIONAL ADVANCE WARNING MAY BE REQUIRED BY THE CITY. DIRECTION OF PEDESTRIAN TRAVEL.
 - CONTROLS FOR PEDESTRIAN TRAFFIC ONLY ARE SHOWN. VEHICULAR TRAFFIC SHALL BE MAINTAINED AS DETAILED ELSEWHERE.
 - STREET LIGHTING SHALL BE CONSIDERED WHEN LOCATING CONTROL DEVICES.
 - EXISTING PEDESTRIAN RAMPS SHALL BE CONSIDERED WHEN LOCATING TEMPORARILY PAINTED CROSSWALKS. TEMPORARILY BITUMINOUS RAMPS ARE REQUIRED FOR TEMPORARILY RELOCATED CROSSWALKS.
 - DIRECTION OF PEDESTRIAN TRAVEL
 - IF THE WORK ZONE DOES NOT PERMIT PEDESTRIANS TO TRAVEL ADJACENT TO IT AS SHOWN IN PEDESTRIAN BYPASS TYPE I, TEMPORARY CROSSWALKS WITH APPROPRIATE SIGNS SHALL BE INSTALLED TO CROSS PEDESTRIANS TO THE OPPOSITE SIDE OF THE STREET AS SHOWN IN PEDESTRIAN BYPASS TYPE II, AND AS REQUIRED BY THE ENGINEER.
 - TEMPORARY ASPHALT WHEEL CHAIR RAMPS ARE REQUIRED FOR TEMPORARILY RELOCATED CROSS WALKS WHERE EXISTING WHEEL CHAIR RAMPS DO NOT EXIST.

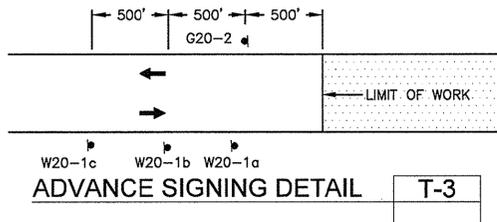
PEDESTRIAN BYPASS DETAIL T-1

TMP LEGEND

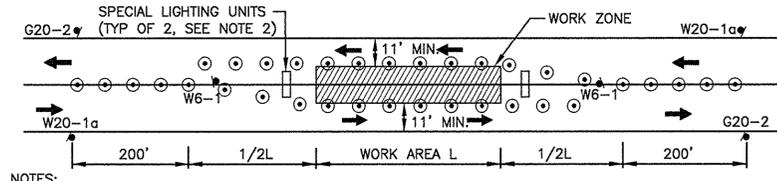
| | |
|---|--|
| PROPOSED WORK AREA | |
| SINGLE LANE UNIDIRECTIONAL TRAFFIC | |
| SINGLE LANE BIDIRECTIONAL TRAFFIC | |
| TRAFFIC BARREL/DRUM | |
| CONE | |
| LANE DELINEATOR | |
| TRAFFIC MANAGEMENT SIGN DURING CONSTRUCTION | |
| POLICE DETAIL | |



ONE LANE B-DIRECTIONAL TRAFFIC AT INTERSECTIONS T-2

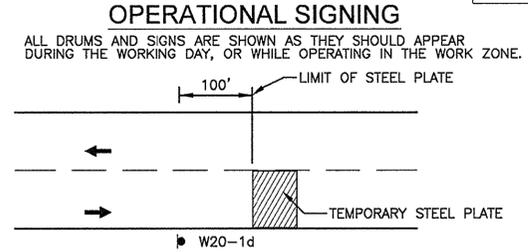


ADVANCE SIGNING DETAIL T-3

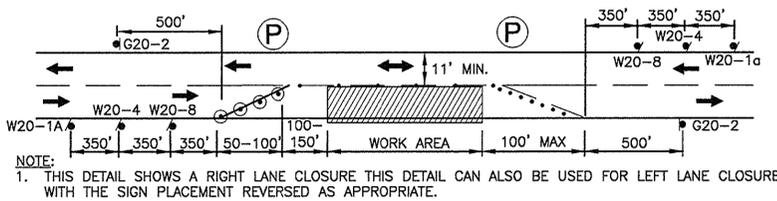


- NOTES:**
- L=AREA OF 1/2 DAY OF WORK (PER MUTCD MANUAL)
 - SPECIAL LIGHTING UNITS (ARROW BOARDS) TO BE ADDED ON BOTH SIDES OF WORKZONE WHERE SPEED LIMIT MEETS OR EXCEEDS 35 M.P.H.

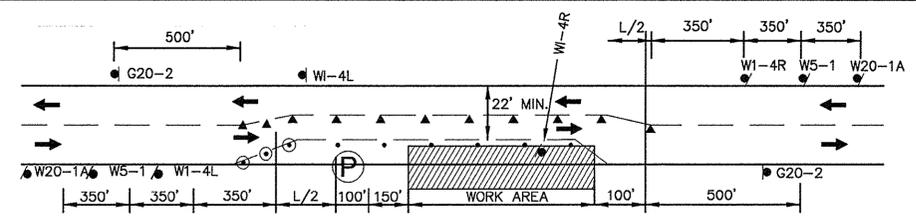
TYPICAL INTERIOR LANE CLOSURE T-4



TRENCH PLATE SIGNING DETAIL T-5

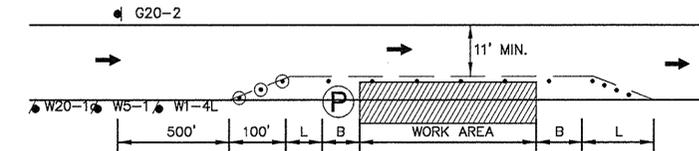


TYPICAL TWO-WAY ALTERNATING TRAFFIC T-6



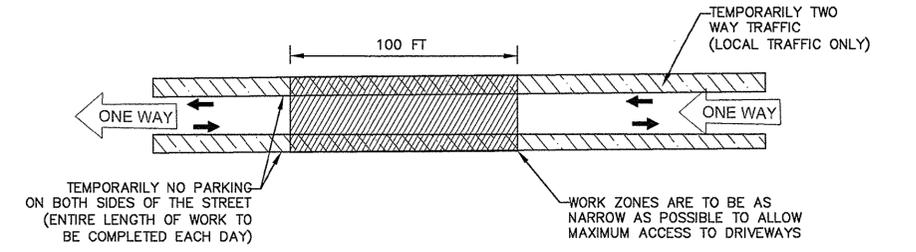
TYPICAL TWO-WAY STREET LANE SHIFT T-7

- NOTE:**
- THIS DETAIL SHOWS A RIGHT LANE CLOSURE. THIS DETAIL CAN ALSO BE USED FOR LEFT LANE CLOSURES, WITH THE SIGN PLACEMENT REVERSED AS APPROPRIATE.
- L = (WS²)/60 FOR 40 MPH OR LESS W=WIDTH OF OFFSET (FT)
 L = WS FOR 45 MPH OR MORE S=POSTED SPEED LIMIT (MPH)

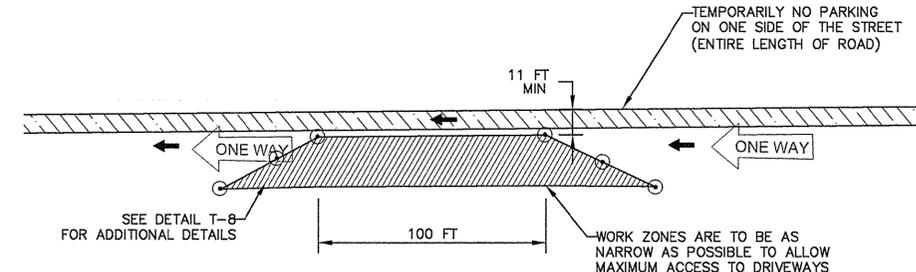


TYPICAL ONE-WAY STREET LANE SHIFT T-8

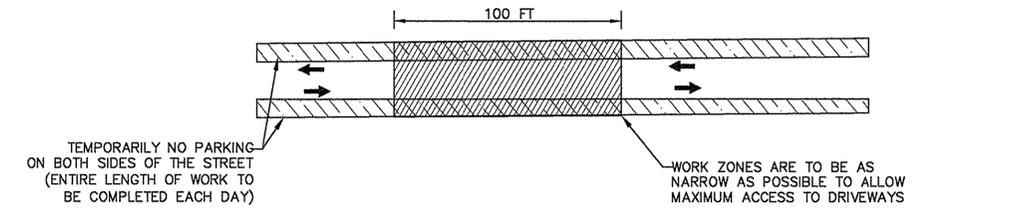
- NOTE:**
- THIS DETAIL SHOWS A RIGHT LANE CLOSURE. THIS DETAIL CAN ALSO BE USED FOR LEFT LANE CLOSURES, WITH THE SIGN PLACEMENT REVERSED AS APPROPRIATE.
- L = (WS²)/60 FOR 40 MPH OR LESS W=WIDTH OF OFFSET (FT)
 L = WS FOR 45 MPH OR MORE S=POSTED SPEED LIMIT (MPH)
 B= BUFFER ZONE



TYPICAL ONE-WAY TRAFFIC PLAN LESS THAN 11 FT BETWEEN CURBING AND EDGE OF WORK ZONE T-9



TYPICAL ONE-WAY TRAFFIC PLAN MORE THAN 11 FT BETWEEN CURBING AND EDGE OF WORK ZONE T-10



TYPICAL TWO-WAY TRAFFIC PLAN LESS THAN 11 FT BETWEEN CURBING AND EDGE OF WORK ZONE T-11

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| Client | CITY OF CAMBRIDGE, MASSACHUSETTS |
| Project | CONCORD (CONTRACT 9) SEWER SEPARATION AND SURFACE IMPROVEMENTS PROJECT |
| Drawing | TRAFFIC GENERAL TRAFFIC DETAILS |

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| Sheet | TG-2 |
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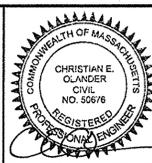
| TRAFFIC DETAIL LOCATIONS | | | | |
|-------------------------------------|----------------|--------------|-----------------------|----------------|
| STREET | TRAFFIC FLOW | FROM STA* | TO STA* | TYPICAL DETAIL |
| ALPINE ST | 1-WAY | 3500+50 | 3506+85 | T-9 |
| ALPINE ST | 1-WAY | 3507+20 | 3510+50 | T-10 |
| ALPINE ST | 1-WAY | 3510+50 | 3510+90 | T-9 |
| BAY STATE RD | 2-WAY | - | - | T-6 / T-7 |
| BIRCH ST | 1-WAY | - | - | T-8 |
| C. BURNS RD | 1-WAY | - | - | T-10 |
| CHILTON ST | 1-WAY | 1250+35 | 1250+90 | T-9 |
| CHILTON ST | 1-WAY | 1250+90 | 1253+05 | T-10 |
| CHILTON ST | 1-WAY | 1253+05 | 1253+55 | T-9 |
| CHILTON ST | 1-WAY | 1253+55 | 1255+65 | T-10 |
| CHILTON ST | 1-WAY | 1255+65 | 1256+10 | T-9 |
| CHILTON ST | 1-WAY | 1265+45 | 1259+35 | T-10 |
| CONCORD AVE | 2-WAY | - | - | T-2 / T-6 |
| COPLEY ST | 1-WAY | - | - | T-9 |
| FAYERWEATHER ST | 1-WAY | 3119+25 | 3125+35 | T-9 |
| FAYERWEATHER ST | 1-WAY | 1327+10 | 1327+30 | T-10 |
| FAYERWEATHER ST | 1-WAY | 1327+30 | 1329+10 | T-10 |
| FAYERWEATHER ST | 1-WAY | 1329+10 | 1329+42 | T-9 |
| FAYERWEATHER ST | 1-WAY | 1329+80 | 1334+20 | T-10 |
| FERN ST | 1-WAY | - | - | T-10 |
| FIELD ST | 2-WAY | 3107+50 | 3114+00 | T-6 |
| FIELD ST | 1-WAY | 3114+75 | 3119+25 | T-9 |
| GARDEN ST | 2-WAY | 3510+90 | 3512+00 | T-2 |
| GARDEN ST | 2-WAY | 3512+00 | 3514+75 | T-6 |
| GARDEN ST | 2-WAY | 3514+75 | 3515+70 | T-11 |
| GARDEN ST | 2-WAY | 3515+70 | 3517+00 | T-6 |
| GARDEN ST | 2-WAY | 3517+00 | 3518+00 | T-11 |
| GARDEN ST | 2-WAY | 3518+00 | 3519+90 | T-6 |
| GARDEN ST | 2-WAY | 3519+90 | 3521+20 | T-11 |
| GARDEN ST | 2-WAY | 3521+20 | 3523+55 | T-6 |
| GARDEN ST | 2-WAY | 3523+55 | 3524+20 | T-2 |
| HAZEL ST | 2-WAY | 4100+15 | 4100+60 | T-10 |
| HAZEL ST | 2-WAY | 4100+60 | 4101+90 | T-9 |
| HAZEL ST | 2-WAY | 4101+90 | 4102+45 | T-10 |
| HAZEL ST | 2-WAY | 4102+45 | 4102+85 | T-9 |
| HYDRAULIC MOD AREA I | 1-WAY ROTARY | CONCORD AVE | AT ALEWIFE BROOK PKWY | SEE TMP-3 |
| HYDRAULIC MOD AREA III | 2-WAY | VASSAL LN | AT LAKEVIEW AVE | SEE TMP-2 |
| HYDRAULIC MOD AREA IV | 1-WAY AT 2-WAY | LEXINGTON ST | AT WORTHINGTON ST | SEE TMP-2 |
| IVY ST | 2-WAY | 4200+10 | 4200+60 | T-10 |
| IVY ST | 2-WAY | 4200+60 | 4200+90 | T-9 |
| IVY ST | 2-WAY | 4200+90 | 4202+45 | T-10 |
| IVY ST | 2-WAY | 4202+45 | 4202+85 | T-9 |
| SAVILLE ST | 2-WAY | - | - | T-11 |
| WALDEN ST | 2-WAY | 1524+65 | 1531+60 | T-6 |
| WALDEN ST | 2-WAY | 1531+60 | 1532+00 | T-2 / T-6 |
| WALDEN ST | 2-WAY | 1532+00 | 1532+45 | DETOURS |
| WHEELER ST MWRA VAULT MODIFICATIONS | 2-WAY | WHEELER ST | - | SEE TMP-3 |

*NOTE: ALL STATIONING IS APPROXIMATE. ACTUAL EXTENT OF SPECIFIC TRAFFIC MANAGEMENT TO BE COORDINATED IN THE FIELD

CONFORMED SET

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| Scale | NO SCALE | | |
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| Client | CITY OF CAMBRIDGE, MASSACHUSETTS | |
| Project | CONCORD (CONTRACT 9) SEWER SEPARATION AND SURFACE IMPROVEMENTS PROJECT | |
| Drawing | TRAFFIC GENERAL TRAFFIC DETAIL LOCATIONS TABLE | |

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| Sheet | TG-3 |
| File No. | |